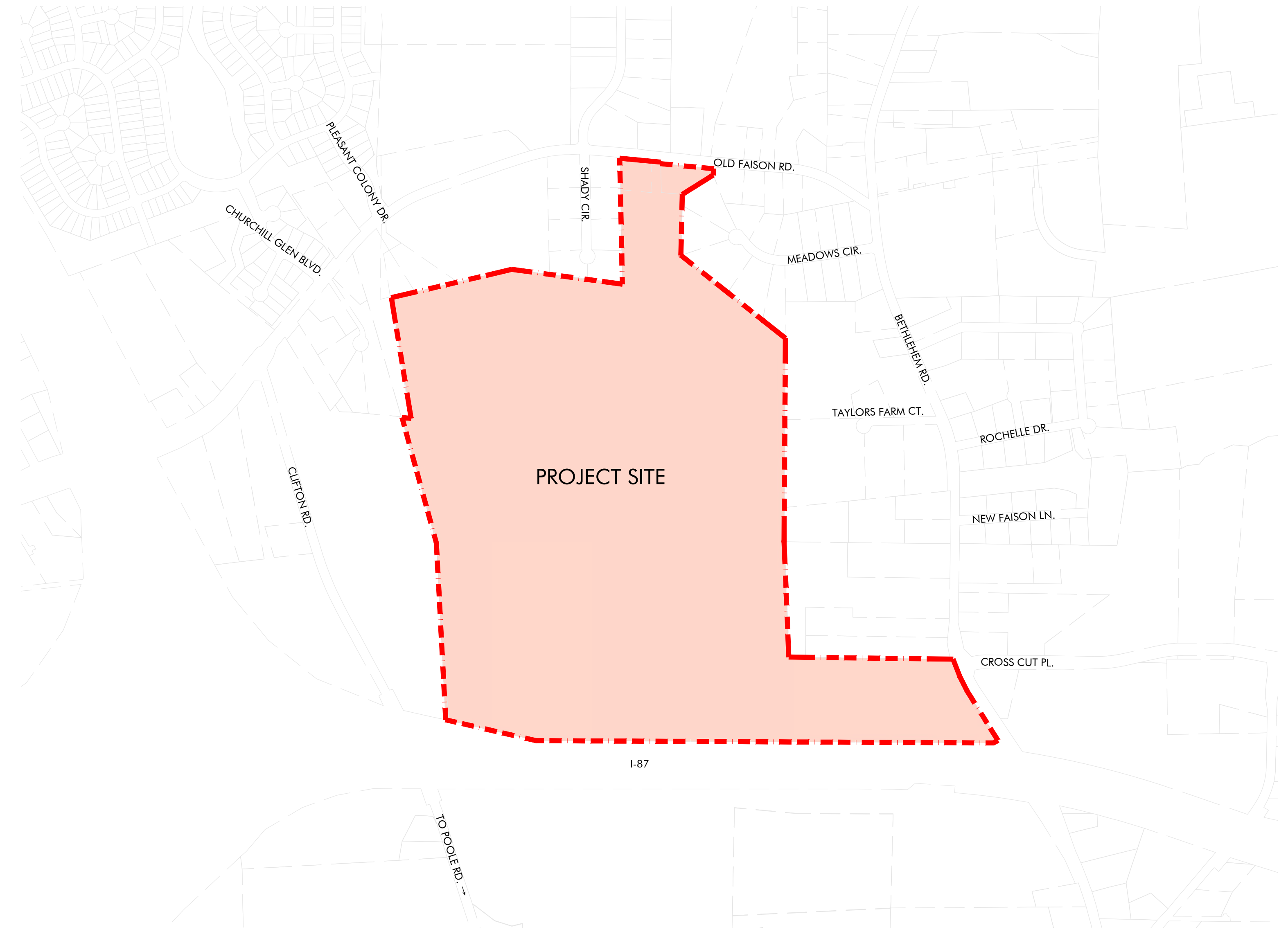


Lyndon Oaks Master Plan

Town of Knightdale RMX-PUD Master Plan Submittal: ZMA-2-23 Fourth Submittal: 05/03/2024



VICINITY MAP 1" = 500'

GENERAL NOTES:

- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH TOWN OF KNIGHTDALE AND NCDOT STANDARDS AND SPECIFICATIONS IF APPLICABLE.
- THE CONTRACTOR SHALL OBTAIN A RIGHT-OF-WAY PERMIT FOR ANY WORK WHICH REQUIRES THE CLOSURE OF A TRAVEL LANE(S), PARKING SPACE, OR SIDEWALK FROM RIGHT-OF-WAY SERVICES AT LEAST 48 HOURS IN ADVANCE.
- NO WORK WILL BE ALLOWED WITHIN NCDOT ROW UNTIL ALL ENCROACHMENT AGREEMENTS ARE APPROVED BY NCDOT.
- IF CONSTRUCTION PLANS FOR PUBLIC AND PRIVATE STREETS OR UTILITIES SHOWN ON THIS PLAN ARE REQUIRED, THEY MUST BE APPROVED BY THE PUBLIC WORKS DEPARTMENT AND PUBLIC UTILITIES DEPARTMENT PRIOR TO ISSUANCE OF PERMITS OR RECORDINGS OF ANY PLAN FOR THIS DEVELOPMENT.
- FIELD ADJUSTMENTS TO THIS PLAN MAY BE REQUIRED BY TOWN OF KNIGHTDALE INSPECTOR AS NEEDED DURING CONSTRUCTION.
- ALL PROPOSED CURB AND GUTTER WITHIN PUBLIC RIGHT OF WAY SHOWN ON PLANS TO BE 30" TOWN OF KNIGHTDALE STANDARD CONCRETE CURB AND GUTTER, AND ALL OTHER PROPOSED CURB AND GUTTER TO BE 24" CONCRETE CURB AND GUTTER UNLESS OTHERWISE STATED ON PLANS.
- ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, UNLESS OTHERWISE STATED ON PLANS.
- WITHIN THE SIGHT TRIANGLES SHOWN ON THIS PLAN, NO OBSTRUCTION BETWEEN 2 FEET AND 8 FEET IN HEIGHT ABOVE THE CURB LINE ELEVATION SHALL BE LOCATED IN WHOLE OR PART. OBSTRUCTIONS INCLUDE BUT ARE NOT LIMITED TO ANY BEAM, FOLIAGE, FENCE, WALL, SIGN, OR PARKED VEHICLE.
- UNLESS NOTED, ACCESS ROUTE FOR EMERGENCY VEHICLES SHALL PROVIDE AN INSIDE TURNING RADIUS OF 28' MINIMUM.
- TRASH AND CARDBOARD DUMPSTER(S) ENCLOSURE SHALL COMPATIBLE WITH MATERIAL AND/OR COLOR OF THE PRINCIPAL BUILDING.
- CONTRACTOR TO FIELD LOCATE AND VERIFY ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION AND REPORT ANY DISCREPANCIES TO LANDSCAPE ARCHITECT PRIOR TO ANY CONSTRUCTION ACTIVITIES. CONTACT NC ONE AT 811 FOR FIELD LOCATION OF UNDERGROUND UTILITIES.
- HANDICAP PARKING SPACE(S) AND HC ACCESS AISLE(S) SHALL BE NO GREATER THAN TWO PERCENT (2%) PITCH IN ANY DIRECTION(S) AS PER ADA STANDARDS.
- PROVIDE SIGNAGE AND STRIPING OF HANDICAP SPACES AS PER ADA STANDARDS.
- ALL RETAINING WALLS GREATER THAN 30" IN HEIGHT TO INCLUDE SAFETY RAIL OR FENCE. NO RETAINING WALLS ARE PERMITTED IN THE RIGHT-OF-WAY UNLESS APPROVED BY ENCROACHMENT.
- THE MINIMUM CORNER CLEARANCE FROM THE CURB LINE OF INTERSECTING STREETS SHALL BE AT LEAST 20 FEET FROM THE POINT OF TANGENCY OF THE CURB FOR RESIDENTIAL DRIVEWAY. NO DRIVEWAYS SHALL ENCOACH ON THIS MINIMUM CORNER CLEARANCE.
- WC ACCESS RAMPS WILL BE PROVIDED IN ACCORDANCE WITH TOWN OF KNIGHTDALE PUBLIC WORKS DEPARTMENT STANDARDS, PROWAG STANDARDS AND ADAAG SPECIFICATIONS
- ALL RAMPS AND HANDRAILS SHALL BE CONFORM TO ANSI STANDARDS.
- ALL ABOVE GROUND UTILITY DEVICES (TO INCLUDE BUT NOT LIMITED TO TELEPHONE AND CABLE PEDESTALS, ELECTRICAL TRANSFORMERS, BACKFLOW DEVICE HOTBOX, ETC) SHALL BE SCREENED FROM OFF-SITE VIEW BY EVERGREEN SHRUBS, FENCE, OR WALL.
- ALL SIDEWALKS MUST BE ACCESSIBLE TO PERSONS WHO ARE BLIND, HAVE LOW VISION AND PEOPLE WITH MOBILITY DISABILITIES. PEDESTRIAN EXISTING ROUTES AND ALTERNATE PEDESTRIAN ROUTES DURING CONSTRUCTION WILL BE REQUIRED TO BE COMPLIANT WITH THE PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG), 2010 ADA STANDARDS FOR ACCESSIBLE DESIGN AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- IF UNFORESEEN CONDITIONS DEVELOP DURING CONSTRUCTION, REFER TO NCDOT SPECIFICATIONS AND DETAILS AND CONTACT TOWN OF KNIGHTDALE PUBLIC WORKS DEPARTMENT FOR FURTHER GUIDANCE.
- PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE TOWN OF KNIGHTDALE ENGINEERING DEPARTMENT TO REVIEW THE SPECIFIC COMPONENTS OF THE PLAN AND OPERATION OF THESE FACILITIES DURING CONSTRUCTION. CONTACT TOWN OF KNIGHTDALE INSPECTOR'S OFFICE AT 919-217-2250 TO SET UP THE MEETING.
- THE CONTRACTOR SHALL CONDUCT THE WORK IN A SAFE MANNER AND WITH A MINIMUM AMOUNT OF INCONVENIENCE TO TRAFFIC.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL AND SHALL ADHERE TO THE PROVISIONS OF THE MUTCD (MOST CURRENT EDITION).
- PRIOR TO CONSTRUCTION BEGINNING, ALL SIGNAGE AND TRAFFIC CONTROL SHALL BE IN PLACE.
- THE PLANS ARE PRELIMINARY AND SUBJECT TO CHANGE AT THE TIME OF SITE PLAN/CONSTRUCTION DOCUMENTS.

BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):	
- FRONT:	0' MIN TO 25' MAX
- SIDE:	10' MIN
- REAR:	10' MIN
SINGLE FAMILY DETACHED (REAR-LOADED):	
- FRONT:	10' MIN
- REAR:	10' MIN
- SIDE:	3' MIN
- CORNER:	10' MIN
SINGLE FAMILY DETACHED (FRONT-LOADED):	
- FRONT:	10' MIN
- REAR:	20' MIN
- SIDE:	5' MIN
- CORNER:	10' MIN
COMMERCIAL:	
- FRONT:	10' MIN
- REAR:	0' MIN
- SIDE:	6' MIN
CLUBHOUSE:	
- FRONT:	10' MIN
- REAR:	0' MIN
- SIDE:	6' MIN

CONTACT INFO:

OWNER:
CONTACT: TERRY LANCE FERRELL, RANDAL HUTCHINS FERRELL
ADDRESS: 26 SAINT JULIANS CT
PAWLEYS ISLAND, SC 29585-6309

OWNER:
CONTACT: WAYNE L. HARPER, EUGENE J. HARPER
ADDRESS: 4113 BREWSTER DR
RALEIGH, NC 27606-1711

OWNER:
CONTACT: DEBORAH JANE TART, MELTON E JR
ADDRESS: 4325 OLD FASHION RD
KNIGHTDALE NC 27545-9179

OWNER:
CONTACT: MILTON EDWARD III TART, JANET STANLEY TART
ADDRESS: 106 TART FARM RD
KNIGHTDALE NC 27545-8134

OWNER:
CONTACT: LEE T ALFORD
ADDRESS: 4313 OLD FASHION RD
KNIGHTDALE NC 27545-9179

OWNER:
CONTACT: MILTON E JR TART, PATSY P TART
ADDRESS: 4325 OLD FASHION RD
KNIGHTDALE NC 27545-9179

CIVIL ENGINEER:
URBAN DESIGN PARTNERS PLLC
CONTACT: SALMAN MOAZZAM, PE
ADDRESS: 150 FAYETTEVILLE ST. SUITE 1310
RALEIGH, NC 27601
PHONE: 919-275-5002
EMAIL: SMOAZZAM@URBANDESIGNPARTNERS.COM

LANDSCAPE ARCHITECT:
URBAN DESIGN PARTNERS PLLC
CONTACT: BRIAN RICHARDS, PLA
ADDRESS: 150 FAYETTEVILLE ST. SUITE 1310
RALEIGH, NC 27601
PHONE: 919-275-5002
EMAIL: BRICHARDS@URBANDESIGNPARTNERS.COM

DEVELOPER:
D.R. HORTON
CONTACT: REESE BRIDGES, PE
ADDRESS: 7208 FALLS OF NEUSE RD. SUITE 201
RALEIGH, NC 27615
PHONE: 984-247-9614
EMAIL: TBRIDGES@DRHORTON.COM

SITE DATA:

PIN(S): 1743953683; 1743971085; 1753071583; 1743976575; 1743989384; 1743986356; 1753152116

TOTAL ACRES: ± 171.88 ACRES

EXISTING ZONING: RT (RURAL TRANSITIONAL)
PROPOSED ZONING: RMX-PUD

EXISTING USE: RESIDENTIAL & AGRICULTURAL
PROPOSED USE: RESIDENTIAL & COMMERCIAL OUTPARCEL

DEVELOPMENT SUMMARY:

COMMERCIAL:
*TOTAL LAND AREA: ± 1.76 AC
*BUILDINGS: 1 BLDG (±15,000 SF)
*BUILDING HEIGHT: MAXIMUM 2 STORIES
ALLOWABLE USES: CHILD/ADULT DAY CARE CENTER (6 OR MORE PEOPLE), PERSONAL SERVICES, PROFESSIONAL SERVICES, MEDICAL SERVICES, NEIGHBORHOOD RETAIL/RESTAURANT (2,000 SF OR LESS), ALLOWED RMX ACCESSORY USES, BAR/TAVERN/MICROBREWERY, STUDIO (ARTS, DANCE, MARTIAL ARTS, MUSIC)
*NOTE: SEE PUD DOCUMENT FOR MORE INFORMATION

RESIDENTIAL:
*TOTAL LAND AREA: ± 129.2 AC
*PROPOSED DENSITY: 482 UNITS/171.88 AC
2.8 UNITS/AC

*PROPOSED UNITS: 482 DU TOTAL (UP TO 500 DU PERMITTED)

- TOWNHOMES/DUPLEXES
 - 20' UNIT: 117 UNITS
 - 22' TOWNHOME: 130 UNITS
 - 22' DUPLEX: 26 UNITS
- SINGLE FAMILY (REAR-LOAD)
 - 30' LOT: 46 LOTS
 - 35' LOT: 39 LOTS
 - 40' LOT: 24 LOTS
- SINGLE FAMILY (FRONT-LOAD)
 - 60' LOT: 66 LOTS
 - 70' LOT: 24 LOTS
 - 80' LOT: 10 LOTS

OPEN SPACE:

- REQUIRED: 17.26 AC TOTAL REQUIRED
- ACTIVE: 8.63 AC (50% OF REQ. OPEN SPACE)
- PASSIVE: 8.63 AC (50% OF REQ. OPEN SPACE)
- PROPOSED: 21.53 AC TOTAL PROPOSED
- ACTIVE: 8.77 AC ACTIVE OPEN SPACE PROP.
- PASSIVE: 12.76 AC PASSIVE OPEN SPACE PROP.

TREE PRESERVATION:

- REQUIRED: 6.44 AC TOTAL REQUIRED
- PROPOSED: 19.39 AC TOTAL PROPOSED

PARKING CALCS: SEE SHEET C-3.0 FOR BREAKDOWN & REQUIREMENTS

LAND USE TOTALS:

- TOTAL ACRES: 171.88 AC TOTAL
- COMMERCIAL: ± 1.76 AC TOTAL PROP.
- RESIDENTIAL: ± 129.2 AC TOTAL PROP.
- OPEN SPACE: ± 21.53 AC TOTAL PROP.
- TREE PRESERVATION: ± 19.39 AC TOTAL PROP.

Sheet List Table

Sheet Number	Sheet Title	
C-1.0	Cover Sheet	C-4.7 Signage & Marking Plan Enlargement 7
--	Existing Conditions ALTA	C-4.8 Street Sections
--	Existing Conditions TOPO	C-4.81 Street Sections
--	Wetland Report	C-4.9 Connectivity Index
C-3.0	Overall Site Plan	C-4.10 Pedestrian Circulation
C-3.1	Site Plan Enlargement 1	C-5.0 Overall Storm Drainage Plan
C-3.2	Site Plan Enlargement 2	C-6.0 Overall Utility Plan
C-3.3	Site Plan Enlargement 3	C-6.1 Utility Plan Enlargement 1
C-3.4	Site Plan Enlargement 4	C-6.2 Utility Plan Enlargement 2
C-3.5	Site Plan Enlargement 5	C-6.3 Utility Plan Enlargement 3
C-3.6	Site Plan Enlargement 6	C-6.4 Utility Plan Enlargement 4
C-3.7	Site Plan Enlargement 7	C-6.5 Utility Plan Enlargement 5
C-3.8	Open Space Plan	C-6.6 Utility Plan Enlargement 6
C-3.9	Phasing Plan	C-6.7 Utility Plan Enlargement 7
C-4.0	Overall Signage & Marking Plan	LS-1.0 Overall Landscape Plan
C-4.1	Signage & Marking Plan Enlargement 1	LS-1.1 Landscape Plan Enlargement 1
C-4.2	Signage & Marking Plan Enlargement 2	LS-1.2 Landscape Plan Enlargement 2
C-4.3	Signage & Marking Plan Enlargement 3	LS-1.3 Landscape Plan Enlargement 3
C-4.4	Signage & Marking Plan Enlargement 4	LS-1.4 Landscape Plan Enlargement 4
C-4.5	Signage & Marking Plan Enlargement 5	LS-1.5 Landscape Plan Enlargement 5
C-4.6	Signage & Marking Plan Enlargement 6	LS-1.6 Landscape Plan Enlargement 6
		LS-1.7 Landscape Plan Enlargement 7
		LS-2.0 Landscape Details
		LS-2.1 Landscape Details
		LS-3.0 Lighting Plan
		A-1.0 Architectural Elevations

DISTRIBUTION OF USES

USE AREAS				
USE TYPE	TYP. LOT SIZE (SF)	# OF LOTS	TOTAL AREA (SF)	ACRES
SINGLE FAMILY				
30' LOT	3,600	46	165,600	
35' LOT	4,200	39	163,800	
40' LOT	4,800	24	115,200	
60' LOT	7,200	66	475,200	
70' LOT	8,400	24	201,600	
80' LOT	9,600	10	96,000	
			1,217,400	27.9
DUPLEX / TOWNHOUSE				
20' TH	1,800	117	210,600	
22' TH	1,980	130	257,400	
22' DUPLEX	2,160	26	56,160	
			524,160	12.0
COMM/RETAIL*			25,689	0.6
DISTRIBUTION OF USES				
				ACRES
GROSS SITE AREA (PER SURVEY)				
				171.8
DEDICATED RECREATIONAL OPEN SPACE				21.53
PUBLIC R/W				28.3
NET AREA**				121.97
USE TYPE	PROP. (AC)	MIN.-MAX. DISTRIBUTION(%)	PROV. DIST. (%)	
SINGLE FAMILY	27.9	15%-60%	22.9%	
DUPLEX / TOWNHOUSE	12.0	10%-40%	9.9%	
COMMERCIAL / RETAIL	0.6	5%-20%	0.5%	



150 Fayetteville st ste 1310
raleigh, nc 27601
+ 919.275.5002
urbanpartners.com

PRELIMINARY DRAWING
FOR REVIEW PURPOSES ONLY

05/03/2024

D.R. Horton
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

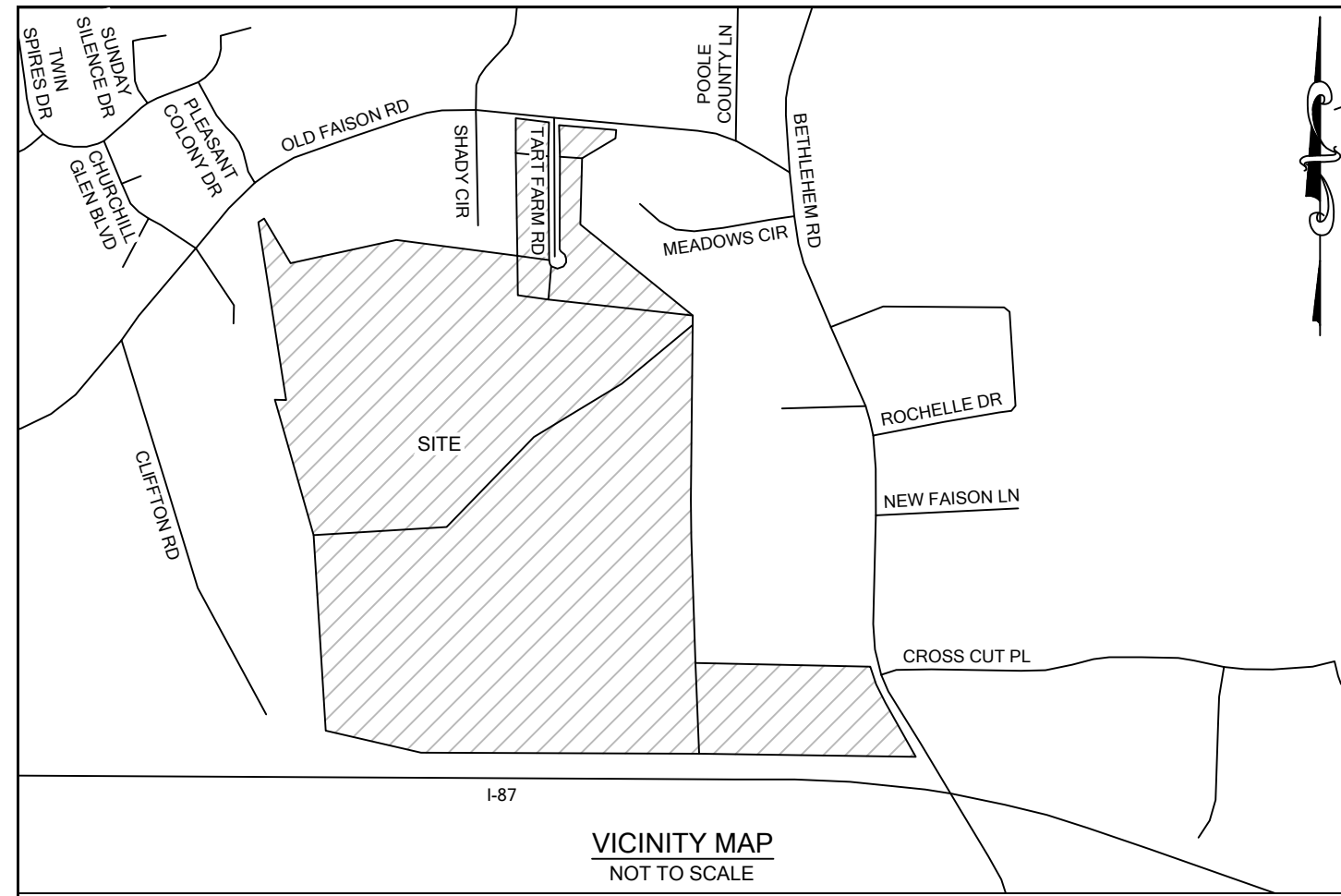
LYNDON OAKS Master Plan Cover Sheet

Knightdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083
Date: 05/03/2024
Sheet No:

C-1.0



LEGEND

●	EXISTING BOUNDARY CORNER FOUND
○	BOUNDARY CORNER SET (3/4" IPS UNLESS OTHERWISE NOTED)
○	COMPUTED POINT
■	CONCRETE MONUMENT FOUND
□	CLEAN OUT
□	TRANSFORMER / ELECTRIC BOX
□	LIGHT POLE
□	UTILITY POLE
□	WATER METER
□	FIRE HYDRANT
□	UTILITY VALVE
□	TELEPHONE PEDESTAL
□	CABLE PEDESTAL
□	CATCH BASIN
□	DROP INLET
□	UTILITY MANHOLE
□	UTILITY HAND HOLE
□	AIR CONDITIONING UNIT
□	WELL
□	SIGN
—	UNDERGROUND FIBER
—	UNDERGROUND TELEPHONE
—	UNDERGROUND GAS
—	UNDERGROUND ELECTRIC
—	STORM DRAIN
—	OVERHEAD UTILITY
—	GUY WIRE
—	BURIED UTILITY MARKER
—	EXISTING IRON PIPE
—	EXISTING IRON REBAR
—	IRON PIPE SET
—	PK NAIL SET
—	PK NAIL FOUND
—	SANITARY SEWER MANHOLE
—	NORTH
—	EAST
—	SOUTH
—	WEST
—	NORTHEAST
—	SOUTHEAST
—	SOUTHWEST
—	NORTHWEST
—	DEED BOOK
—	PAGE
—	BOOK OF MAPS
—	RIGHT OF WAY
—	POINT OF BEGINNING
—	TOTAL
—	TIE (INDICATES TWO PARTS OF THE SAME PARCEL)

NOTES

1. THE PROPERTY LIES IN ZONE "X" PER NATIONAL FLOOD INSURANCE PROGRAM FLOOD INSURANCE RATE MAP COMMUNITY NUMBER 3720174300K, DATED 07/19/2022.
2. ALL DISTANCES ARE HORIZONTAL GROUND DISTANCES AND ALL BEARINGS ARE BASED ON GPS OBSERVATIONS, NAD 83/NSRS 2011/SPC UNLESS OTHERWISE SHOWN.
3. SITE ZONED "RT" AND "GR3" FOR THE TOWN OF KNIGHTDALE PER WAKE COUNTY GIS.
4. AREAS COMPUTED BY COORDINATE METHOD.
5. THIS SURVEY WAS PREPARED BY BATEMAN CIVIL SURVEY COMPANY, UNDER THE SUPERVISION OF STEVEN P. CARSON, PLS.
6. THIS MAP HAS NOT BEEN REVIEWED BY A LOCAL GOVERNMENT AGENCY FOR COMPLIANCE WITH ANY APPLICABLE LAND DEVELOPMENT REGULATIONS.
7. NO CEMETERIES OR GRAVES WERE OBSERVED AS A RESULT OF THIS SURVEY.
8. WETLANDS SHOWN PER FLAGGING SET BY SAGE ECOLOGICAL SERVICES AND REVISED WETLAND SKETCH MAP DATED FEBRUARY 6, 2024.

TO: DHI TITLE OF NORTH CAROLINA, AND D. R. HORTON, INC., A DELAWARE CORPORATION

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTNPS/LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS OF TABLE A THEREOF (NONE SELECTED). THE FIELDWORK WAS COMPLETED ON OCTOBER 3RD, 2022.

PRELIMINARY

STEVEN P. CARSON, PLS. DATE
NC LICENSE NO. 4752

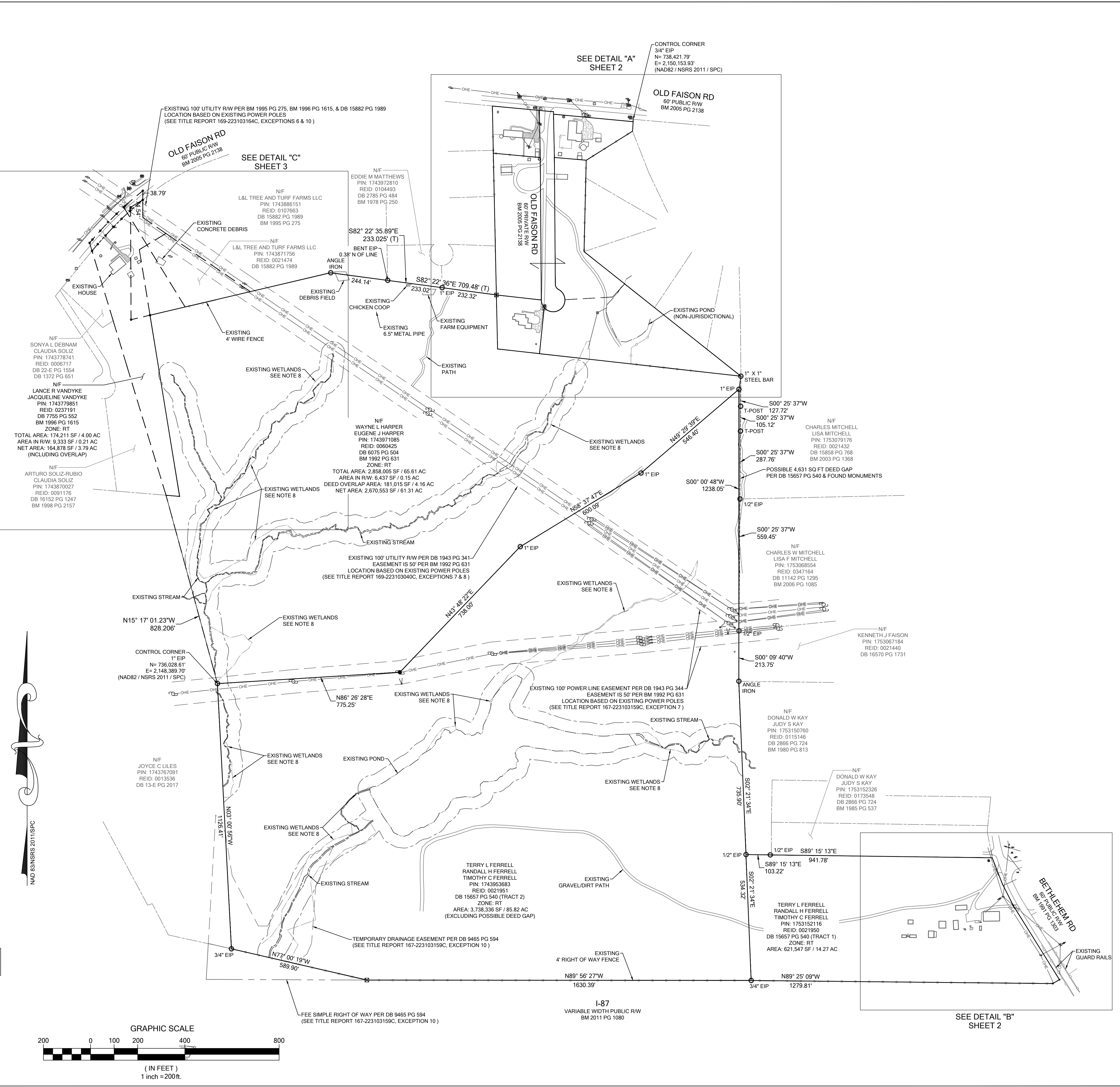
I, STEVEN P. CARSON, CERTIFY THAT THIS PLAT WAS DRAWN UNDER MY SUPERVISION FROM AN ACTUAL SURVEY MADE UNDER MY SUPERVISION (DEED REFERENCES AS SHOWN IN TITLE BLOCK), THAT THE BOUNDARIES NOT SURVEYED ARE CLEARLY INDICATED AS DRAWN FROM INFORMATION FOUND AS SHOWN HERE ON, THAT THE RATIO OF PRECISION AS CALCULATED IS 1:10,000 +, THAT THIS PLAT WAS PREPARED IN ACCORDANCE WITH G.S. 47-30 AS AMENDED, WITNESS MY ORIGINAL SIGNATURE, REGISTRATION NUMBER AND SEAL THIS DAY OF A.D., 2022.

I, FURTHER THAT IN ACCORDANCE WITH G.S. 47-30-F-11-C-1, CERTIFY THAT THE SURVEY IS OF AN EXISTING PARCEL OR PARCELS OF LAND OR ONE OR MORE EXISTING EASEMENTS AND DOES NOT CREATE A NEW STREET OR CHANGE AN EXISTING STREET. FOR THE PURPOSES OF THIS SUBSECTION, AN "EXISTING PARCEL" OR "EXISTING EASEMENT" IS AN AREA OF LAND DESCRIBED IN A SINGULAR LEGAL DESCRIPTION OR LEGALLY RECORDED SUBDIVISION THAT HAS BEEN OR MAY BE LEGALLY CONVEYED TO A NEW OWNER BY DEED IN ITS EXISTING CONFIGURATION.

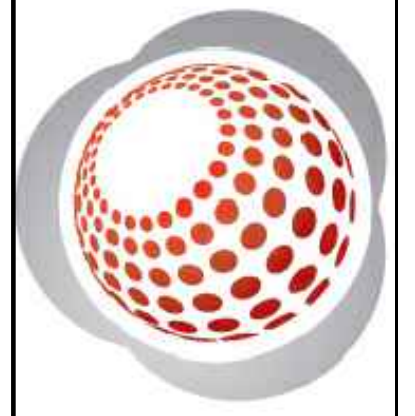
PRELIMINARY

STEVEN P. CARSON, PLS.
NC LICENSE NO. 4752

THIS MAP IS CONSIDERED PRELIMINARY, NOT FOR RECORDATION, CONVEYANCE OR SALES UNLESS SIGNED AND SEALED BY THE LICENSED SURVEYOR.



BATEMAN CIVIL SURVEY COMPANY
ENGINEERS • SURVEYORS • PLANNERS
2524 RELIANCE AVENUE - APEX, NC 27539
PHONE: (919) 577-1080 FAX: (919) 577-1081
INFO@BATEMANCIVILSURVEY.COM
NCBELS FIRM# C-2378

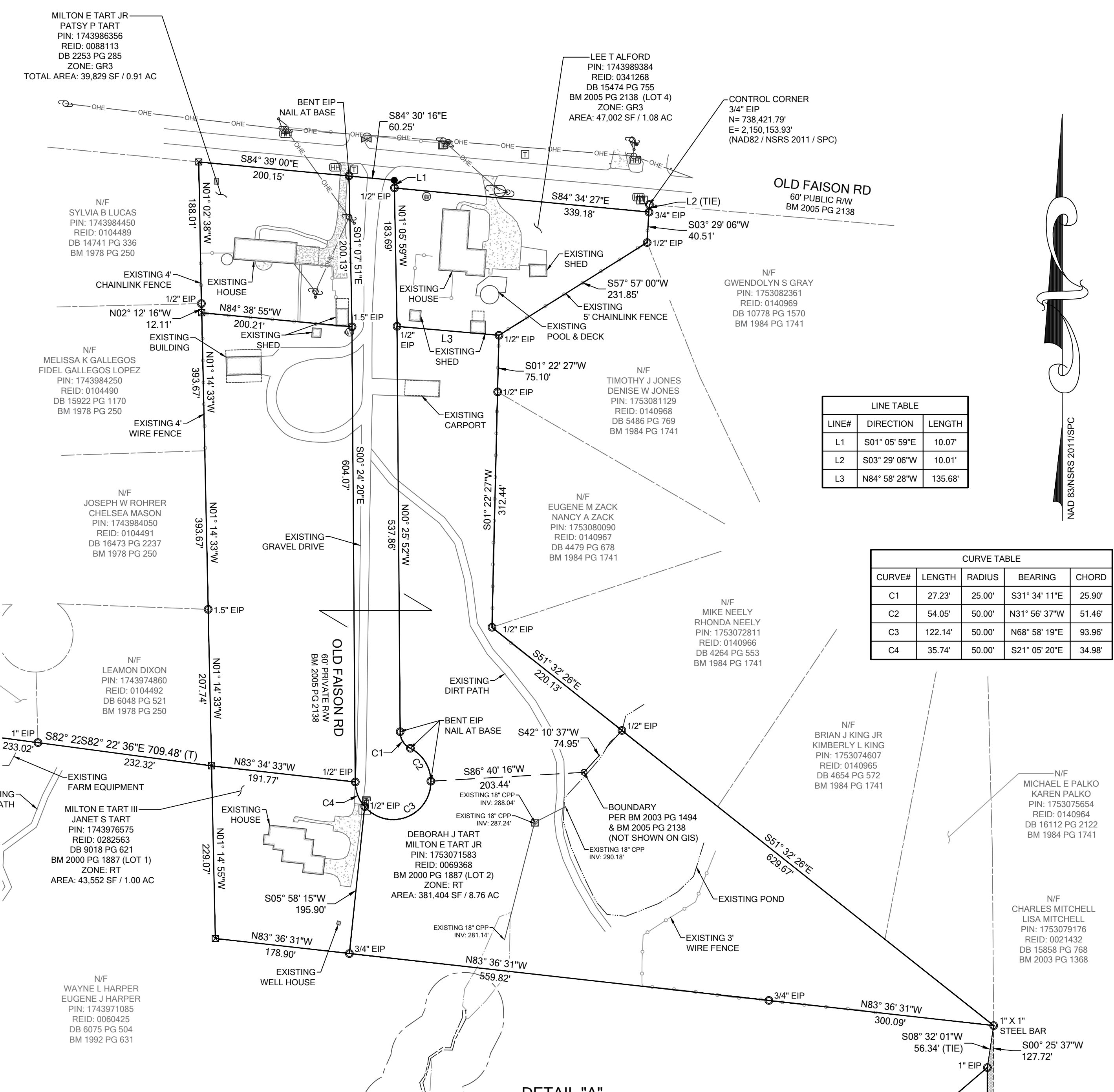
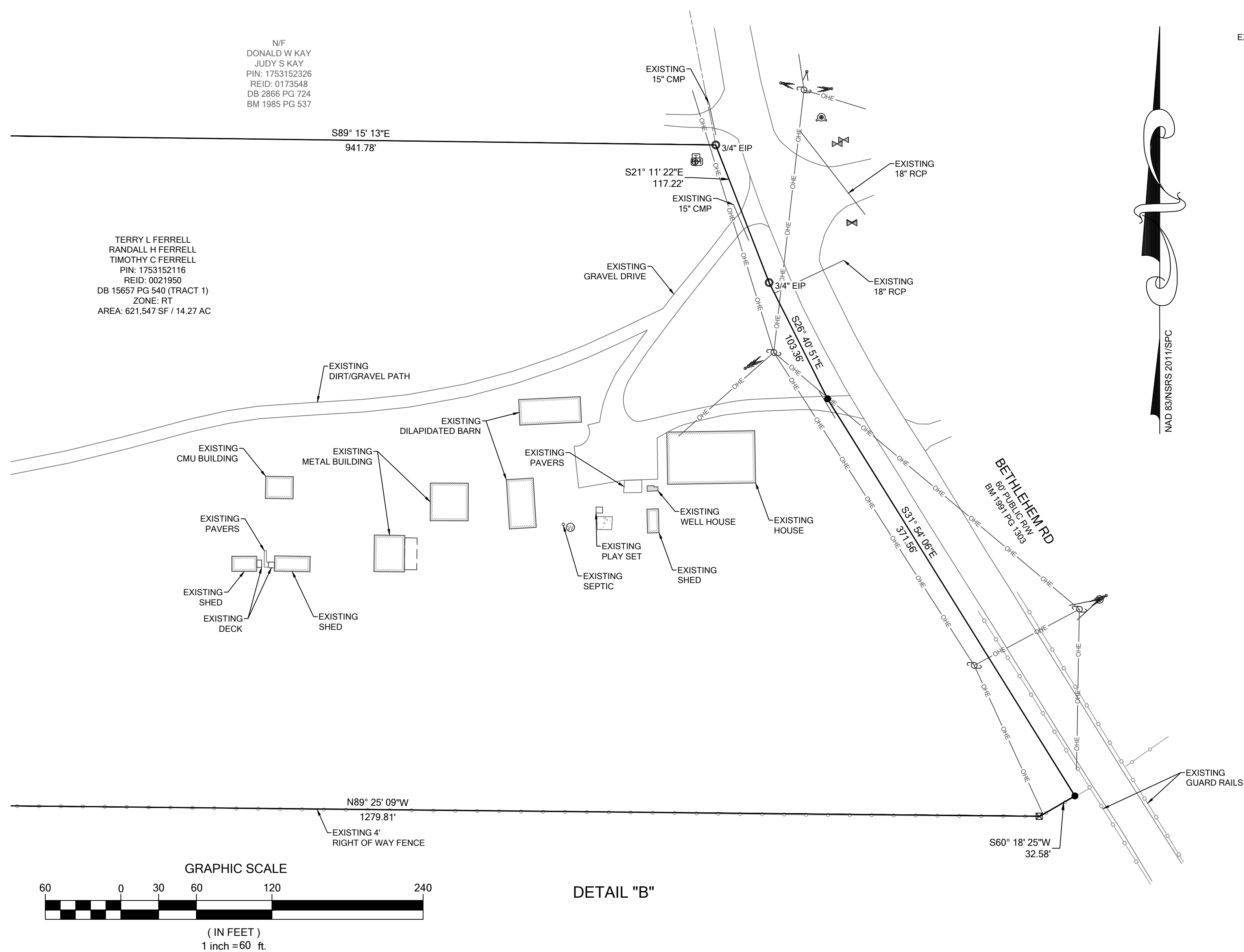


ALTANPS/LAND TITLE SURVEY
EXCLUSIVELY FOR: DR HORTON
PROPERTY PINS: 175316216, 174395363, 1743971085, 174398656, 174398634, 175071983, 1743978575 & 174379851
AS RECORDED IN: DB 15657 PG 540, DB 2253 PG 285, DB 15474 PG 755, BM 2000, PG 1887, DB 9018 PG 621, DB 6075 PG 504 & DB 7755 PG 552
ST MATTHEWS TOWNSHIP - WAKE COUNTY - NORTH CAROLINA

REVISIONS	
1.	
2.	
3.	
4.	
5.	
DESIGNED BY: N/A	
DRAWN BY: ELS	
CHECKED BY: SPC	
SCALE: 1" = 200'	
DATE: 1/27/2023	
JOB NUMBER: 220655	
SHEET 1 OF 4	

LEGEND	
○	EXISTING BOUNDARY CORNER FOUND
●	BOUNDARY CORNER SET (3/4" IPS UNLESS OTHERWISE NOTED)
⊙	COMPUTED POINT
⊠	CONCRETE MONUMENT FOUND
□	CLEAN OUT
⊞	TRANSFORMER / ELECTRIC BOX
⊕	LIGHT POLE
⊖	UTILITY POLE
⊗	WATER METER
⊘	FIRE HYDRANT
⊙	UTILITY VALVE
⊚	TELEPHONE PEDESTAL
⊛	CABLE PEDESTAL
⊜	CATCH BASIN
⊝	DROP INLET
⊞	UTILITY MANHOLE
⊠	UTILITY HAND HOLE
⊡	AIR CONDITIONING UNIT
⊢	WELL
⊣	SIGN
— F —	UNDERGROUND FIBER
— T —	UNDERGROUND TELEPHONE
— GAS —	UNDERGROUND GAS
— E —	UNDERGROUND ELECTRIC
— SD —	STORM DRAIN
— OHE —	OVERHEAD UTILITY
⊙	GUY WIRE
⊙	BURIED UTILITY MARKER
⊙	EXISTING IRON PIPE
⊙	EXISTING IRON REBAR
⊙	IRON PIPE SET
⊙	PK NAIL SET
⊙	PK NAIL FOUND
⊙	SANITARY SEWER MANHOLE
N	NORTH
E	EAST
S	SOUTH
W	WEST
NE	NORTHEAST
SE	SOUTHEAST
SW	SOUTHWEST
NW	NORTHWEST
DB	DEED BOOK
PG	PAGE
BM	BOOK OF MAPS
R/W	RIGHT OF WAY
POB	POINT OF BEGINNING
(T)	TOTAL
(T)	TIE (INDICATES TWO PARTS OF THE SAME PARCEL)

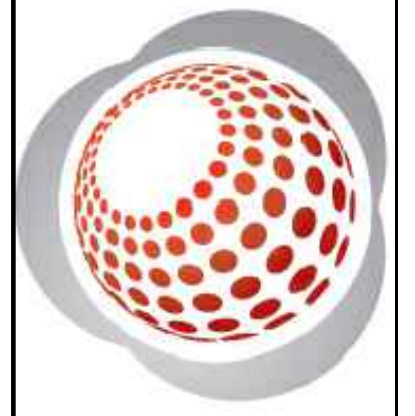
THIS MAP IS CONSIDERED PRELIMINARY, NOT FOR RECORDATION, CONVEYANCE OR SALES UNLESS SIGNED AND SEALED BY THE LICENSED SURVEYOR.



LINE TABLE		
LINE#	DIRECTION	LENGTH
L1	S01° 05' 59"E	10.07'
L2	S03° 29' 06"W	10.01'
L3	N84° 58' 28"W	135.68'

CURVE TABLE				
CURVE#	LENGTH	RADIUS	BEARING	CHORD
C1	27.23'	25.00'	S31° 34' 11"E	25.90'
C2	54.05'	50.00'	N31° 56' 37"W	51.46'
C3	122.14'	50.00'	N68° 58' 19"E	93.96'
C4	35.74'	50.00'	S21° 05' 20"E	34.98'

BATEMAN CIVIL SURVEY COMPANY
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 NCBELS FIRM# C-2378



ALTANSP/LAND TITLE SURVEY
EXCLUSIVELY FOR: DR HORTON
 PROPERTY PINS: 1753162116, 1743958683, 1743971085, 1743986356, 1743989384, 1750071983, 1743978575 & 1743779851
 AS RECORDED IN: DB 15657 PG 540, DB 2253 PG 285, DB 15474 PG 755, BM 2000, PG 1887, DB 9018 PG 621, DB 6075 PG 504 & DB 7755 PG 552
 ST MATTHEWS TOWNSHIP - WAKE COUNTY - NORTH CAROLINA

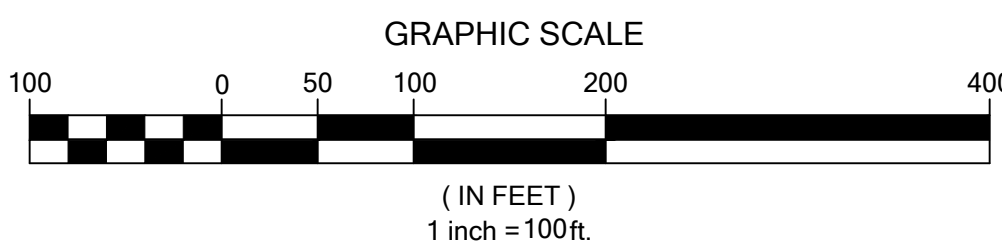
REVISIONS	
1.	
2.	
3.	
4.	
5.	

DESIGNED BY: N/A
DRAWN BY: ELS
CHECKED BY: SPC
SCALE: VARIES
DATE: 1/27/2023
JOB NUMBER: 220655

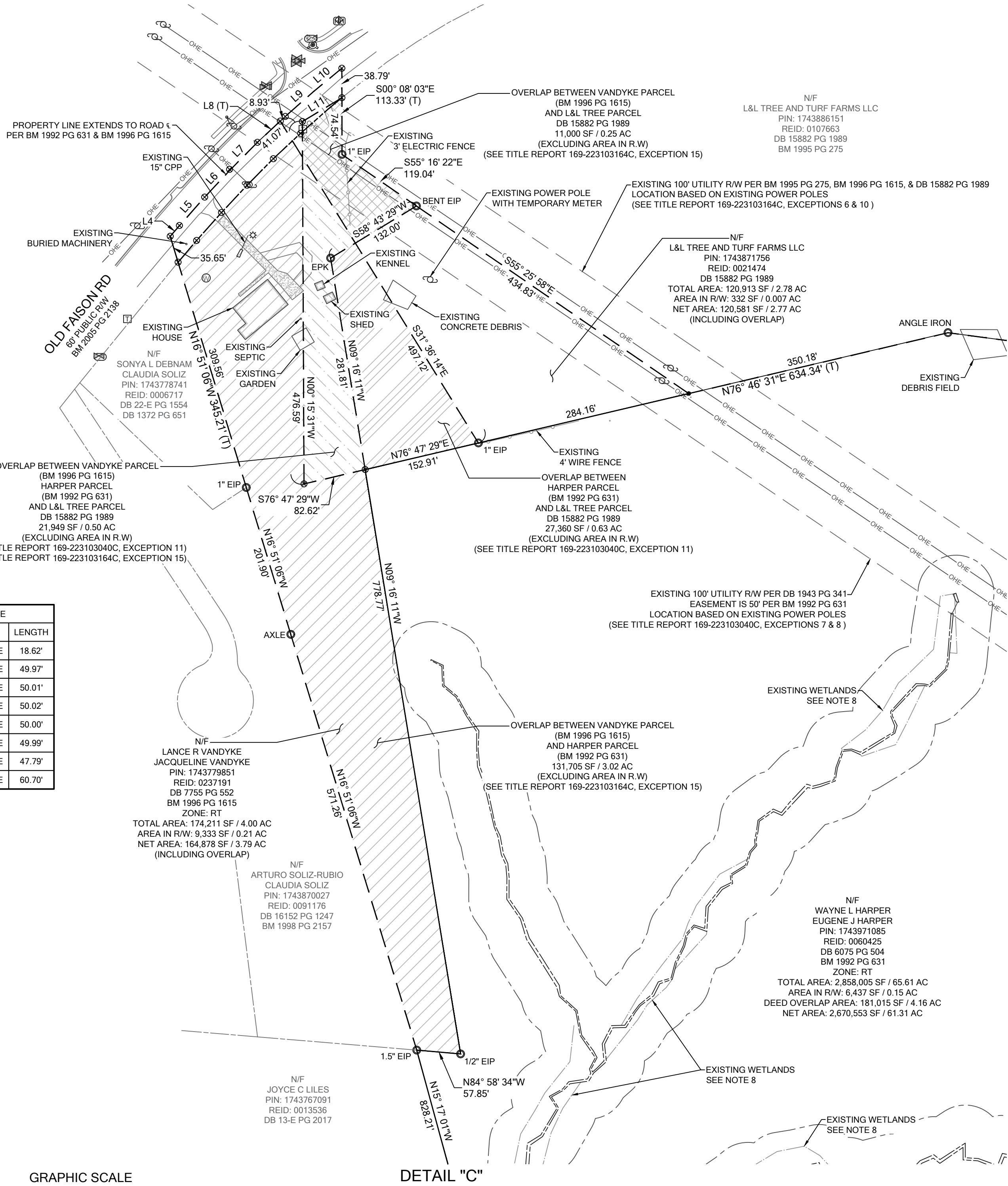
SHEET 2 OF 4



LINE#	DIRECTION	LENGTH
L4	N40° 26' 43"E	18.62'
L5	N41° 41' 39"E	49.97'
L6	N43° 09' 47"E	50.01'
L7	N44° 37' 45"E	50.02'
L8	N47° 05' 10"E	50.00'
L9	N48° 40' 18"E	49.99'
L10	N50° 32' 01"E	47.79'
L11	N58° 56' 59"E	60.70'



THIS MAP IS CONSIDERED PRELIMINARY, NOT FOR RECORDATION, CONVEYANCE OR SALES UNLESS SIGNED AND SEALED BY THE LICENSED SURVEYOR.



DETAIL "C"

LEGEND	
	EXISTING BOUNDARY CORNER FOUND
	BOUNDARY CORNER SET (3/4" IPS UNLESS OTHERWISE NOTED)
	COMPUTED POINT
	CONCRETE MONUMENT FOUND
	CLEAN OUT
	TRANSFORMER / ELECTRIC BOX
	LIGHT POLE
	UTILITY POLE
	WATER METER
	FIRE HYDRANT
	UTILITY VALVE
	TELEPHONE PEDESTAL
	CABLE PEDESTAL
	CATCH BASIN
	DROP INLET
	UTILITY MANHOLE
	UTILITY HAND HOLE
	AIR CONDITIONING UNIT
	WELL
	SIGN
	UNDERGROUND FIBER
	UNDERGROUND TELEPHONE
	UNDERGROUND GAS
	UNDERGROUND ELECTRIC
	STORM DRAIN
	OVERHEAD UTILITY
	GUY WIRE
	BURIED UTILITY MARKER
	EXISTING IRON PIPE
	EXISTING IRON REBAR
	IRON PIPE SET
	PK NAIL SET
	PK NAIL FOUND
	SANITARY SEWER MANHOLE
	NORTH
	EAST
	SOUTH
	WEST
	NORTHEAST
	SOUTHEAST
	SOUTHWEST
	NORTHWEST
	DEED BOOK
	PAGE
	BOOK OF MAPS
	RIGHT OF WAY
	POINT OF BEGINNING
	TOTAL
	TIE (INDICATES TWO PARTS OF THE SAME PARCEL)

SURVEY COMMENTS
SURVEY MADE BY BATEMAN CIVIL SURVEY COMPANY
DATED 12/12/2022, JOB NO. 220655

SCHEDULE A

ISSUING OFFICE: DHI TITLE OF NORTH CAROLINA, 12276 SAN JOSE BLVD, SUITE 739, JACKSONVILLE, FL 32223
COMMITMENT NO.: 167-223103159C
PROPERTY ADDRESS: LOT ACQUISITION FERRELL; MASTER, N/A, NC 00000

- COMMITMENT DATE: NOVEMBER 8, 2022 AT 09:00 AM
- POLICY TO BE ISSUED:
(A) ALTA OWNER'S POLICY (06-16-07)
PROPOSED INSURED: D.R. HORTON, INC., A DELAWARE CORPORATION
PROPOSED POLICY AMOUNT: \$10,000.00
- THE ESTATE OR INTEREST IN THE LAND DESCRIBED OR REFERRED TO IN THIS COMMITMENT IS:
FEE SIMPLE
- THE TITLE IS, AT THE COMMITMENT DATE, VESTED IN:
TIMOTHY CLARENCE FERRELL, RANDALL HUTCHINS FERRELL, AND TERRY LANCE FERRELL AND CHERYL ANNE FERRELL, TRUSTEES OF THE TERRY LANCE FERRELL AND CHERYL ANN FERRELL FAMILY TRUST DATED JULY 26, 2018
- THE LAND IS DESCRIBED AS FOLLOWS:
LYING AND BEING IN WAKE COUNTY, NORTH CAROLINA, AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

FIRST TRACT:
LOCATED ON THE WEST SIDE OF PAVED ROAD NO. 2049 AND BEGINNING AT POINT IN CENTER OF SAID ROAD BEING THE SOUTHEAST CORNER OF TRACT NO. 2 ON MAP HEREAFTER REFERRED TO, AND RUNS THENCE SOUTH 29 DEG. 00 MIN. EAST 632 FEET TO CORNER IN SAID ROAD; THENCE NORTH 85 DEG. 33 MIN. WEST 1350 FEET TO CORNER; THENCE NORTH 00 DEG. 48 MIN. EAST 551 FEET TO THE SOUTHWEST CORNER OF THE NORTH PORTION OF TRACT NO. L; THENCE A DIVIDING LINE BETWEEN THE NORTH AND SOUTH PORTIONS OF TRACT NO. L, SOUTH 85 DEG. 33 MIN. EAST 1045 FEET TO THE POINT OF BEGINNING, AND BEING THE SOUTH PORTION OF TRACT NO. L OF THE PROPERTY OF THE J. J. FERRELL HEIRS AS SHOWN ON MAP PREPARED BY SMITH AND SMITH, APEX, N. C. FROM DEEDS NOTED ON SAID MAP. CONTAINS 1 1/4 ACRES MORE OR LESS.

SECOND TRACT:
BOUNDED ON THE NORTH BY A ROAD; ON THE EAST BY THE FAISON AND FERRELL LAND; ON THE SOUTH BY A. C. SPARKS AND ON THE WEST BY A BRANCH, AND DESCRIBED AS FOLLOWS: BEGINNING AT A FENCE POST, CORNER BETWEEN J. J. FERRELL AND A. C. SPARKS, AND RUNS THENCE NORTH 86 DEG. WEST 231 FEET TO A BRANCH, SAID POINT BEING WITNESSED BY A STAKE AND POINTERS; THENCE ALONG SAID BRANCH IN A NORTHERLY DIRECTION 1100 FEET TO A ROAD; THENCE ALONG SAID ROAD THE FOLLOWING COURSES AND DISTANCES, N. 88 DEG. 30 MIN. EAST 770 FEET; N. 48 DEG. 10 MIN. E. 758 FEET; N. 53 DEG. 50 MIN. E. 546.3 FEET TO A STAKE IN FAISON'S LINE ON SOUTH SIDE OF SAID ROAD; THENCE ALONG FAISON'S LINE S. 4 DEG. 40 MIN. W. 1238.7 FEET TO A STAKE, A CORNER BETWEEN THE FAISON AND FERRELL LANDS; THENCE ALONG THE FERRELL LINE S. 1 DEG. 45 MIN. W. 1295 FEET TO THE BEGINNING, CONTAINING 82 ACRES MORE OR LESS ACCORDING TO A SURVEY AND MAP MADE BY PITTMAN STELL, C. S. DATED DEC. 14, 1944, AND BEING THE NORTHERN END OF TRACT NO. 13 OF "THE OAKS FARM", SEE SURVEY AND MAP MADE G. SAM ROWE, C. E. DATED NOVEMBER 1944.

LESS AND EXCEPT THAT PROPERTY DESCRIBED IN THE CONSENT JUDGMENT WITH THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION OF RECORD IN BOOK 9465, PAGE 594, WAKE COUNTY REGISTRY.

COMMITMENT NO.: 167-223103159C

SCHEDULE B, PART I
EXCEPTIONS

THIS COMMITMENT DOES NOT REPUBLISH ANY COVENANT, CONDITION, RESTRICTION, OR LIMITATION CONTAINED IN ANY DOCUMENT REFERRED TO IN THIS COMMITMENT TO THE EXTENT THAT THE SPECIFIC COVENANT, CONDITION, RESTRICTION, OR LIMITATION VIOLATES STATE OR FEDERAL LAW BASED ON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, GENDER IDENTITY, HANDICAP, FAMILIAL STATUS, OR NATIONAL ORIGIN.

THE POLICY WILL NOT INSURE AGAINST LOSS OR DAMAGE RESULTING FROM THE TERMS AND PROVISIONS OF ANY LEASE OR EASEMENT IDENTIFIED IN SCHEDULE A, AND WILL INCLUDE THE FOLLOWING EXCEPTIONS UNLESS CLEARED TO THE SATISFACTION OF THE COMPANY:

- ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHED, OR IS DISCLOSED BETWEEN THE COMMITMENT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
(NO MATTER OF SURVEY)
- TAXES AND ASSESSMENTS FOR THE YEAR 2023, A LIEN NOW DUE AND PAYABLE AND SUBSEQUENT YEARS, A LIEN NOT YET DUE AND PAYABLE.
(NO MATTER OF SURVEY)
- TITLE TO ANY AREAS WITHIN STREET, HIGHWAY OR RAILROAD RIGHTS OF WAY, IF ANY.
(NO MATTER OF SURVEY)
- THE CREATION OR LOSS OF LAND BY NATURAL OR ARTIFICIAL CHANGES ALONG WATER FORMING PART OF THE BOUNDARY OF THE LAND; AND/OR TITLE TO LAND LYING BELOW THE HIGHER OF THE MEAN HIGH WATER MARK AND/OR THE NORMAL BOUNDS OF ANY BODY OF WATER; AND/OR RIPARIAN RIGHTS INCIDENT TO ANY BRANCHES, CREEKS, STREAMS, LAKES OR OTHER WATERS COURSING OR ABUTTING THE LAND.
(NO MATTER OF SURVEY)
- ANY DISCREPANCY, CONFLICT, ACCESS, SHORTAGE IN AREA OR BOUNDARY LINES, ENCROACHMENT, ENCUMBRANCE, VIOLATION, OVERLAP, SETBACK, EASEMENT OR CLAIMS OF EASEMENT, RIPARIAN RIGHT, AND TITLE TO LAND WITHIN ROADS, WAYS, RAILROADS, WATERCOURSES, BURIAL GROUNDS, MARSHES, DREGGED OR FILLED AREAS OR LAND BELOW THE MEAN HIGH WATER MARK OR WITHIN THE BOUNDS OF ANY ADJOINING BODY OF WATER, OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE THAT WOULD BE DISCLOSED BY A CURRENT INSPECTION AND ACCURATE AND COMPLETE LAND SURVEY OF THE LAND.
(AS SHOWN HEREON)
- ALL DEFERRED TAXES
(NO MATTER OF SURVEY)
- EASEMENTS TO CAROLINA POWER & LIGHT RECORDED IN BOOK 1828, PAGE 340; BOOK 1943, PAGE 344; AND BOOK 4023, PAGE 701, WAKE COUNTY REGISTRY.
(AS SHOWN HEREON)
- MEMORANDUM OF CONTRACT DATED JULY 14, 2022, RECORDED IN BOOK 19098, PAGE 869, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
- THE CORRECTNESS OF THE SQUARE FOOTAGE/ACREAGE COMPUTATION CONTAINED IN THE DESCRIPTION OF THE LAND IS NOT INSURED.
(NO MATTER OF SURVEY)
- CONSENT JUDGMENT IN FAVOR OF THE DEPARTMENT OF TRANSPORTATION FOR 1.193 ACRES, WAKE COUNTY CIVIL ACTION 01-CVS-3306, RECORDED IN BOOK 9465, PAGE 594, WAKE COUNTY REGISTRY.
(AS SHOWN HEREON)
- UTILITY EASEMENTS AFFECTING THE LAND.
(NO DOCUMENTS CITED)
- ACCESS TO SECOND TRACT THE LAND IS AVAILABLE ONLY BY MEANS OF ACCESS OVER FIRST TRACT, AND IS CONDITIONED UPON OWNERSHIP OF SAID ADJOINING PARCEL BY THE INSURED.
(NO MATTER OF SURVEY)
- ACCESS BY WAY OF INTERSTATE 87, A CONTROLLED ACCESS HIGHWAY, IS NOT INSURED.
(NO MATTER OF SURVEY)
- NO CLOSING SERVICES INSURANCE: AS TO THE TRANSACTION FOR WHICH THIS COMMITMENT IS ISSUED, THE COMPANY DOES NOT AFFORD INSURED CLOSING PROTECTION/CLOSING SERVICES INSURANCE ABSENT REVISION OF THIS COMMITMENT TO INCLUDE A STATEMENT THAT COVERAGE IS AFFORDED TO THOSE PARTIES IDENTIFIED IN A SEPARATE CLOSING PROTECTION LETTER ISSUED SIMULTANEOUSLY WITH REVISION OF THE COMMITMENT. ANY CLOSING PROTECTION COVERAGE OR LETTER WHICH HERETOFORE MAY HAVE BEEN ISSUED IS HEREBY RESCINDED AND SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION. ANY CLOSING PROTECTION LETTER SUBSEQUENTLY ISSUED SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION ABSENT THE ACCOMPANYING REQUISITE REVISION OF THIS COMMITMENT.
(NO MATTER OF SURVEY)

BATEMAN CIVIL SURVEY COMPANY
ENGINEERS • SURVEYORS • PLANNERS
2524 RELIANCE AVENUE - APEX, NC 27539
PHONE: (919) 577-1080 FAX: (919) 577-1081
INFO@BATEMANCIVILSURVEY.COM
NCBELS FIRM# C-2378



ALTANSP/LAND TITLE SURVEY
EXCLUSIVELY FOR: DR HORTON
PROPERTY PINS: 1753162116; 1743953663; 1743971085; 1743986356; 1743986384;
1750071983; 1743978575 & 1743779851
AS RECORDED IN: DB 15657 PG 540, DB 2253 PG 285, DB 15474 PG 755, BM 2000,
PG 1887, DB 9018 PG 621, DB 6075 PG 504 & DB 7755 PG 552
ST MATTHEWS TOWNSHIP - WAKE COUNTY - NORTH CAROLINA

REVISIONS	
1.	
2.	
3.	
4.	
5.	

DESIGNED BY: N/A
DRAWN BY: ELS
CHECKED BY: SPC
SCALE: 1" = 100'
DATE: 1/27/2023
JOB NUMBER: 220655
SHEET 3 OF 4

SURVEY COMMENTS
SURVEY MADE BY BATEMAN CIVIL SURVEY COMPANY
DATED 12/12/2022, JOB NO. 220655

SCHEDULE A

ISSUING OFFICE: DHI TITLE OF NORTH CAROLINA, 12276 SAN JOSE BLVD, SUITE 739, JACKSONVILLE, FL 32223
COMMITMENT NO.: 169-223103040C
PROPERTY ADDRESS: LOT ACQUISITION TRACT 12, N/A, NC 00000

1. COMMITMENT DATE: OCTOBER 28, 2022 AT 12:00 AM
2. POLICY TO BE ISSUED:
(A) ALTA OWNER'S POLICY (06-16-07)
PROPOSED INSURED: D.R. HORTON, INC., A DELAWARE CORPORATION
PROPOSED POLICY AMOUNT: \$10,000.00
3. THE ESTATE OR INTEREST IN THE LAND DESCRIBED OR REFERRED TO IN THIS COMMITMENT IS:
FEE SIMPLE
4. THE TITLE IS, AT THE COMMITMENT DATE, VESTED IN:
WAYNE L. HARPER AND EUGENE J. HARPER
5. THE LAND IS DESCRIBED AS FOLLOWS:

ALL THAT CERTAIN TRACT OR PARCEL OF LAND LYING IN SAINT MATTHEWS TOWNSHIP, WAKE COUNTY, NORTH CAROLINA, CONTAINING 61.35 ACRES MORE OR LESS, AND BEING MORE PARTICULARLY SHOWN ON THAT CERTAIN PLAT OF SURVEY TITLED, "SURVEY OF TRACT #12 OF THE OAKS FARM IN THE D. J. ROBERTSON ESTATE", BY TALLEY AND ASSOCIATES, WHICH PLAT IS RECORDED IN BOOK OF MAPS 1992, PAGE 631, WAKE COUNTY REGISTRY, AND IS BY REFERENCE, INCORPORATED HEREIN AS A PART OF THIS DESCRIPTION.

COMMITMENT NO.: 169-223103040C

**SCHEDULE B, PART II
EXCEPTIONS**

THIS COMMITMENT DOES NOT REPUBLISH ANY COVENANT, CONDITION, RESTRICTION, OR LIMITATION CONTAINED IN ANY DOCUMENT REFERRED TO IN THIS COMMITMENT TO THE EXTENT THAT THE SPECIFIC COVENANT, CONDITION, RESTRICTION, OR LIMITATION VIOLATES STATE OR FEDERAL LAW BASED ON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, GENDER IDENTITY, HANDICAP, FAMILIAL STATUS, OR NATIONAL ORIGIN.

THE POLICY WILL NOT INSURE AGAINST LOSS OR DAMAGE RESULTING FROM THE TERMS AND PROVISIONS OF ANY LEASE OR EASEMENT IDENTIFIED IN SCHEDULE A, AND WILL INCLUDE THE FOLLOWING EXCEPTIONS UNLESS CLEARED TO THE SATISFACTION OF THE COMPANY:

1. ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE COMMITMENT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
(NO MATTER OF SURVEY)
2. TAXES AND ASSESSMENTS FOR THE YEAR 2023, A LIEN NOW DUE AND PAYABLE AND SUBSEQUENT YEARS, A LIEN NOT YET DUE AND PAYABLE.
(NO MATTER OF SURVEY)
3. TITLE TO ANY AREAS WITHIN STREET, HIGHWAY OR RAILROAD RIGHTS OF WAY, IF ANY.
(NO MATTER OF SURVEY)
4. THE CREATION OR LOSS OF LAND BY NATURAL OR ARTIFICIAL CHANGES ALONG WATER FORMING PART OF THE BOUNDARY OF THE LAND; AND/OR TITLE TO LAND LYING BELOW THE HIGHER OF THE MEAN HIGH WATER MARK AND/OR THE NORMAL BOUNDS OF ANY BODY OF WATER; AND/OR RIPARIAN RIGHTS INCIDENT TO ANY BRANCHES, CREEKS, STREAMS, LAKES OR OTHER WATERS COURSING OR ABUTTING THE LAND.
(NO MATTER OF SURVEY)
5. ANY DISCREPANCY, CONFLICT, ACCESS, SHORTAGE IN AREA OR BOUNDARY LINES, ENCROACHMENT, ENCUMBRANCE, VIOLATION, OVERLAP, SETBACK, EASEMENT OR CLAIMS OF EASEMENT, RIPARIAN RIGHT, AND TITLE TO LAND WITHIN ROADS, WAYS, RAILROADS, WATERCOURSES, BURIAL GROUNDS, MARSHES, DREDGED OR FILLED AREAS OR LAND BELOW THE MEAN HIGH WATER MARK OR WITHIN THE BOUNDS OF ANY ADJOINING BODY OF WATER, OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE THAT WOULD BE DISCLOSED BY A CURRENT INSPECTION AND ACCURATE AND COMPLETE LAND SURVEY OF THE LAND.
(AS SHOWN HEREON)
6. ALL DEFERRED TAXES.
(NO MATTER OF SURVEY)
7. ANY RIGHT, EASEMENT, SETBACK, INTEREST, CLAIM, ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATIONS OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE DISCLOSED BY PLAT(S) RECORDED IN BOOK OF MAPS 1992, AT PAGE 631 AND MATTERS SHOWN THEREON.
(AS SHOWN HEREON)
8. RIGHT OF WAY TO CAROLINA POWER AND LIGHT OF RECORD IN BOOK 1943, PAGE 341, BOOK 4945, PAGE 929, WAKE COUNTY REGISTRY.
(AS SHOWN HEREON)
9. RIGHT OF WAY TO THE NC STATE HIGHWAY COMMISSION OF RECORD IN BOOK 1445, PAGE 577, WAKE COUNTY REGISTRY.
(NO PLOTTABLE DESCRIPTION FOUND)
10. ANY RIGHT, EASEMENT, SETBACK, INTEREST, CLAIM, ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATIONS OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE DISCLOSED BY PLAT(S) RECORDED IN BOOK OF MAPS 1996, AT PAGE 1615 AND MATTERS SHOWN THEREON.
(AS SHOWN HEREON)
11. SUBJECT TO THE CLAIMS OF L&L TREE AND TURF FARMS, LLC BASED UPON THE DEED INTO L&L TREE AND TURF FARMS, LLC IN BOOK 15882, PAGE 1989 (TRACT 3).
(AS SHOWN HEREON)
12. THE CORRECTNESS OF THE SQUARE FOOTAGE/ACREAGE COMPUTATION CONTAINED IN THE DESCRIPTION OF THE LAND IS NOT INSURED.
(NO MATTER OF SURVEY)
13. ANY RIGHT, EASEMENT, SETBACK, INTEREST, CLAIM, ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATIONS OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE DISCLOSED BY PLAT(S) RECORDED IN BOOK OF MAPS _____, AT PAGE _____ AND MATTERS SHOWN THEREON.
(NO DOCUMENT CITED)
14. NO CLOSING SERVICES INSURANCE: AS TO THE TRANSACTION FOR WHICH THIS COMMITMENT IS ISSUED, THE COMPANY DOES NOT AFFORD INSURED CLOSING PROTECTION/CLOSING SERVICES INSURANCE ABSENT REVISION OF THIS COMMITMENT TO INCLUDE A STATEMENT THAT COVERAGE IS AFFORDED TO THOSE PARTIES IDENTIFIED IN A SEPARATE CLOSING PROTECTION LETTER ISSUED SIMULTANEOUSLY WITH REVISION OF THE COMMITMENT. ANY CLOSING PROTECTION COVERAGE OR LETTER WHICH HERETOFORE MAY HAVE BEEN ISSUED IS HEREBY RESCINDED AND SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION. ANY CLOSING PROTECTION LETTER SUBSEQUENTLY ISSUED SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION ABSENT THE ACCOMPANYING REQUISITE REVISION OF THIS COMMITMENT.
(NO MATTER OF SURVEY)

THIS MAP IS CONSIDERED PRELIMINARY, NOT FOR RECORDATION, CONVEYANCE OR SALES UNLESS SIGNED AND SEALED BY THE LICENSED SURVEYOR.

SURVEY COMMENTS
SURVEY MADE BY BATEMAN CIVIL SURVEY COMPANY
DATED 12/12/2022, JOB NO. 220655

SCHEDULE A

ISSUING OFFICE: DHI TITLE OF NORTH CAROLINA, 12276 SAN JOSE BLVD, SUITE 739, JACKSONVILLE, FL 32223
COMMITMENT NO.: 167-223103038C
PROPERTY ADDRESS: FOUR TRACTS IN WAKE COUNTY TO BE KNOWN AS TART, N/A, NC 00000

1. COMMITMENT DATE: OCTOBER 27, 2022 AT 12:00 AM
2. POLICY TO BE ISSUED:
(A) ALTA OWNER'S POLICY (06-16-07)
PROPOSED INSURED: D.R. HORTON, INC., A DELAWARE CORPORATION
PROPOSED POLICY AMOUNT: \$1,800,000.00
3. THE ESTATE OR INTEREST IN THE LAND DESCRIBED OR REFERRED TO IN THIS COMMITMENT IS:
FEE SIMPLE
4. THE TITLE IS, AT THE COMMITMENT DATE, VESTED IN:
TRACT 1
DEBORAH JANE TART AND MILTON E. TART, JR. AS TENANTS IN COMMON

TRACT 2
MILTON EDWARD TART, III AND WIFE, JANET STANLEY TART

TRACT 3
MILTON EDWARD TART, JR. AND WIFE, PATSY PRICE TART

TRACT 4
LEE T. ALFORD
5. THE LAND IS DESCRIBED AS FOLLOWS:

TRACT 1
PARCEL 1
ALL THAT CERTAIN TRACT OR PARCEL OF LAND LYING IN SAINT MATTHEWS TOWNSHIP, WAKE COUNTY, NORTH CAROLINA, CONTAINING 2.694 ACRES MORE OR LESS, AND BEING MORE PARTICULARLY SHOWN AS LOT 2 ON THAT CERTAIN PLAT OF SURVEY ENTITLED, "FINAL PLAT MINOR FAMILY SUBDIVISION GLADYS F. TART", BY GIL CLARK SURVEYING, WHICH PLAT IS RECORDED IN BOOK OF MAPS 2003, PAGE 194, WAKE COUNTY REGISTRY, AND IS BY REFERENCE, INCORPORATED HEREIN AS A PART OF THIS DESCRIPTION.

PARCEL 2
ALL THAT CERTAIN TRACT OR PARCEL OF LAND LYING IN SAINT MATTHEWS TOWNSHIP, WAKE COUNTY, NORTH CAROLINA, CONTAINING 3.896 ACRES MORE OR LESS, AND BEING MORE PARTICULARLY SHOWN AS LOT 5 ON THAT CERTAIN PLAT OF SURVEY BY GIL CLARK SURVEYING, WHICH PLAT IS RECORDED IN BOOK OF MAPS 2003, PAGE 1494, WAKE COUNTY REGISTRY, AND IS BY REFERENCE, INCORPORATED HEREIN AS A PART OF THIS DESCRIPTION.

PARCEL 3
ALL THAT CERTAIN TRACT OR PARCEL OF LAND LYING IN SAINT MATTHEWS TOWNSHIP, WAKE COUNTY, NORTH CAROLINA, CONTAINING 2.166 ACRES MORE OR LESS, AND BEING MORE PARTICULARLY SHOWN AS LOT 3 ON THAT CERTAIN PLAT OF SURVEY BY GIL CLARK SURVEYING, WHICH PLAT IS RECORDED IN BOOK OF MAPS 2000, PAGE 2138, WAKE COUNTY REGISTRY, AND IS BY REFERENCE, INCORPORATED HEREIN AS A PART OF THIS DESCRIPTION.

PARCEL 4
ALL THAT CERTAIN TRACT OR PARCEL OF LAND LYING IN SAINT MATTHEWS TOWNSHIP, WAKE COUNTY, NORTH CAROLINA, SHOWN AS "TART FARM RD NEW 6 PRIVATE RW" ON THAT CERTAIN PLAT OF SURVEY BY GIL CLARK SURVEYING, WHICH PLAT IS RECORDED IN BOOK OF MAPS 2000, PAGE 1877, WAKE COUNTY REGISTRY, AND IS BY REFERENCE, INCORPORATED HEREIN AS A PART OF THIS DESCRIPTION.

TRACT 2
ALL THAT CERTAIN TRACT OR PARCEL OF LAND LYING IN SAINT MATTHEWS TOWNSHIP, WAKE COUNTY, NORTH CAROLINA, CONTAINING 1.000 ACRE MORE OR LESS, AND BEING MORE PARTICULARLY SHOWN AS LOT 1 ON THAT CERTAIN PLAT OF SURVEY BY GIL CLARK SURVEYING, WHICH PLAT IS RECORDED IN BOOK OF MAPS 2000, PAGE 1877, WAKE COUNTY REGISTRY, AND IS BY REFERENCE, INCORPORATED HEREIN AS A PART OF THIS DESCRIPTION.

TRACT 3
BEGINNING AT A NAIL AND CAP IN THE CENTER OF FAISON ROAD (STATE ROAD 2515) AND THE NORTHEASTERN CORNER OF THE PROPERTY OF JAMES R. FAISON, SOUTHERN LINE OF THE PROPERTY HERETOFORE BELONGING TO OLLIE W. FAISON AND WIFE DAISY P. FAISON AND FROM THE POINT AND PLACE OF BEGINNING SOUTH 00 DEGREES 19 MINUTES WEST 230.74 FEET TO A STAKE IN THE EASTERN LINE OF THE PROPERTY OF JAMES R. FAISON AND THE WESTERN LINE OF GLADYS FAISON TART; THENCE SOUTH 83 DEGREES 4 MINUTES EAST 200 FEET TO A STAKE; THENCE NORTH 00 DEGREES 19 MINUTES EAST 230.74 FEET TO A NAIL AND CAP LOCATES IN THE CENTER OF FAISON ROAD (STATE ROAD 2515); THENCE WITH THE CENTER OF SAID ROAD NORTH 83 DEGREES 4 MINUTES WEST 200 FEET TO THE POINT AND PLACE OF BEGINNING CONTAINING 1.05 ACRES MORE OR LESS INCLUSIVE OF THE RIGHT OF WAY OF FAISON ROAD (STATE ROAD 2515).

TRACT 4
ALL THAT CERTAIN TRACT OR PARCEL OF LAND LYING IN SAINT MATTHEWS TOWNSHIP, WAKE COUNTY, NORTH CAROLINA, CONTAINING 1.079 ACRES MORE OR LESS, AND BEING MORE PARTICULARLY SHOWN AS LOT 4 ON THAT CERTAIN PLAT OF SURVEY BY GIL CLARK SURVEYING, WHICH PLAT IS RECORDED IN BOOK OF MAPS 2006, PAGE 2138, WAKE COUNTY REGISTRY, AND IS BY REFERENCE, INCORPORATED HEREIN AS A PART OF THIS DESCRIPTION.

COMMITMENT NO.: 167-223103038C

**SCHEDULE B, PART II
EXCEPTIONS**

THIS COMMITMENT DOES NOT REPUBLISH ANY COVENANT, CONDITION, RESTRICTION, OR LIMITATION CONTAINED IN ANY DOCUMENT REFERRED TO IN THIS COMMITMENT TO THE EXTENT THAT THE SPECIFIC COVENANT, CONDITION, RESTRICTION, OR LIMITATION VIOLATES STATE OR FEDERAL LAW BASED ON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, GENDER IDENTITY, HANDICAP, FAMILIAL STATUS, OR NATIONAL ORIGIN.

THE POLICY WILL NOT INSURE AGAINST LOSS OR DAMAGE RESULTING FROM THE TERMS AND PROVISIONS OF ANY LEASE OR EASEMENT IDENTIFIED IN SCHEDULE A, AND WILL INCLUDE THE FOLLOWING EXCEPTIONS UNLESS CLEARED TO THE SATISFACTION OF THE COMPANY:

1. ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE COMMITMENT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
(NO MATTER OF SURVEY)
2. TAXES AND ASSESSMENTS FOR THE YEAR 2023, A LIEN NOW DUE AND PAYABLE AND SUBSEQUENT YEARS, A LIEN NOT YET DUE AND PAYABLE.
(NO MATTER OF SURVEY)
3. TITLE TO ANY AREAS WITHIN STREET, HIGHWAY OR RAILROAD RIGHTS OF WAY, IF ANY.
(NO MATTER OF SURVEY)
4. THE CREATION OR LOSS OF LAND BY NATURAL OR ARTIFICIAL CHANGES ALONG WATER FORMING PART OF THE BOUNDARY OF THE LAND; AND/OR TITLE TO LAND LYING BELOW THE HIGHER OF THE MEAN HIGH WATER MARK AND/OR THE NORMAL BOUNDS OF ANY BODY OF WATER; AND/OR RIPARIAN RIGHTS INCIDENT TO ANY BRANCHES, CREEKS, STREAMS, LAKES OR OTHER WATERS COURSING OR ABUTTING THE LAND.
(NO MATTER OF SURVEY)
5. ANY DISCREPANCY, CONFLICT, ACCESS, SHORTAGE IN AREA OR BOUNDARY LINES, ENCROACHMENT, ENCUMBRANCE, VIOLATION, OVERLAP, SETBACK, EASEMENT OR CLAIMS OF EASEMENT, RIPARIAN RIGHT, AND TITLE TO LAND WITHIN ROADS, WAYS, RAILROADS, WATERCOURSES, BURIAL GROUNDS, MARSHES, DREDGED OR FILLED AREAS OR LAND BELOW THE MEAN HIGH WATER MARK OR WITHIN THE BOUNDS OF ANY ADJOINING BODY OF WATER, OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE THAT WOULD BE DISCLOSED BY A CURRENT INSPECTION AND ACCURATE AND COMPLETE LAND SURVEY OF THE LAND.
(AS SHOWN HEREON)
6. ALL DEFERRED TAXES.
(NO MATTER OF SURVEY)
7. TRACT 1:
A. ANY RIGHT, EASEMENT, SETBACK, INTEREST, CLAIM, ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATIONS OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE DISCLOSED BY PLAT(S) RECORDED IN BOOK MAP 2003, AT PAGE 1494 AND BOOK MAP 2005, PAGE 2138 AND MATTERS SHOWN THEREON.
(AS SHOWN HEREON)
8. TRACTS 1 AND 2:
A. EASEMENTS TO CAROLINA POWER & LIGHT COMPANY OF RECORD IN BOOK 1946, PAGE 168; BOOK 4327, PAGE 198; AND BOOK 9483, PAGE 792, WAKE COUNTY REGISTRY.
(LOCATION CANNOT BE DETERMINED FROM RECORD DOCUMENTS)
9. ORDER GRANTING A VARIANCE BY THE KNIGHTDALE BOARD OF ADJUSTMENT, DATED JULY 19, 2000, OF RECORD IN BOOK 8647, PAGE 1755, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
10. ROAD MAINTENANCE AGREEMENT FOR TART FARM ROAD BETWEEN GLADYS F. TART, MILTON EDWARD TART, JR., AND MILTON EDWARD TART, III, OF RECORD IN BOOK 8892, PAGE 161, AND RE-RECORDED IN BOOK 8710, PAGE 2779, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
11. MEMORANDUM OF CONTRACT WITH D.R. HORTON, INC. OF RECORD IN BOOK 19098, PAGE 791, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
12. TRACT 2:
A. ANY RIGHT, EASEMENT, SETBACK, INTEREST, CLAIM, ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATIONS OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE DISCLOSED BY PLAT(S) RECORDED IN BOOK MAP 2000, AT PAGE 1877 AND MATTERS SHOWN THEREON.
(AS SHOWN HEREON)
13. TRACTS 3 AND 4:
A. RIGHT OF WAY AGREEMENT BETWEEN MILTON E. TART, GLADYS F. TART, AND THE STATE HIGHWAY COMMISSION FOR SR# 2515, RECORDED IN BOOK 1445, PAGE 579, WAKE COUNTY REGISTRY.
(NO PLOTTABLE DESCRIPTION FOUND)
14. EASEMENTS TO CAROLINA POWER AND LIGHT RECORDED IN BOOK 1946, PAGE 168; BOOK 4327, PAGE 199; AND BOOK 9483, PAGE 790, WAKE COUNTY REGISTRY.
(LOCATION CANNOT BE DETERMINED FROM RECORD DOCUMENTS)
15. ROAD MAINTENANCE AGREEMENT FOR TART FARM ROAD BETWEEN GLADYS F. TART, MILTON EDWARD TART, JR., AND MILTON EDWARD TART, III, OF RECORD IN BOOK 8892, PAGE 161, AND RE-RECORDED IN BOOK 8710, PAGE 2779, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
16. MEMORANDUM OF CONTRACT WITH D.R. HORTON, INC., DATED JULY 13, 2022, OF RECORD IN BOOK 19098, PAGE 791, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
17. ORDER GRANTING A VARIANCE BY THE KNIGHTDALE BOARD OF ADJUSTMENT, DATED JULY 19, 2000, OF RECORD IN BOOK 8647, PAGE 1755, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
18. TRACT 4:
A. ANY RIGHT, EASEMENT, SETBACK, INTEREST, CLAIM, ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATIONS OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE DISCLOSED BY PLAT(S) RECORDED IN BOOK MAP 2005, AT PAGE 2138 AND MATTERS SHOWN THEREON.
(AS SHOWN HEREON)
19. THE CORRECTNESS OF THE SQUARE FOOTAGE/ACREAGE COMPUTATION CONTAINED IN THE DESCRIPTION OF THE LAND IS NOT INSURED.
(NO MATTER OF SURVEY)
20. NO CLOSING SERVICES INSURANCE: AS TO THE TRANSACTION FOR WHICH THIS COMMITMENT IS ISSUED, THE COMPANY DOES NOT AFFORD INSURED CLOSING PROTECTION/CLOSING SERVICES INSURANCE ABSENT REVISION OF THIS COMMITMENT TO INCLUDE A STATEMENT THAT COVERAGE IS AFFORDED TO THOSE PARTIES IDENTIFIED IN A SEPARATE CLOSING PROTECTION LETTER ISSUED SIMULTANEOUSLY WITH REVISION OF THE COMMITMENT. ANY CLOSING PROTECTION COVERAGE OR LETTER WHICH HERETOFORE MAY HAVE BEEN ISSUED IS HEREBY RESCINDED AND SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION. ANY CLOSING PROTECTION LETTER SUBSEQUENTLY ISSUED SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION ABSENT THE ACCOMPANYING REQUISITE REVISION OF THIS COMMITMENT.
(NO MATTER OF SURVEY)

SURVEY COMMENTS
SURVEY MADE BY BATEMAN CIVIL SURVEY COMPANY
DATED 12/12/2022, JOB NO. 220655

SCHEDULE A

ISSUING OFFICE: DHI TITLE OF NORTH CAROLINA, 12276 SAN JOSE BLVD, SUITE 739, JACKSONVILLE, FL 32223
COMMITMENT NO.: 169-223103164C
PROPERTY ADDRESS: ACQUISITION TRACT 4, ED PLUMMER ESTATE PROPERTY, N/A, NC 00000

1. COMMITMENT DATE: NOVEMBER 8, 2022 AT 12:00 AM
2. POLICY TO BE ISSUED:
(A) ALTA OWNER'S POLICY (06-16-07)
PROPOSED INSURED: D.R. HORTON, INC., A DELAWARE CORPORATION
PROPOSED POLICY AMOUNT: \$1,400,000.00
3. THE ESTATE OR INTEREST IN THE LAND DESCRIBED OR REFERRED TO IN THIS COMMITMENT IS:
FEE SIMPLE
4. THE TITLE IS, AT THE COMMITMENT DATE, VESTED IN:
LANCE R. VANDYKE AND WIFE, JACQUELINE VANDYKE
5. THE LAND IS DESCRIBED AS FOLLOWS:

BEING ALL OF TRACT 4 OF THE ED PLUMMER ESTATE PROPERTY ON OLD FAISON RD. AS DEPICTED IN MAP BOOK 1996, PAGE 1615, WAKE COUNTY REGISTRY.

COMMITMENT NO.: 169-223103164C

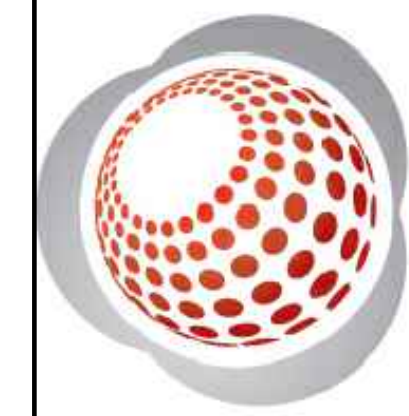
**SCHEDULE B, PART II
EXCEPTIONS**

THIS COMMITMENT DOES NOT REPUBLISH ANY COVENANT, CONDITION, RESTRICTION, OR LIMITATION CONTAINED IN ANY DOCUMENT REFERRED TO IN THIS COMMITMENT TO THE EXTENT THAT THE SPECIFIC COVENANT, CONDITION, RESTRICTION, OR LIMITATION VIOLATES STATE OR FEDERAL LAW BASED ON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, GENDER IDENTITY, HANDICAP, FAMILIAL STATUS, OR NATIONAL ORIGIN.

THE POLICY WILL NOT INSURE AGAINST LOSS OR DAMAGE RESULTING FROM THE TERMS AND PROVISIONS OF ANY LEASE OR EASEMENT IDENTIFIED IN SCHEDULE A, AND WILL INCLUDE THE FOLLOWING EXCEPTIONS UNLESS CLEARED TO THE SATISFACTION OF THE COMPANY:

1. ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE COMMITMENT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
(NO MATTER OF SURVEY)
2. TAXES AND ASSESSMENTS FOR THE YEAR 2022 AND SUBSEQUENT YEARS, NOT YET DUE AND PAYABLE.
(NO MATTER OF SURVEY)
3. TITLE TO ANY AREAS WITHIN STREET, HIGHWAY OR RAILROAD RIGHTS OF WAY, IF ANY.
(NO MATTER OF SURVEY)
4. THE CREATION OR LOSS OF LAND BY NATURAL OR ARTIFICIAL CHANGES ALONG WATER FORMING PART OF THE BOUNDARY OF THE LAND; AND/OR TITLE TO LAND LYING BELOW THE HIGHER OF THE MEAN HIGH WATER MARK AND/OR THE NORMAL BOUNDS OF ANY BODY OF WATER; AND/OR RIPARIAN RIGHTS INCIDENT TO ANY BRANCHES, CREEKS, STREAMS, LAKES OR OTHER WATERS COURSING OR ABUTTING THE LAND.
(NO MATTER OF SURVEY)
5. ANY DISCREPANCY, CONFLICT, ACCESS, SHORTAGE IN AREA OR BOUNDARY LINES, ENCROACHMENT, ENCUMBRANCE, VIOLATION, OVERLAP, SETBACK, EASEMENT OR CLAIMS OF EASEMENT, RIPARIAN RIGHT, AND TITLE TO LAND WITHIN ROADS, WAYS, RAILROADS, WATERCOURSES, BURIAL GROUNDS, MARSHES, DREDGED OR FILLED AREAS OR LAND BELOW THE MEAN HIGH WATER MARK OR WITHIN THE BOUNDS OF ANY ADJOINING BODY OF WATER, OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE THAT WOULD BE DISCLOSED BY A CURRENT INSPECTION AND ACCURATE AND COMPLETE LAND SURVEY OF THE LAND.
(AS SHOWN HEREON)
6. ANY RIGHT, EASEMENT, SETBACK, INTEREST, CLAIM, ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATIONS OR OTHER ADVERSE CIRCUMSTANCE AFFECTING THE TITLE DISCLOSED BY PLAT(S) RECORDED IN BOOK OF MAPS 1996, AT PAGE 1615, OF THE WAKE COUNTY REGISTRY.
(AS SHOWN HEREON)
7. EASEMENT FOR ROADWAY DATED 11/10/1945 IN BOOK 930, PAGE 590, WAKE COUNTY REGISTRY.
(NO PLOTTABLE DESCRIPTION FOUND)
8. RIGHT OF WAY EASEMENT DATED 4/11/1949 IN BOOK 1018, PAGE 553, WAKE COUNTY REGISTRY.
(EASEMENT HAS TERMINATED PER TERMS)
9. EASEMENT TO CAROLINA POWER AND LIGHT DATED 9/13/1956 OF RECORD IN BOOK 1252, PAGE 485, WAKE COUNTY REGISTRY.
(EASEMENT IS BLANKET IN NATURE)
10. RIGHT OF WAY AGREEMENT DATED 12/7/1960 TO THE STATE HIGHWAY COMMISSION IN BOOK 1445, PAGE 575, WAKE COUNTY REGISTRY.
(NO PLOTTABLE DESCRIPTION FOUND)
11. EASEMENT TO CAROLINA POWER AND LIGHT DATED 7/14/1970 OF RECORD IN BOOK 1939, PAGE 230, WAKE COUNTY REGISTRY.
(AS SHOWN HEREON)
12. EASEMENT TO CAROLINA POWER AND LIGHT DATED 1/13/1972 OF RECORD IN BOOK 2052, PAGE 131, WAKE COUNTY REGISTRY.
(EASEMENT IS BLANKET IN NATURE)
13. EQUITY LINE DEED OF TRUST TO COASTAL FEDERAL CREDIT UNION DATED 2/14/2022 OF RECORD IN BOOK 18924, PAGE 945, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
14. MEMORANDUM OF CONTRACT WITH DR HORTON, INC. OF RECORD IN BOOK 19098, PAGE 653, WAKE COUNTY REGISTRY.
(NO MATTER OF SURVEY)
15. SUBJECT TO THE CLAIMS OF PROPERTY OWNERS TO THE EAST BASED UPON THE MAP IN BM 1996, PAGE 1615, WHICH CREATES AN OVERLAP AND SHOWS THE ADJACENT TRACT WITH NO ACCESS AND NOT ADJACENT TO THE PUBLIC ROAD
(AS SHOWN HEREON)
16. NO CLOSING SERVICES INSURANCE: AS TO THE TRANSACTION FOR WHICH THIS COMMITMENT IS ISSUED, THE COMPANY DOES NOT AFFORD INSURED CLOSING PROTECTION/CLOSING SERVICES INSURANCE ABSENT REVISION OF THIS COMMITMENT TO INCLUDE A STATEMENT THAT COVERAGE IS AFFORDED TO THOSE PARTIES IDENTIFIED IN A SEPARATE CLOSING PROTECTION LETTER ISSUED SIMULTANEOUSLY WITH REVISION OF THE COMMITMENT. ANY CLOSING PROTECTION COVERAGE OR LETTER WHICH HERETOFORE MAY HAVE BEEN ISSUED IS HEREBY RESCINDED AND SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION. ANY CLOSING PROTECTION LETTER SUBSEQUENTLY ISSUED SHALL NOT BE EFFECTIVE AS TO THIS TRANSACTION ABSENT THE ACCOMPANYING REQUISITE REVISION OF THIS COMMITMENT.
(NO MATTER OF SURVEY)

BATEMAN CIVIL SURVEY COMPANY
ENGINEERS • SURVEYORS • PLANNERS
2524 RELIANCE AVENUE - APEX, NC 27539
PHONE: (919) 577-1080 FAX: (919) 577-1081
INFO@BATEMANCIVILSURVEY.COM
NCBELS FIRM# C-2378



ALTANSPS/LAND TITLE SURVEY
EXCLUSIVELY FOR: DR HORTON
PROPERTY PINS: 1753162116; 1743953683; 1743971085; 1743986356; 1743989384;
1753071983; 1743978575 & 1743779851
AS RECORDED IN: DB 16567 PG 540, DB 2253 PG 265, DB 15474 PG 755, BM 2000,
PG 1887, DB 9018 PG 621, DB 6075 PG 904 & DB 7755 PG 552
ST MATTHEWS TOWNSHIP - WAKE COUNTY - NORTH CAROLINA

REVISIONS	
1.	
2.	
3.	
4.	
5.	
DESIGNED BY:	N/A
DRAWN BY:	ELS
CHECKED BY:	SPC
SCALE:	VARIES
DATE:	1/27/2023
JOB NUMBER:	220655
SHEET 4 OF 4	



NOTES:
TOPOGRAPHY PER WAKE COUNTY GIS



150 Fayetteville st ste 1310
Raleigh, NC 27601
+ 919.275.5022
urbandesignpartners.com
nc firm no: P-2671 sc coo no: C-03044

FOR
REFERENCE
ONLY

05/03/2024

D.R. Horton

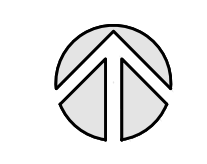
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

LYNDON OAKS
Master Plan
Existing Conditions TOPO
Knightdale, North Carolina

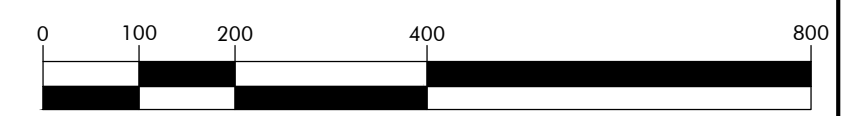
NO.	DATE	BY	REVISIONS:

Project No: 22-RDU-083
Date: 05/03/2024

Sheet No:
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GRAPHIC SCALE



(IN FEET)
1 inch = 200 ft.

NOTE: Location, shape and size of depicted features on the evaluated site are approximate and should be surveyed by a licensed NC surveyor for final site planning.

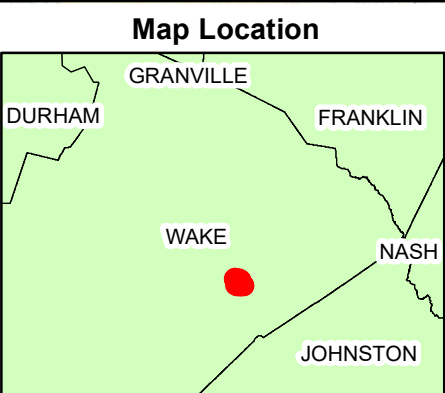
NOTE: This map is a revision of the previously submitted map based on the ite meeting with Matt Martin of the U.S. Army Corp of Engineers Raleigh Regulatory Field Office on January 25, 2024.



Legend

- Project Study Area
- 2' Contour
- A/D Boundary
- Stream Form
- Data Point
- Potential Non-Wetland - Non-Buffered Intermittent Stream - Jurisdictional Waters of the US
- Potential Non-Wetland - Perennial Stream - Jurisdictional Waters of the US; Subject to 50-Foot Neuse River Riparian Buffers
- Potential Wetlands - Jurisdictional Waters of the US
- Non-Jurisdictional Pond - Pond Built In High Ground
- Potential Non-Wetland - Pond - Jurisdictional Waters of the US; Subject to 50-Foot Neuse River Riparian Buffer

NCDOT GIS Unit, NC OneMap, NC Center for Geographic Information and Analysis, NC 911 Board



AJD/ PJD Revised Wetland Sketch Map

**Bethlehem Rd. Project
Sage Project # 2022.105**

February 6, 2024

1" = 400'

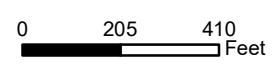


Figure 3

**Drawn By:
David Gainey**

Sage Ecological Services, Inc.
Office: 919-335-6757
Cell: 919-559-1537



150 Fayetteville St Ste 1310
Raleigh, NC 27601
919.275.5022
urbandesignpartners.com

nc firm no: P-2671 sc coa no: C-03044

PRELIMINARY DRAWING
FOR REVIEW PURPOSES ONLY

05/03/2024

D.R. Horton
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

LYNDON OAKS Master Plan Overall Site Plan

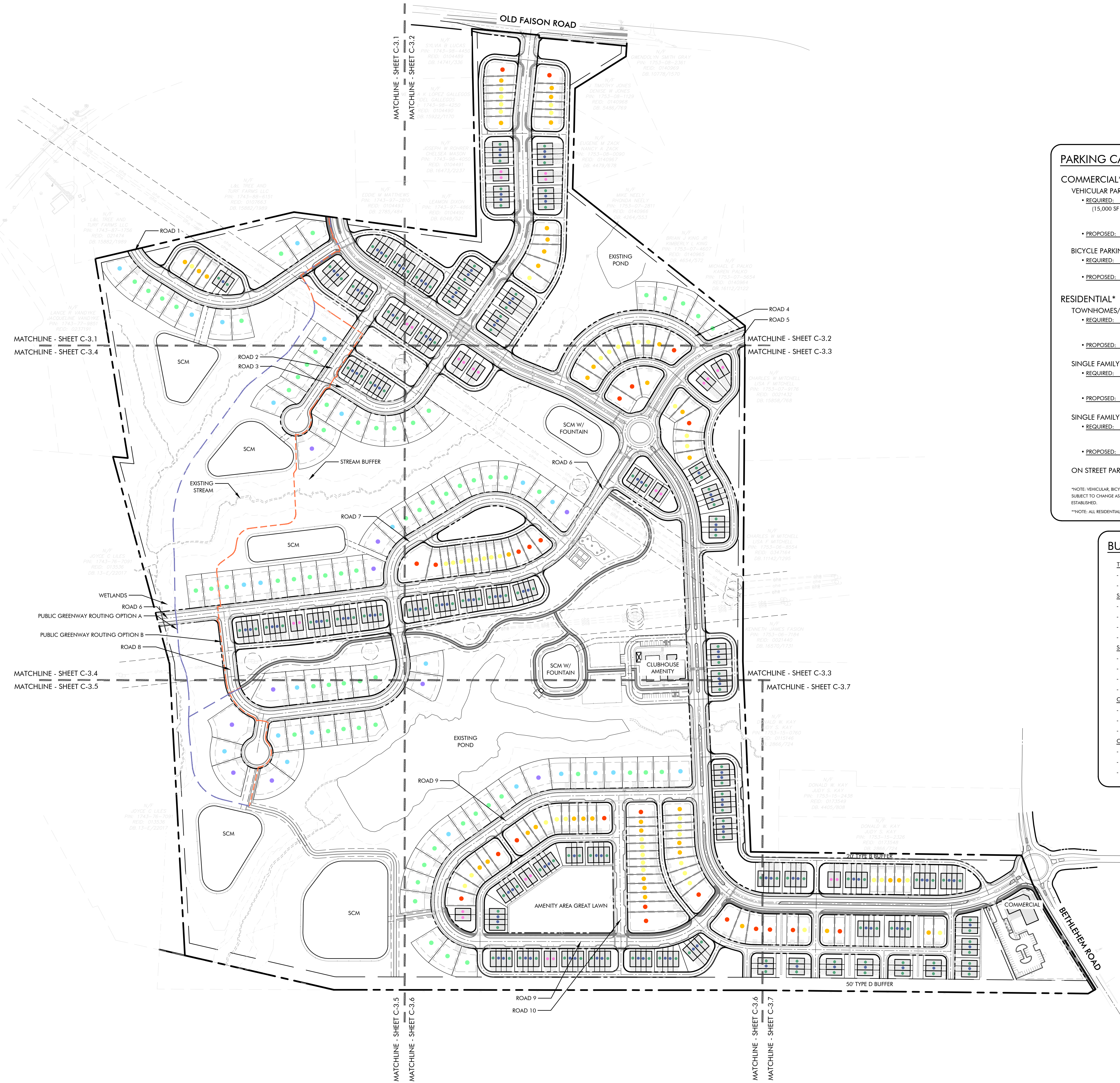
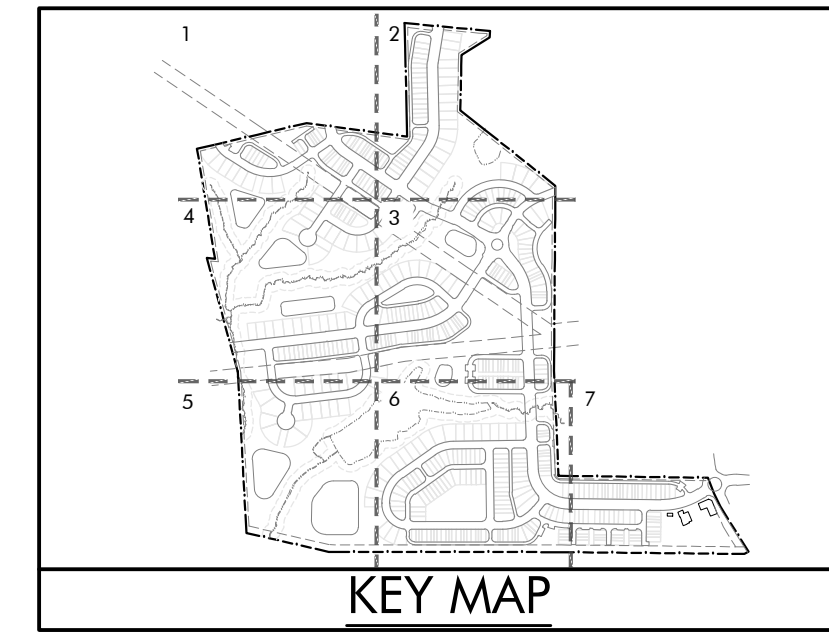
Knightsdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083
Date: 05/03/2024

Sheet No:

C-3.0



PARKING CALCS

COMMERCIAL*

VEHICULAR PARKING

- REQUIRED: 27-53 SPACES
(15,000 SF x 3.5 SP/1000 SF = 52.5 MAX
52.5/2 = 26.2 MIN)
- PROPOSED: 60 SPACES

BICYCLE PARKING

- REQUIRED: 3 SPACES
- PROPOSED: 3 SPACES

RESIDENTIAL*

TOWNHOMES/DUPLEX

- REQUIRED: 819 SP (1.0 SP/BED)
273 DU x 3 BED** = 819 SP
- PROPOSED: 819 SPACES TOTAL

SINGLE FAMILY REAR-LOADED

- REQUIRED: 327 SP (1.0 SP/BED)
109 DU x 3 BED** = 327 SP
- PROPOSED: 327 SPACES TOTAL

SINGLE FAMILY FRONT-LOADED

- REQUIRED: 300 SP (1.0 SP/BED)
100 DU x 3 BED** = 300 SP
- PROPOSED: 300 SPACES TOTAL

ON STREET PARKING: 220 SPACES PROP.

*NOTE: VEHICULAR, BICYCLE, AND ELECTRIC VEHICLE CHARGING STATION TOTALS SUBJECT TO CHANGE AS MASTER PLAN SET PROGRESSES AND AS USES/TENANTS ARE ESTABLISHED.
**NOTE: ALL RESIDENTIAL UNITS ASSUMED TO BE 3 BEDROOMS.

BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):

- FRONT: 0' MIN TO 25' MAX
- SIDE: 10' MIN

SINGLE FAMILY DETACHED (REAR-LOADED):

- FRONT: 10' MIN
- REAR: 10' MIN
- SIDE: 3' MIN
- CORNER: 10' MIN

SINGLE FAMILY DETACHED (FRONT-LOADED):

- FRONT: 10' MIN
- REAR: 20' MIN
- SIDE: 5' MIN
- CORNER: 10' MIN

COMMERCIAL:

- FRONT: 10' MIN
- REAR: 0' MIN
- SIDE: 6' MIN

CLUBHOUSE:

- FRONT: 10' MIN
- REAR: 0' MIN
- SIDE: 6' MIN

SITE DATA:

PIN(S): 1743953683; 1743971085; 1753071583;
1743976575; 1743989384; 1743986356;
1753152116

TOTAL ACRES: ± 171.88 ACRES

EXISTING ZONING: RT (RURAL TRANSITIONAL)
PROPOSED ZONING: RMX-PUD

EXISTING USE: RESIDENTIAL & AGRICULTURAL
PROPOSED USE: RESIDENTIAL & COMMERCIAL OUTPARCEL

DEVELOPMENT SUMMARY:

COMMERCIAL

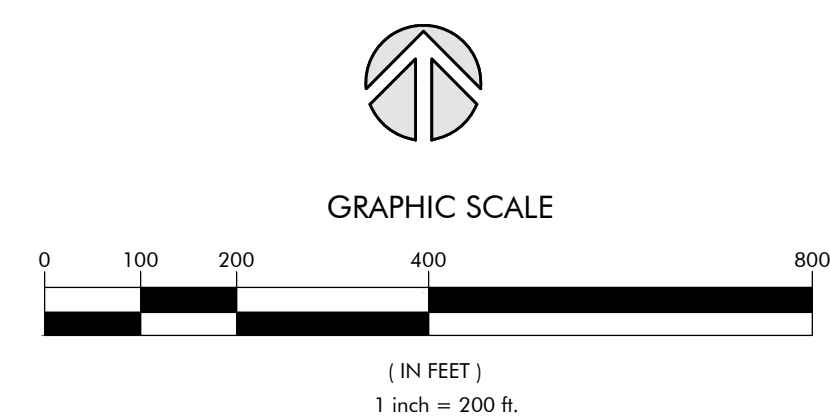
- TOTAL LAND AREA: ± 1.7 AC (77,033 SF)
- BUILDINGS: 1 BLDG
- BUILDING HEIGHT: MAX. 2 STORES
- POTENTIAL USES: USES SET FORTH IN PLANNED UNIT DEVELOPMENT (SEE ZMA-2-23)

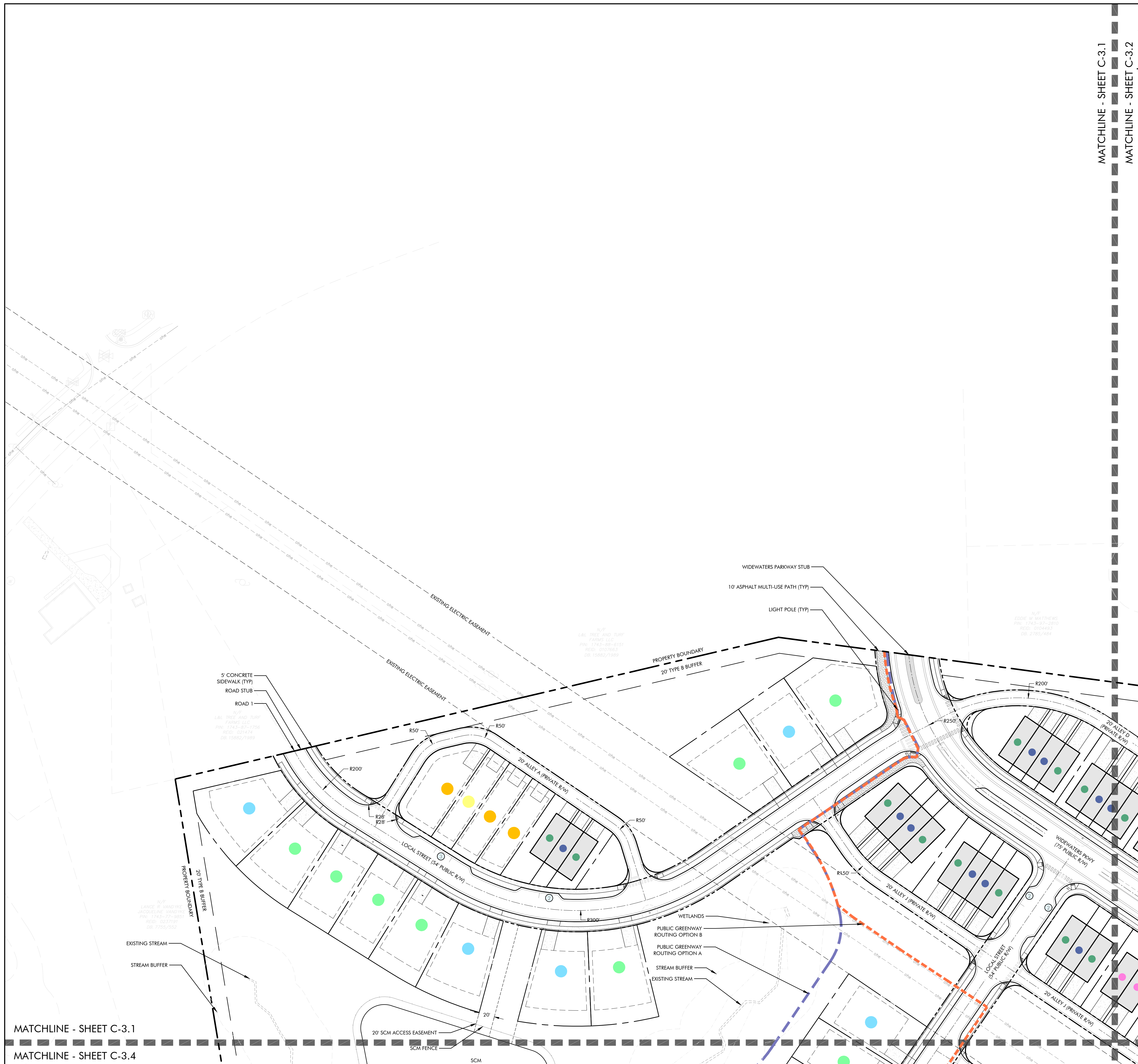
RESIDENTIAL

- TOTAL LAND AREA: ± 129.2 AC
- PROPOSED UNITS: 482 DU (UP TO 500 DU ALLOWED)
- TOWNHOMES: 247 TOWNHOMES
- 117 UNITS
- TYP LOT SIZE: 20' X 90' (1,800 SF)
- 22' (MIN.) UNIT: 130 UNITS
- TYP LOT SIZE: 22' X 90' (1,980 SF)
- DUPLEX: 26 DUPLEX UNITS
- 26 UNITS
- TYP LOT SIZE: 32' X 90' (2,880 SF)
- SINGLE FAMILY (REAR-LOAD): 109 REAR LOAD SINGLES
- 46 LOTS
- TYP LOT SIZE: 30' X 120' (3,600 SF)
- 39 LOTS
- TYP LOT SIZE: 35' X 120' (4,200 SF)
- 24 LOTS
- TYP LOT SIZE: 40' X 120' (4,800 SF)
- SINGLE FAMILY (FRONT-LOAD): 100 FRONT LOAD SINGLES
- 66 LOTS
- TYP LOT SIZE: 60' X 120' (7,200 SF)
- 24 LOTS
- TYP LOT SIZE: 70' X 120' (8,400 SF)
- 10 LOTS
- TYP LOT SIZE: 80' X 120' (9,600 SF)



BEFORE YOU DIG!
CALL 811 OR 1-800-632-4949
N.C. ONE-CALL CENTER
IT'S THE LAW!





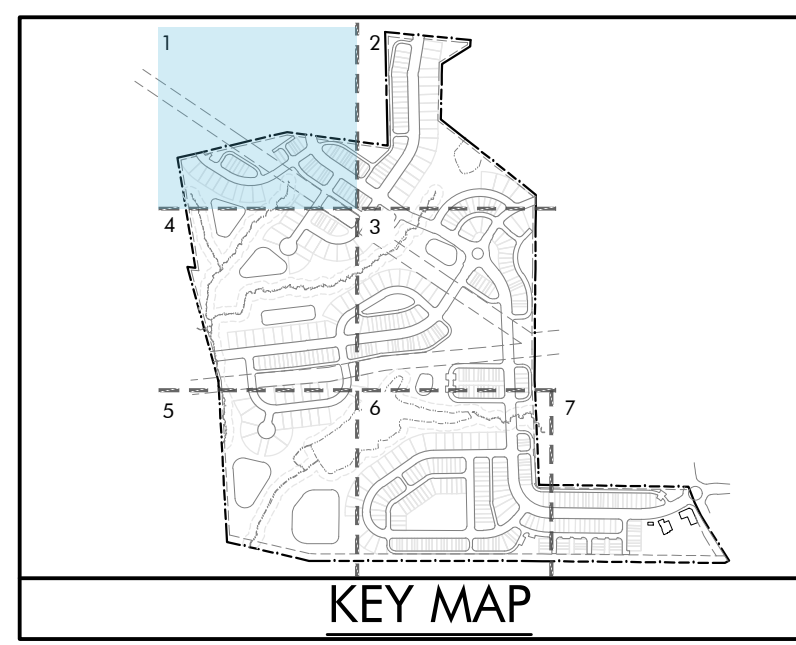
MATCHLINE - SHEET C-3.1

MATCHLINE - SHEET C-3.2

MATCHLINE - SHEET C-3.1
MATCHLINE - SHEET C-3.4

BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):	
- FRONT:	0' MIN TO 25' MAX
- SIDE:	10' MIN
SINGLE FAMILY DETACHED (REAR-LOADED):	
- FRONT:	10' MIN
- REAR:	10' MIN
- SIDE:	3' MIN
- CORNER:	10' MIN
SINGLE FAMILY DETACHED (FRONT-LOADED):	
- FRONT:	10' MIN
- REAR:	20' MIN
- SIDE:	5' MIN
- CORNER:	10' MIN



LEGEND

UNIT TYPE	DU COUNT*
TOWNHOMES	21 DU
● 20' UNIT	10 DU
● 22' UNIT	11 DU
DUPLEXES	0 DU
● 22' UNIT	0 DU
SINGLE FAM. (REAR-LOAD)	4 DU
● 30' LOT	1 DU
● 35' LOT	3 DU
● 40' LOT	0 DU
SINGLE FAM. (FRONT-LOAD)	12 DU
● 60' LOT	7 DU
● 70' LOT	5 DU
● 80' LOT	0 DU

*NOTE: UNIT COUNTS REFLECT NUMBER OF UNITS THAT FIT WITHIN MATCHLINES OF EACH SHEET. ANY UNITS SHOWN OUTSIDE OF MATCHLINES WILL BE COUNTED ON THEIR RESPECTIVE SHEETS.
TOTAL UNIT COUNT FOUND ON OVERALL SITE PLAN SHEET & COVER SHEET.

- NOTES:**
- BOUNDARY AND TOPOGRAPHIC INFORMATION PROVIDED BY: BATEMAN CIVIL SURVEY COMPANY, 2524 RELIANCE AVENUE, APEX, NC 27539, 919-577-1080
 - SEE ARCHITECTURAL PLANS FOR BUILDING FOOTPRINT DIMENSIONS. PREPARED BY: D.R. HORTON, 7208 FALLS OF NEUSE RD, SUITE 201, RALEIGH, NC 27615, 919-604-7929
 - CONTACT THE UTILITY COMPANY TO RELOCATE ANY EXISTING UTILITY POLES. ALL EXISTING FACILITIES WHICH CONFLICT WITH THE IMPROVEMENTS UNDER THE SCOPE OF THIS PROJECT MUST BE RELOCATED AT THE EXPENSE OF THE APPLICANT.
 - ALL DEVELOPMENT CREATING A TOTAL 20,000 SQUARE FEET OF IMPERVIOUS AREA SINCE SEPTEMBER 1978 WILL REQUIRE STORM DRAINAGE DETENTION.
 - DIMENSIONS SHOWN ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - LOCATIONS OF SITE LIGHTING ARE APPROXIMATE. THE FINAL NUMBER AND LOCATIONS OF LIGHTS SHALL BE DETERMINED BY THE OWNER AND DUKE ENERGY.
 - ALL UTILITIES WILL BE UNDERGROUND.
 - UNIT MIX SUBJECT TO CHANGE AS MASTER PLAN SET PROGRESSES.

150 Fayetteville St. Ste. 1310
Raleigh, NC 27601
919.275.5002
urbandesignpartners.com

nc firm no: P-2671 sc coo no: C-03044

PRELIMINARY DRAWING FOR REVIEW PURPOSES ONLY

05/03/2024

D.R. Horton

7208 Falls of Neuse Rd., Ste. 201
Raleigh, NC 27615

LYNDON OAKS

Master Plan

Site Plan Enlargement 1

Knightsdale, North Carolina

BEFORE YOU DIG!
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0 40 60 120 240
(IN FEET)
1 inch = 60 ft.

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083
Date: 05/03/2024
Sheet No:

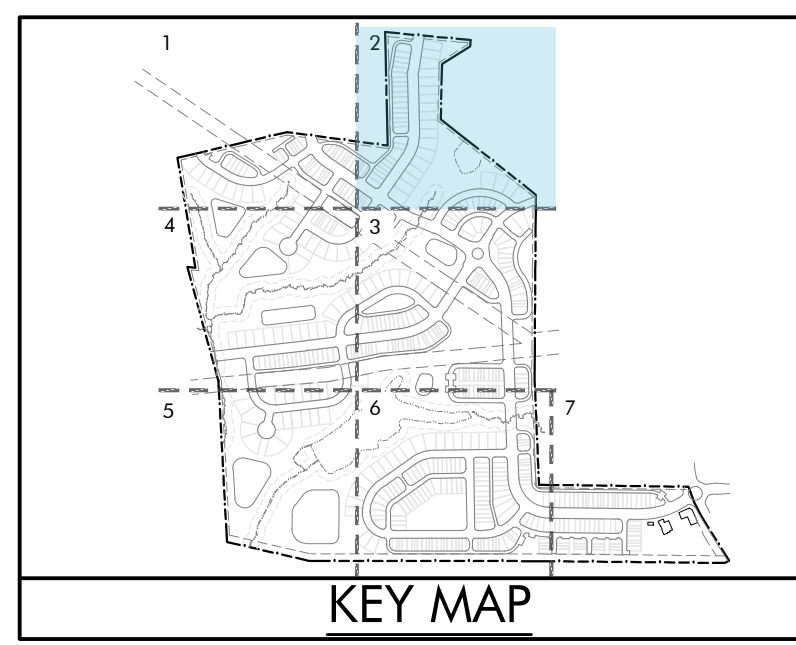
C-3.1

MATCHLINE - SHEET C-3.1
MATCHLINE - SHEET C-3.2



BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):	
- FRONT:	0' MIN TO 25' MAX
- SIDE:	10' MIN
SINGLE FAMILY DETACHED (REAR-LOADED):	
- FRONT:	10' MIN
- REAR:	10' MIN
- SIDE:	3' MIN
- CORNER:	10' MIN
SINGLE FAMILY DETACHED (FRONT-LOADED):	
- FRONT:	10' MIN
- REAR:	20' MIN
- SIDE:	5' MIN
- CORNER:	10' MIN



LEGEND

UNIT TYPE	DU COUNT*
TOWNHOMES	41 DU
20' UNIT	20 DU
22' UNIT	21 DU
DUPLEXES	6 DU
22' UNIT	6 DU
SINGLE FAM. (REAR-LOAD)	22 DU
30' LOT	10 DU
35' LOT	10 DU
40' LOT	2 DU
SINGLE FAM. (FRONT-LOAD)	5 DU
60' LOT	5 DU
70' LOT	0 DU
80' LOT	0 DU

*NOTE: UNIT COUNTS REFLECT NUMBER OF UNITS THAT FIT WITHIN MATCHLINES OF EACH SHEET. ANY UNITS SHOWN OUTSIDE OF MATCHLINES WILL BE COUNTED ON THEIR RESPECTIVE SHEETS.
TOTAL UNIT COUNT FOUND ON OVERALL SITE PLAN SHEET & COVER SHEET.

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raleigh, nc 27601
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nc firm no: P-2671 sc coo no: C-03044

PRELIMINARY DRAWING
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05/03/2024

D.R. Horton
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

LYNDON OAKS

Master Plan

Site Plan Enlargement 2

Knightsdale, North Carolina

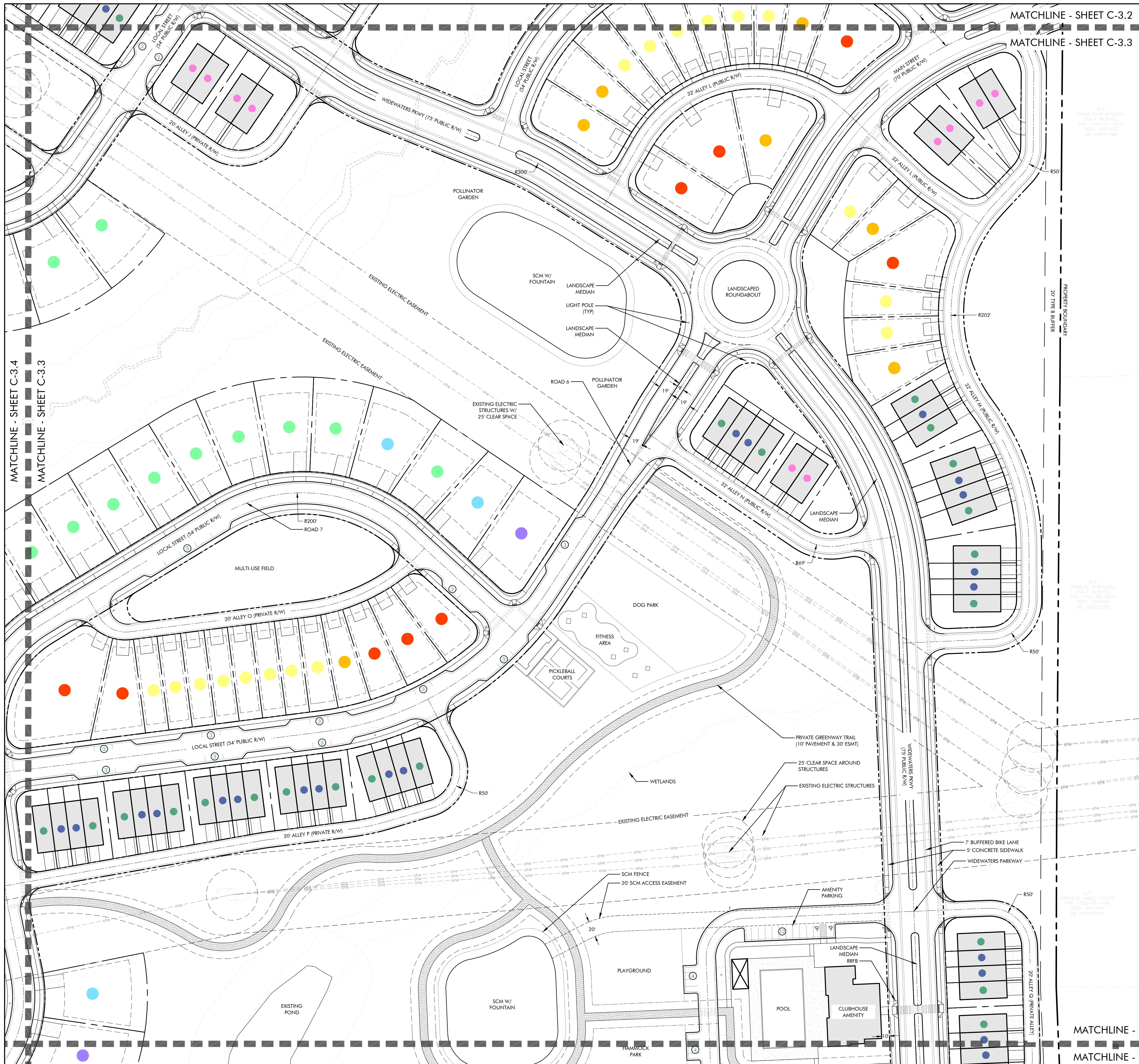
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GRAPHIC SCALE
0 40 60 120 240
(IN FEET)
1 inch = 60 ft.

NO.	DATE	BY	REVISIONS:
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#3	03/28/2024		

Project No: 22-RDU-083
Date: 05/03/2024
Sheet No:

C-3.2



MATCHLINE - SHEET C-3.2

MATCHLINE - SHEET C-3.3

MATCHLINE - SHEET C-3.4

MATCHLINE - SHEET C-3.3

MATCHLINE - SHEET C-3.3

MATCHLINE - SHEET C-3.6

BUILDING SETBACKS

TOWNHOMES/DUPEX (REAR-LOADED):

- FRONT: 0' MIN TO 25' MAX
- SIDE: 10' MIN

SINGLE FAMILY DETACHED (REAR-LOADED):

- FRONT: 10' MIN
- REAR: 10' MIN
- SIDE: 3' MIN
- CORNER: 10' MIN

SINGLE FAMILY DETACHED (FRONT-LOADED):

- FRONT: 10' MIN
- REAR: 20' MIN
- SIDE: 5' MIN
- CORNER: 10' MIN

CLUBHOUSE:

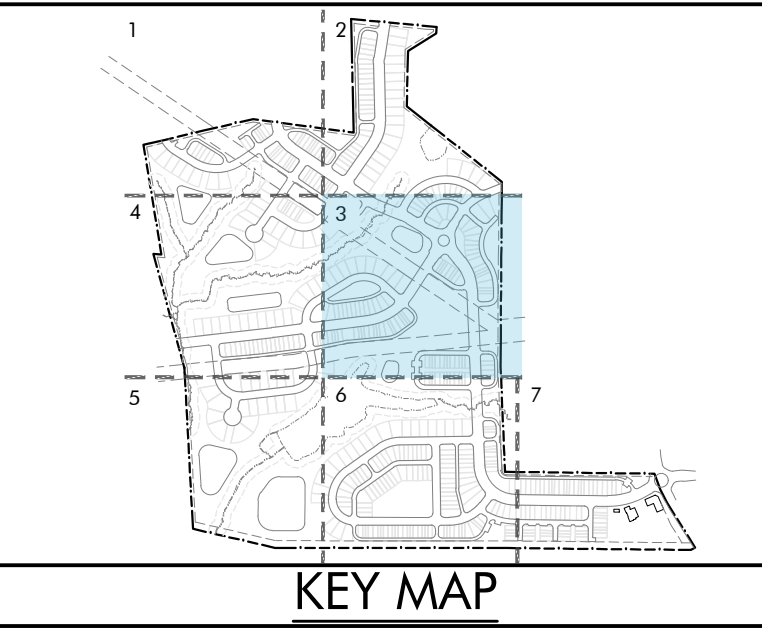
- FRONT: 10' MIN
- REAR: 0' MIN
- SIDE: 6' MIN

LEGEND

UNIT TYPE	DU COUNT*
TOWNHOMES	41 DU
20' UNIT	20 DU
22' UNIT	21 DU
DUPEXES	10 DU
22' UNIT	10 DU
SINGLE FAM. (REAR-LOAD)	29 DU
30' LOT	14 DU
35' LOT	6 DU
40' LOT	9 DU
SINGLE FAM. (FRONT-LOAD)	15 DU
60' LOT	11 DU
70' LOT	3 DU
80' LOT	1 DU

*NOTE: UNIT COUNTS REFLECT NUMBER OF UNITS THAT FIT WITHIN MATCHLINES OF EACH SHEET. ANY UNITS SHOWN OUTSIDE OF MATCHLINES WILL BE COUNTED ON THEIR RESPECTIVE SHEETS.

TOTAL UNIT COUNT FOUND ON OVERALL SITE PLAN SHEET & COVER SHEET.



NOTES:

- BOUNDARY AND TOPOGRAPHIC INFORMATION PROVIDED BY: BATEMAN CIVIL SURVEY COMPANY, 2524 RELIANCE AVENUE, APEX, NC 27539, 919-577-1080
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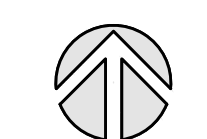
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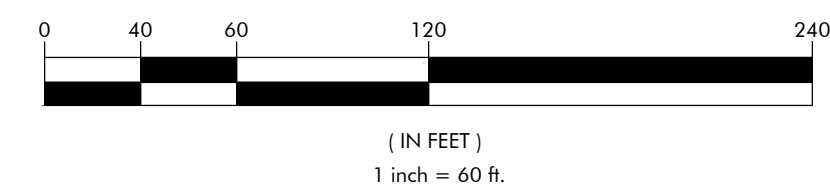
LYNDON OAKS
Master Plan
Site Plan Enlargement 3
Knightdale, North Carolina



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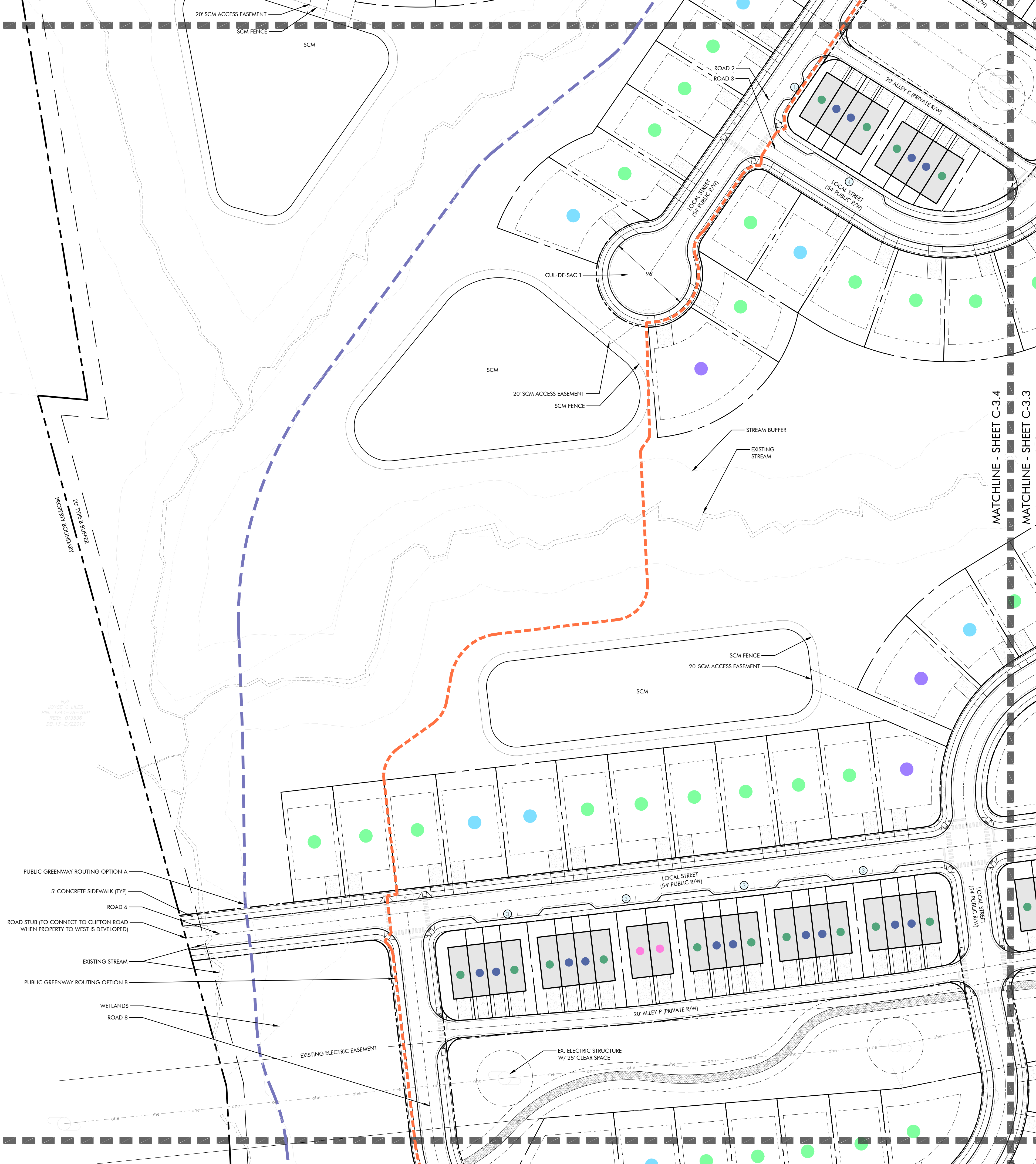
C-3.3

05/03/2024

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FOR REVIEW PURPOSES ONLY

MATCHLINE - SHEET C-3.1
 MATCHLINE - SHEET C-3.4

MATCHLINE - SHEET C-3.4
 MATCHLINE - SHEET C-3.5



BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):

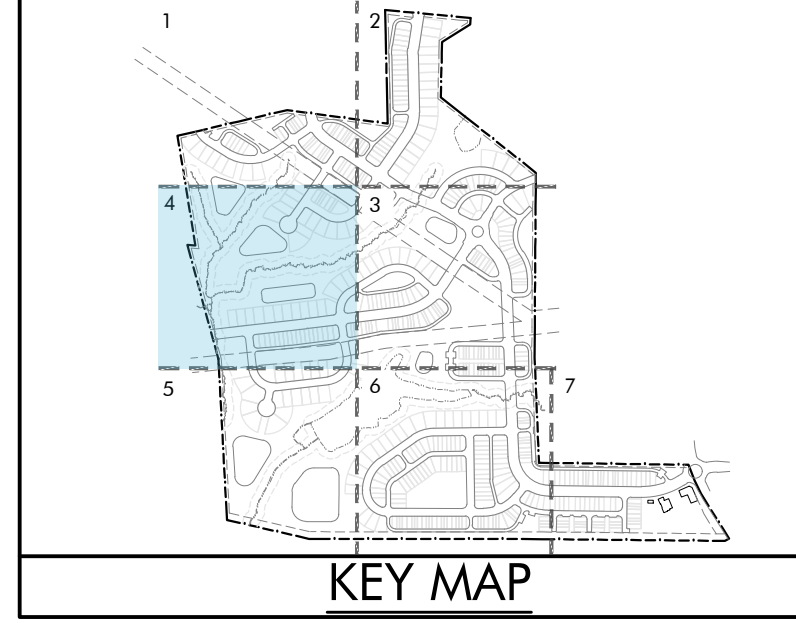
- FRONT: 0' MIN TO 25' MAX
- SIDE: 10' MIN

SINGLE FAMILY DETACHED (REAR-LOADED):

- FRONT: 10' MIN
- REAR: 10' MIN
- SIDE: 3' MIN
- CORNER: 10' MIN

SINGLE FAMILY DETACHED (FRONT-LOADED):

- FRONT: 10' MIN
- REAR: 20' MIN
- SIDE: 5' MIN
- CORNER: 10' MIN



LEGEND

UNIT TYPE	DU COUNT*
TOWNHOMES	28 DU
● 20' UNIT	14 DU
● 22' UNIT	14 DU
DUPLEXES	2 DU
● 22' UNIT	2 DU
SINGLE FAM. (REAR-LOAD)	0 DU
● 30' LOT	0 DU
● 35' LOT	0 DU
● 40' LOT	0 DU
SINGLE FAM. (FRONT-LOAD)	27 DU
● 60' LOT	19 DU
● 70' LOT	5 DU
● 80' LOT	3 DU

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 TOTAL UNIT COUNT FOUND ON OVERALL SITE PLAN SHEET & COVER SHEET.

- NOTES:**
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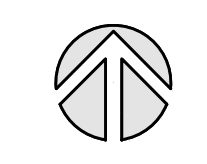
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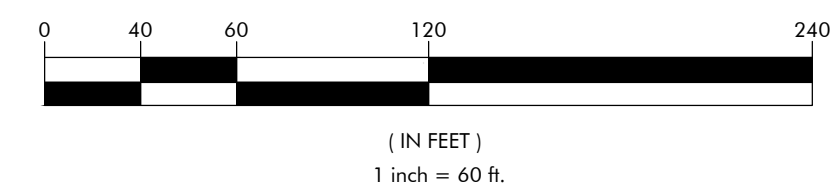
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Master Plan
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 Knightdale, North Carolina



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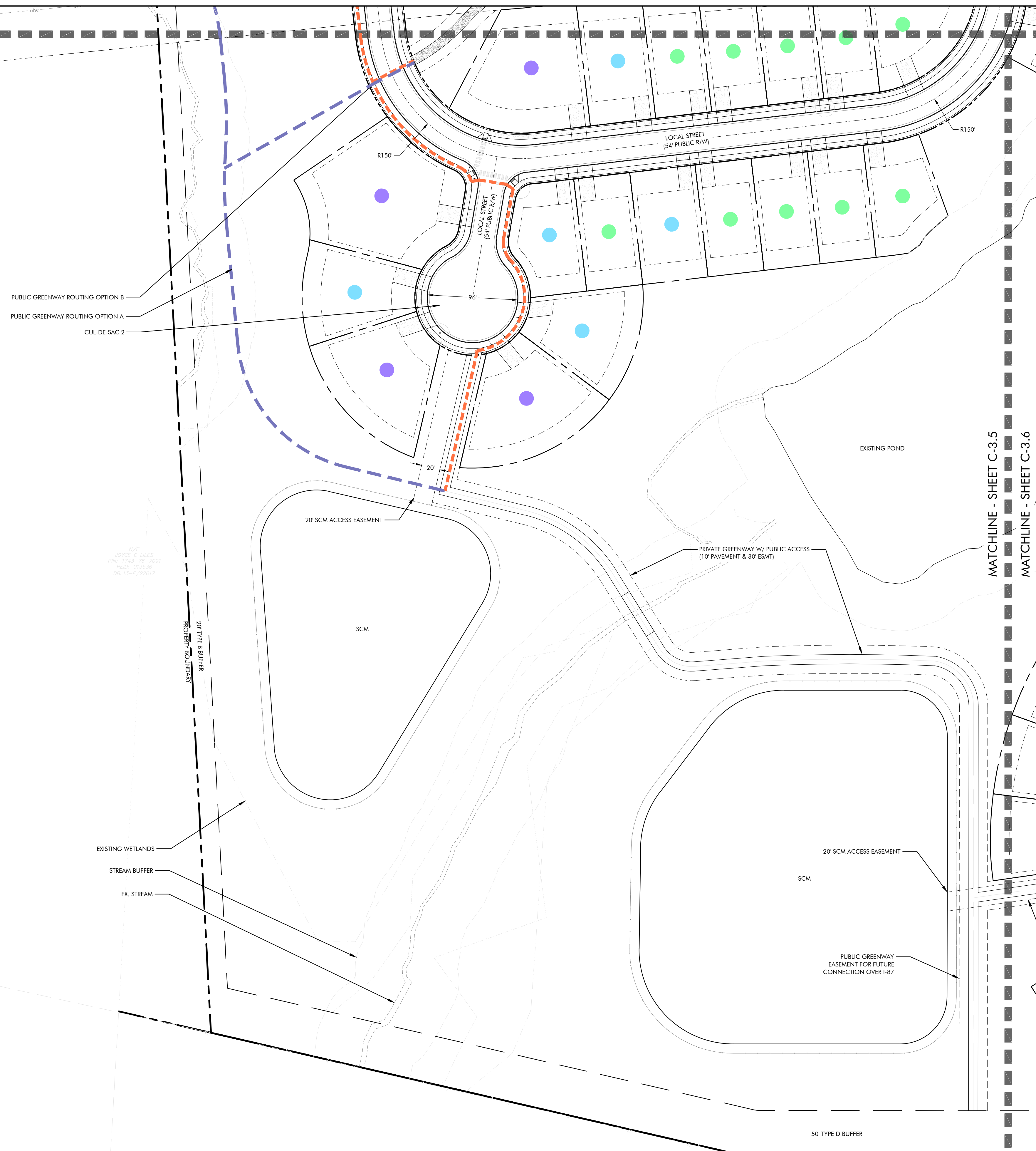


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Project No: 22-RDU-083
 Date: 05/03/2024
 Sheet No:

C-3.4

MATCHLINE - SHEET C-3.4
 MATCHLINE - SHEET C-3.5

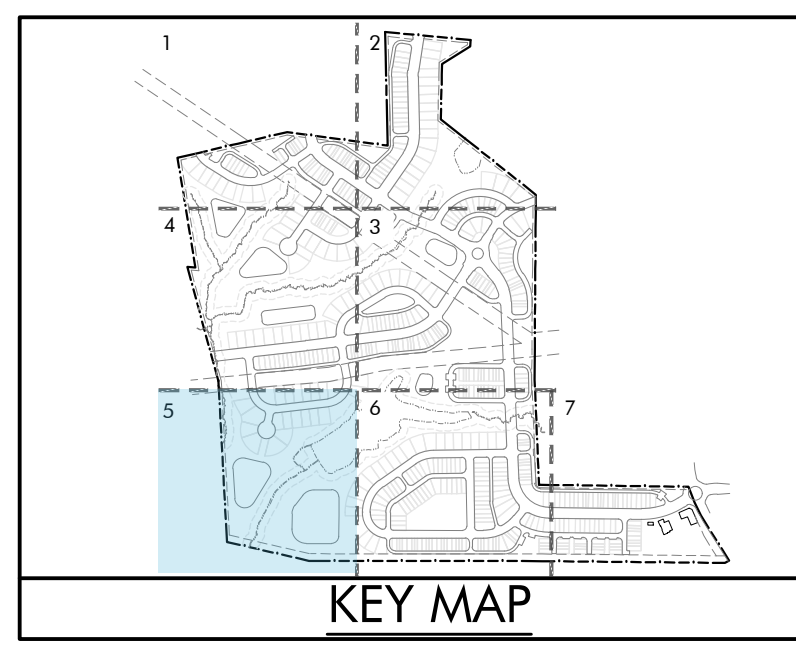


BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):
 - FRONT: 0' MIN TO 25' MAX
 - SIDE: 10' MIN

SINGLE FAMILY DETACHED (REAR-LOADED):
 - FRONT: 10' MIN
 - REAR: 10' MIN
 - SIDE: 3' MIN
 - CORNER: 10' MIN

SINGLE FAMILY DETACHED (FRONT-LOADED):
 - FRONT: 10' MIN
 - REAR: 20' MIN
 - SIDE: 5' MIN
 - CORNER: 10' MIN



LEGEND

UNIT TYPE	DU COUNT*
TOWNHOMES	0 DU
● 20' UNIT	0 DU
● 22' UNIT	0 DU
DUPLEXES	0 DU
● 22' UNIT	0 DU
SINGLE FAM. (REAR-LOAD)	0 DU
● 30' LOT	0 DU
● 35' LOT	0 DU
● 40' LOT	0 DU
SINGLE FAM. (FRONT-LOAD)	18 DU
● 60' LOT	9 DU
● 70' LOT	5 DU
● 80' LOT	4 DU

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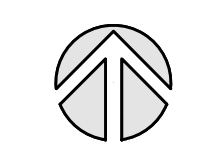
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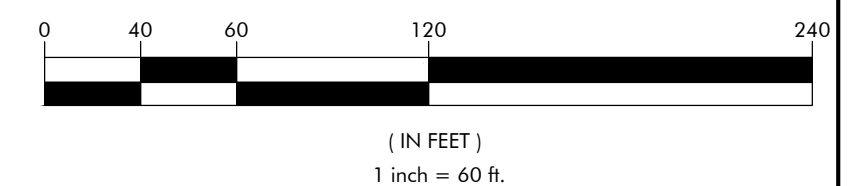
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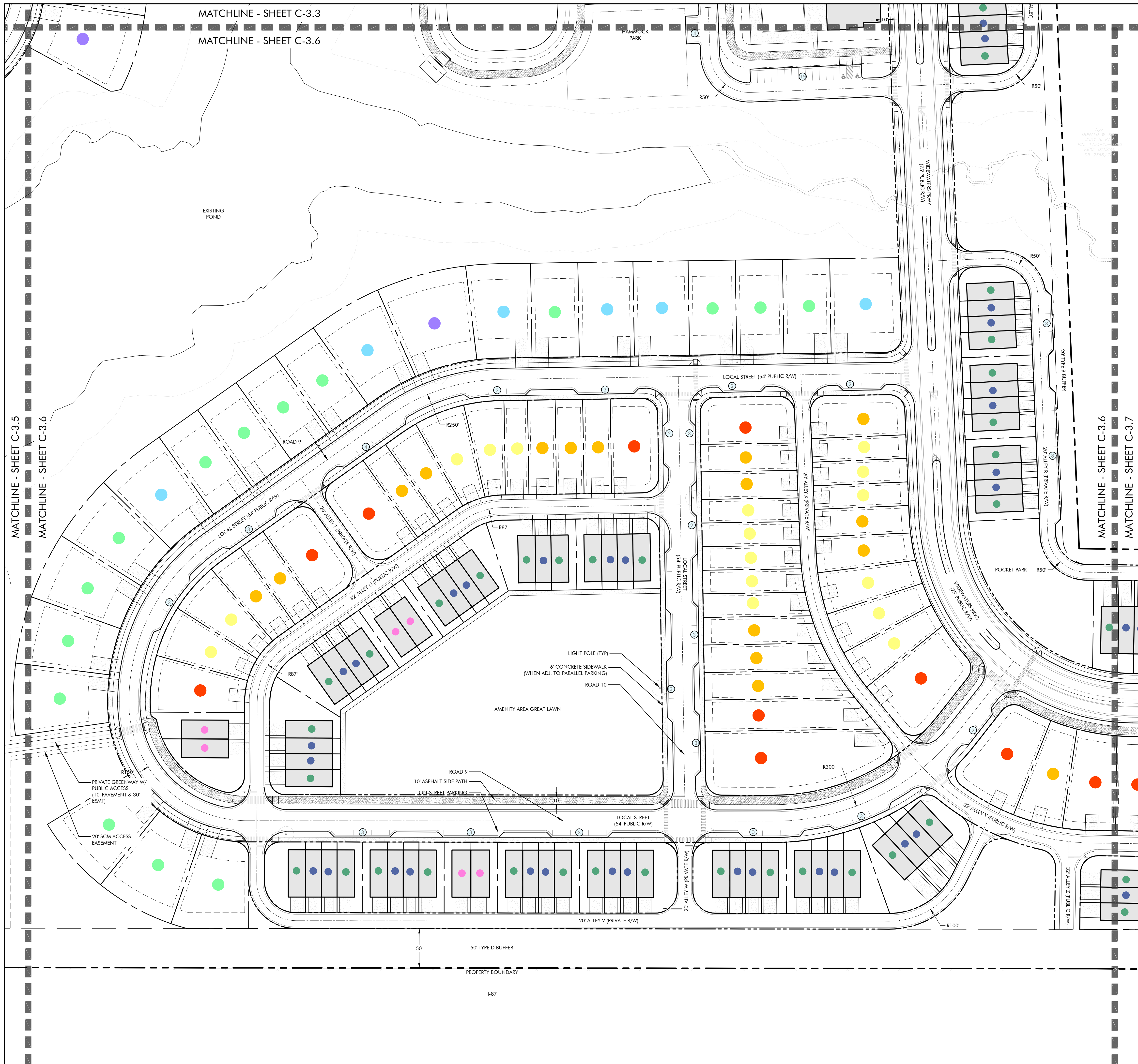


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C-3.5



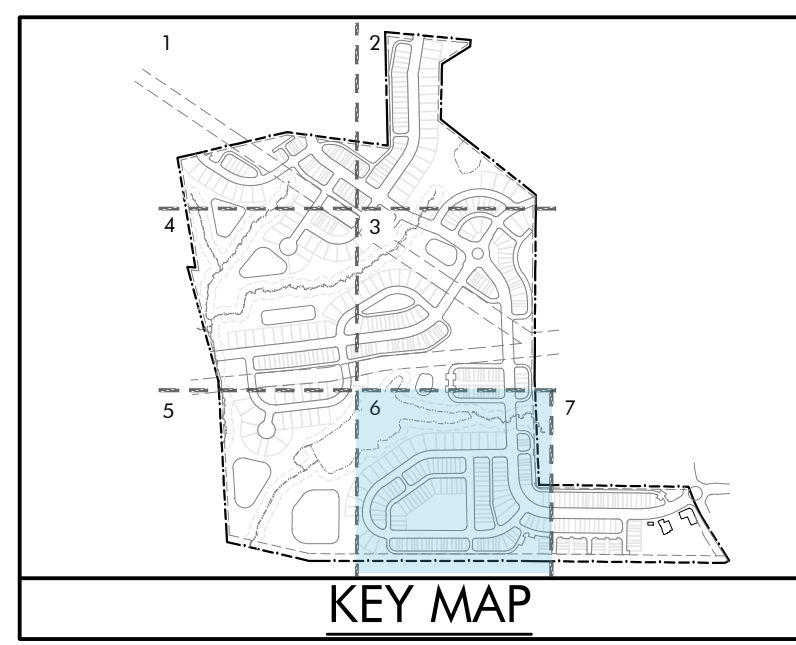
MATCHLINE - SHEET C-3.3
 MATCHLINE - SHEET C-3.6

MATCHLINE - SHEET C-3.5
 MATCHLINE - SHEET C-3.6

MATCHLINE - SHEET C-3.6
 MATCHLINE - SHEET C-3.7

BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):	
- FRONT:	0' MIN TO 25' MAX
- SIDE:	10' MIN
SINGLE FAMILY DETACHED (REAR-LOADED):	
- FRONT:	10' MIN
- REAR:	10' MIN
- SIDE:	3' MIN
- CORNER:	10' MIN
SINGLE FAMILY DETACHED (FRONT-LOADED):	
- FRONT:	10' MIN
- REAR:	20' MIN
- SIDE:	5' MIN
- CORNER:	10' MIN



LEGEND

UNIT TYPE	DU COUNT*
TOWNHOMES	30 DU
● 20' UNIT	30 DU
● 22' UNIT	32 DU
DUPLEXES	6 DU
● 22' UNIT	6 DU
SINGLE FAM. (REAR-LOAD)	42 DU
● 30' LOT	16 DU
● 35' LOT	16 DU
● 40' LOT	10 DU
SINGLE FAM. (FRONT-LOAD)	23 DU
● 60' LOT	15 DU
● 70' LOT	6 DU
● 80' LOT	2 DU

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LYNDON OAKS

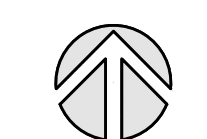
Master Plan

Site Plan Enlargement 6

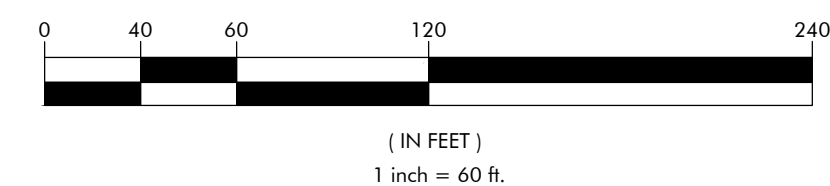
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Project No: 22-RDU-083
 Date: 05/03/2024
 Sheet No:

C-3.6



BUILDING SETBACKS

TOWNHOMES/DUPLEX (REAR-LOADED):

- FRONT: 0' MIN TO 25' MAX
- SIDE: 10' MIN

SINGLE FAMILY DETACHED (REAR-LOADED):

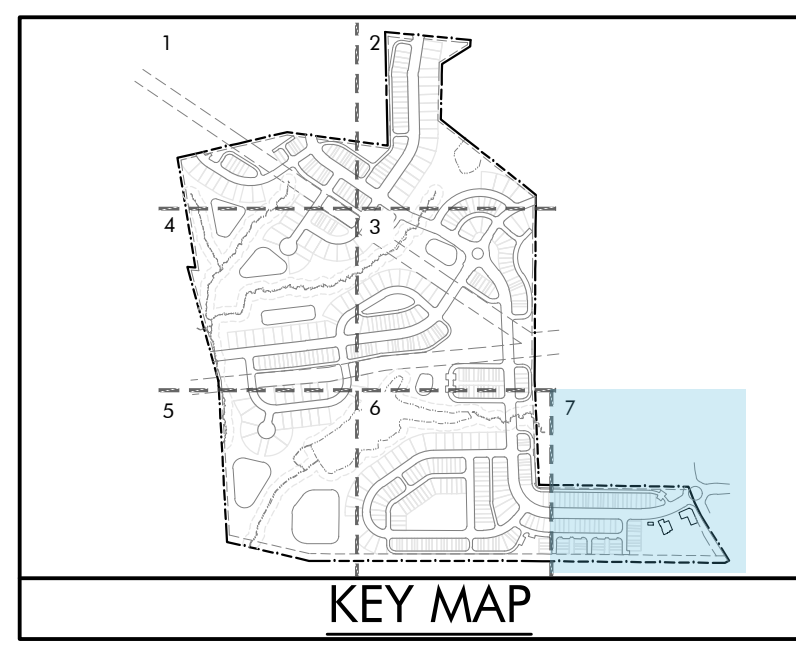
- FRONT: 10' MIN
- REAR: 10' MIN
- SIDE: 3' MIN
- CORNER: 10' MIN

SINGLE FAMILY DETACHED (FRONT-LOADED):

- FRONT: 10' MIN
- REAR: 20' MIN
- SIDE: 5' MIN
- CORNER: 10' MIN

COMMERCIAL:

- FRONT: 10' MIN
- REAR: 0' MIN
- SIDE: 6' MIN



LEGEND

UNIT TYPE	DU COUNT*
TOWNHOMES	54 DU
● 20' UNIT	23 DU
● 22' UNIT	31 DU
DUPLEXES	2 DU
● 22' UNIT	2 DU
SINGLE FAM. (REAR-LOAD)	12 DU
● 30' LOT	5 DU
● 35' LOT	4 DU
● 40' LOT	3 DU
SINGLE FAM. (FRONT-LOAD)	0 DU
● 60' LOT	0 DU
● 70' LOT	0 DU
● 80' LOT	0 DU

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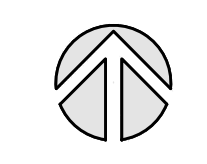
Master Plan

Site Plan Enlargement 7

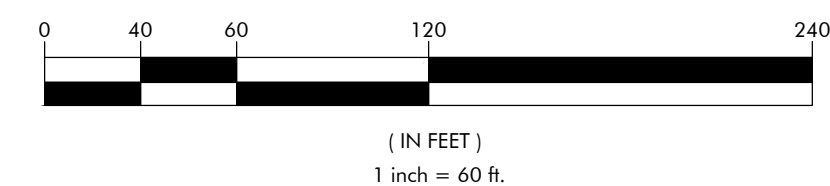
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LYNDON OAKS Master Plan Open Space Plan

Knightsdale, North Carolina

NO.	DATE	BY	REVISIONS:
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Project No: 22-RDU-083
Date: 05/03/2024

Sheet No:

C-3.8

OPEN SPACE CALCULATIONS

TOTAL SITE AREA: ± 171.88 ACRES

DWELLING UNITS:
• SINGLE FAM. DETACHED: 482 DU PROP. TOTAL (UP TO 500 DU ALLOWED)
• TOWNHOMES/DUPLEXES: 209 DU
273 DU

DENSITY: 2.8 DU/AC

RECREATIONAL OPEN SPACE DEDICATION:
• PROXIMITY ZONE: OUTSIDE 1/2 MILE
• BEDROOM ESTIMATE:
--SINGLE FAMILY DETACHED: 209 DU x 3 BEDS = 627 BEDS
--TOWNHOMES/DUPLEXES: 273 DU x 3 BEDS = 819 BEDS

• DEDICATION RATE:
530
--SINGLE FAMILY DETACHED: 627 BEDS x 520 SF = 326,040 SF
--TOWNHOMES/DUPLEXES: 819 BEDS x 520 SF = 425,880 SF

• CALCS:
326,040 SF + 425,880 SF = 751,920 SF
± 751,920 SF [17.26 AC] TOTAL OPEN SPACE REQ.

• REQUIRED OPEN SPACE:
17.26 AC TOTAL REQUIRED
--REQ. ACTIVE: 8.63 AC (50% OF REQ. OPEN SPACE)
--REQ. PASSIVE: 8.63 AC (50% OF REQ. OPEN SPACE)

• PROPOSED OPEN SPACE: 21.53 AC TOTAL PROPOSED (4.27 AC EXTRA PROV.)

--PROP. ACTIVE: 8.77 AC ACTIVE OPEN SPACE PROPOSED
- REQUIRED: 8.63 AC (TO MEET 50% OF TOTAL REQ. OPEN SPACE)
- CREDIT: 2.15 AC CREDIT DUE TO PROP. NEIGHBORHOOD AMENITIES**

- REQ. AFTER CREDIT: 6.48 AC REQ. AFTER CREDIT
- PROPOSED: 8.77 AC ACTIVE OPEN SPACE PROP.
- NET: 0.14 AC EXTRA PROVIDED

--PROP. PASSIVE: 12.76 AC PASSIVE OPEN SPACE PROPOSED
- REQUIRED: 8.63 AC (TO MEET 50% OF TOTAL REQ. OPEN SPACE)
- CREDIT: 2.15 AC CREDIT DUE TO PROP. NEIGHBORHOOD AMENITIES**

- REQ. AFTER CREDIT: 6.48 AC REQ. AFTER CREDIT
- PROPOSED: 12.76 AC PASSIVE OPEN SPACE PROP.
- NET: 4.13 AC EXTRA PROVIDED

OPEN SPACE TOTALS

NAME	AREA	
	SF	AC
PUBLIC PLAZA	2,000.0	0.05
CLUBHOUSE & POOL *	31,776.99	0.73
POCKET PARK 1	4,721.25	0.11
POCKET PARK 2	3,170.44	0.07
POCKET PARK 3	30,287.55	0.70
POCKET PARK 4	18,005.71	0.41
BENCH SWING PARK	37,100.61	0.85
PICKLEBALL COURTS & FITNESS AREAS	36,275.43	0.83
PLAYGROUND	10,763.77	0.25
HAMMOCK PARK	15,056.64	0.35
PUBLIC GREENWAY TRAIL**	71,377.14	1.64
MULTI-PURPOSE FIELD	27,717.99	0.64
OPEN SPACE CREDIT	93,990.00	2.15
TOTAL	382,243.53	8.77

NAME	AREA	
	SF	AC
VILLAGE GREEN***	89,987.06	2.07
SCM W/ FOUNTAIN & POLLINATOR GARDENS	77,052.62	1.77
PRIVATE GREENWAY, SCM W/ FOUNTAIN, OVERLOOK & GAZEBO	106,172.53	2.44
PRIVATE GREENWAY W/ PUBLIC ACCESS	39,811.99	0.91
PRIVATE GREENWAY UNDER EASEMENT	19,112.89	0.44
PRIVATE GREENWAY CONNECTOR	5,003.27	0.11
EXISTING POND NATURAL ACCESS AREA	11,902.75	0.27
DOG PARK	19,113.65	0.44
BENCHES & LANDSCAPE AREA 1	2,506.12	0.06
BENCHES & LANDSCAPE AREA 2	4,806.12	0.11
BENCHES & LANDSCAPE AREA 3	11,527.41	0.26
BENCHES & LANDSCAPE AREA 4	4,800.00	0.11
BENCHES & LANDSCAPE AREA 5	4,800.00	0.11
BENCHES & LANDSCAPE AREA 6	4,800.00	0.11
BENCHES & LANDSCAPE AREA 7	3,671.76	0.08
BENCHES & LANDSCAPE AREA 8	3,671.76	0.08
BENCHES & LANDSCAPE AREA 9	3,671.76	0.08
PUBLIC DISPLAY OF ART	6,640.19	0.15
ENTRY MONUMENT & LANDSCAPING	42,581.60	0.98
OPEN SPACE CREDIT	93,990.00	2.16
TOTAL	555,643.48	12.76

* PER TOWN OF KNIGHTSDALE UDO SECTION 11.2.D.3, "DEVELOPMENTS THAT PROVIDE NEIGHBORHOOD AMENITY FACILITIES WILL RECEIVE A CREDIT OF FIFTY-FIVE (55) PERCENT OF THE REQUIRED PASSIVE OPEN SPACE AND TWENTY-FIVE (25) PERCENT OF THE REQUIRED ACTIVE OPEN SPACE TO BE EQUALLY DIVIDED."
** PER TOWN OF KNIGHTSDALE UDO SECTION 11.2.D.3, "DEVELOPMENTS THAT PROVIDE NEIGHBORHOOD AMENITY FACILITIES WILL RECEIVE A CREDIT OF TWENTY-FIVE (25) PERCENT OF THE REQUIRED PASSIVE OPEN SPACE AND TWENTY-FIVE (25) PERCENT OF THE REQUIRED ACTIVE OPEN SPACE TO BE EQUALLY DIVIDED."
*** PER TOWN OF KNIGHTSDALE UDO SECTION 11.2.D.3, "FOR EACH STREET SEGMENT WITHIN A DEVELOPMENT THAT IS SINGLE-LOADED, THE AREA CONSISTING OF THE LENGTH TIMES HALF OF THE WIDTH OF SAID STREET SEGMENT RIGHT-OF-WAY SHALL BE CREDITED AT A RATE OF ONE HUNDRED (100) PERCENT AGAINST THE PASSIVE RECREATIONAL OPEN SPACE SQUARE FOOT REQUIREMENT."

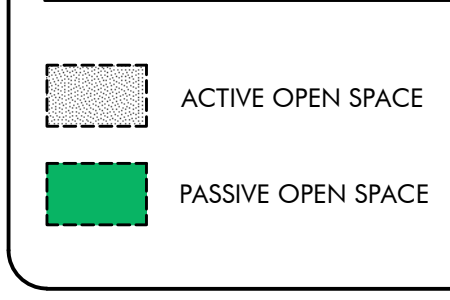
WATER ALLOCATION

WATER ALLOCATION POLICY	
MAJOR SUBDIVISION	
MAJOR SUBDIVISION BASE POINTS	15 pts.
CONSERVATION OF NATURAL HABITAT	7 pts.
CONSTRUCT A FOUNTAIN WITHIN THE BMP	4 pts.
ON-STREET PARKING	4 pts.
RESIDENTIAL ARCHITECTURAL STANDARDS	15 pts.
3,000' OR MORE OF A 6-FOOT WIDE PATH	3 pts.
RESORT STYLE POOL	2 pts.
DECK/PATIO (2,000 SF MIN.)	2 pts.
CLUBHOUSE (NO MEETING SPACE, BATHROOMS & CHANGING ROOMS ONLY)	3 pts.
IPEMA CERTIFIED PLAYGROUND EQUIPMENT	4 pts.
PICKLEBALL COURTS	5 pts.
OUTDOOR DISPLAY OF PUBLIC ART	4 pts.
TOTAL	68 pts.

SINGLE-USE RETAIL	
SINGLE USE RETAIL BASE POINTS	41 pts.
CONSTRUCTION OF GATEWAY LANDSCAPING	5 pts.
DECK/PATIO (2,000 SF MIN.)	2 pts.
OUTDOOR DISPLAY OF PUBLIC ART	4 pts.
TOTAL	52 pts.

NOTE: SINGLE-USE RETAIL WATER ALLOCATION POLICY POINTS ARE PRELIMINARY AND SUBJECT TO CHANGE WHEN THE DEVELOPER OF THE OUTPARCEL SUBMITS A SITE PLAN.

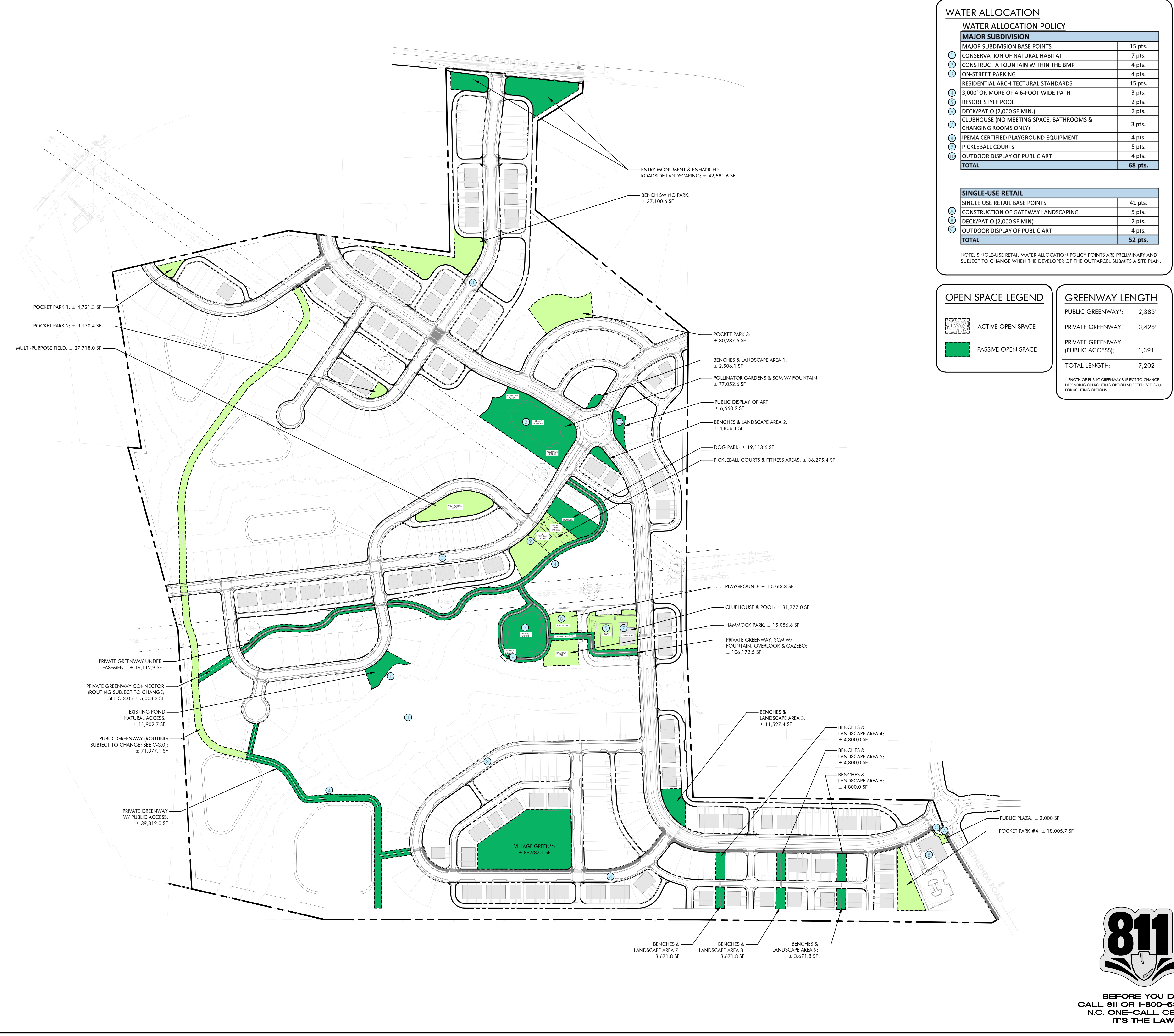
OPEN SPACE LEGEND



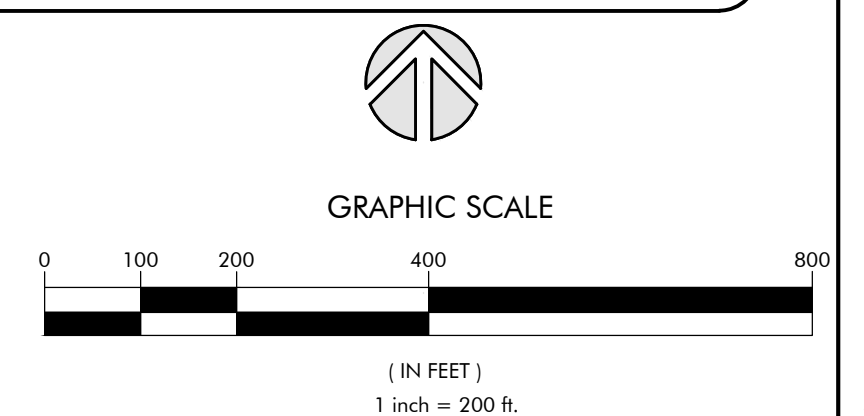
GREENWAY LENGTH

PUBLIC GREENWAY*: 2,385'
PRIVATE GREENWAY: 3,426'
PRIVATE GREENWAY (PUBLIC ACCESS): 1,391'
TOTAL LENGTH: 7,202'

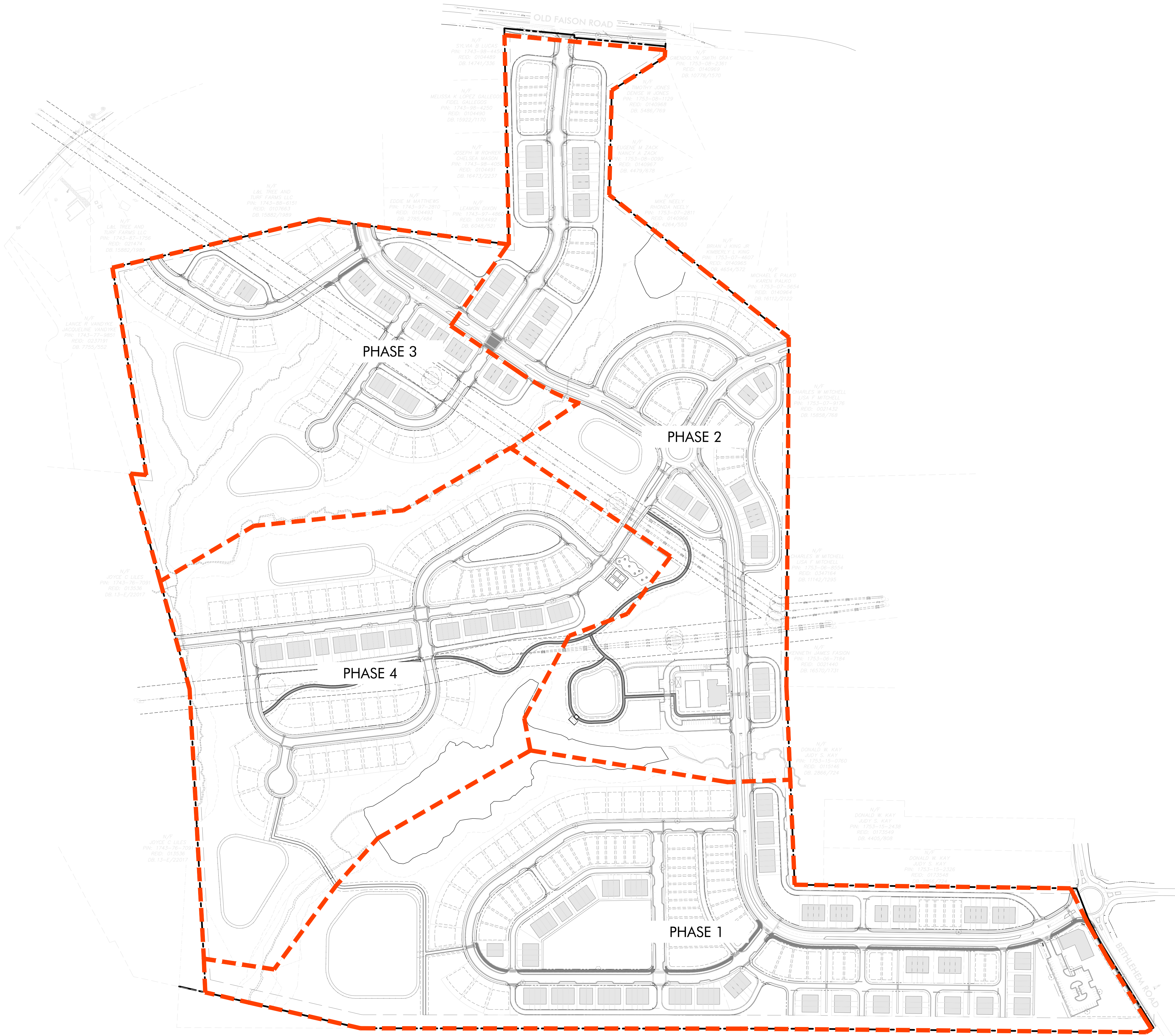
*LENGTH OF PUBLIC GREENWAY SUBJECT TO CHANGE DEPENDING ON ROUTING OPTION SELECTED. SEE C-3.0 FOR ROUTING OPTIONS.



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NOTES:
 COMMERCIAL PARCEL WILL BE PLATTED WITH FIRST PHASE OF DEVELOPMENT.



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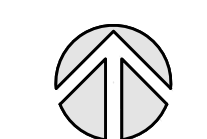
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LYNDON OAKS Master Plan Phasing Plan

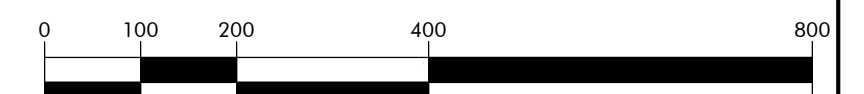
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GRAPHIC SCALE



(IN FEET)
 1 inch = 200 ft.

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#3	03/28/2024		

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Date: 05/03/2024

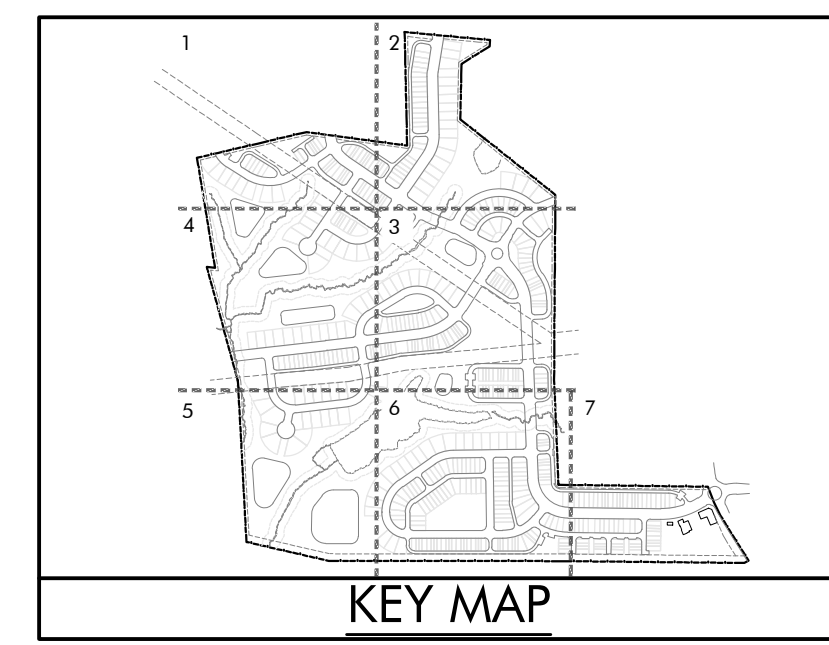
Sheet No:

C-3.9



STREET TABLE	
NAME	LENGTH (LF)
ROAD 1	934'
ROAD 2	443'
ROAD 3	698'
ROAD 4	740'
ROAD 5	513'
ROAD 6	2,081'
ROAD 7	923'
ROAD 8	1,372'
ROAD 9	2,504'
ROAD 10	565'
CUL-DE-SAC 1	200'
CUL-DE-SAC 2	152'
TART FARM ROAD	1,235'
WIDEWATERS PARKWAY	4,424'

ALLEY TABLE	
NAME	LENGTH
ALLEY A	501'
ALLEY B	913'
ALLEY C	156'
ALLEY D	465'
ALLEY E	414'
ALLEY F	1,340'
ALLEY G	169'
ALLEY H	154'
ALLEY I	191'
ALLEY J	981'
ALLEY K	324'
ALLEY L	614'
ALLEY M	986'
ALLEY N	353'
ALLEY O	473'
ALLEY P	1,372'
ALLEY Q	486'
ALLEY R	1,477'
ALLEY S	165'
ALLEY T	163'
ALLEY U	788'
ALLEY V	1,104'
ALLEY W	127'
ALLEY X	543'
ALLEY Y	997'
ALLEY Z	1111'
ALLEY AA	287'
ALLEY BB	108'
ALLEY CC	289'



LEGEND:

- PRIVATE ALLEY**
R/W: 20'
WIDTH: 20' (BOC-BOC)
SPEED: 10 MPH
- PUBLIC ALLEY**
R/W: 32'
WIDTH: 18' (FOC-FOC)
SPEED: 10' MPH
- LOCAL STREET**
R/W: 54' - 60' (GREENWAY CONNECTOR)
WIDTH: 28'-41' (FOC-FOC)*
SPEED: 20-25 MPH
- MAIN STREET**
R/W: 70'
WIDTH: 47' (BOC-BOC)
SPEED: 20-25 MPH
- URBAN MAIN STREET (WIDE SIDEWALK)**
R/W: 72'
WIDTH: 48' (BOC-BOC)*
SPEED: 20-25 MPH
- AVENUE - 3 LANE DIVIDED (WIDEWATERS PKW)**
R/W: 75' - 80'
WIDTH: VARIES (BOC-BOC)
SPEED: 30-35 MPH
- CUL-DE-SAC**
R/W: 61.5' RADIUS
WIDTH: 48' RADIUS (BOC-BOC)
LENGTH: 200' MAX
- ROUNDBOUT**
R/W: VARIES
SPEED: 20-25 MPH

*NOTE: ON STREET PARKING UTILIZED WHERE APPROPRIATE. SEE PLAN FOR LOCATIONS.

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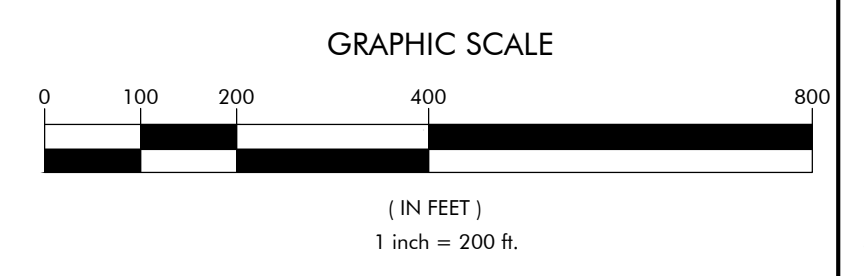
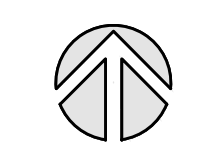
LYNDON OAKS Master Plan

Overall Signage & Marking Plan

Knightsdale, North Carolina

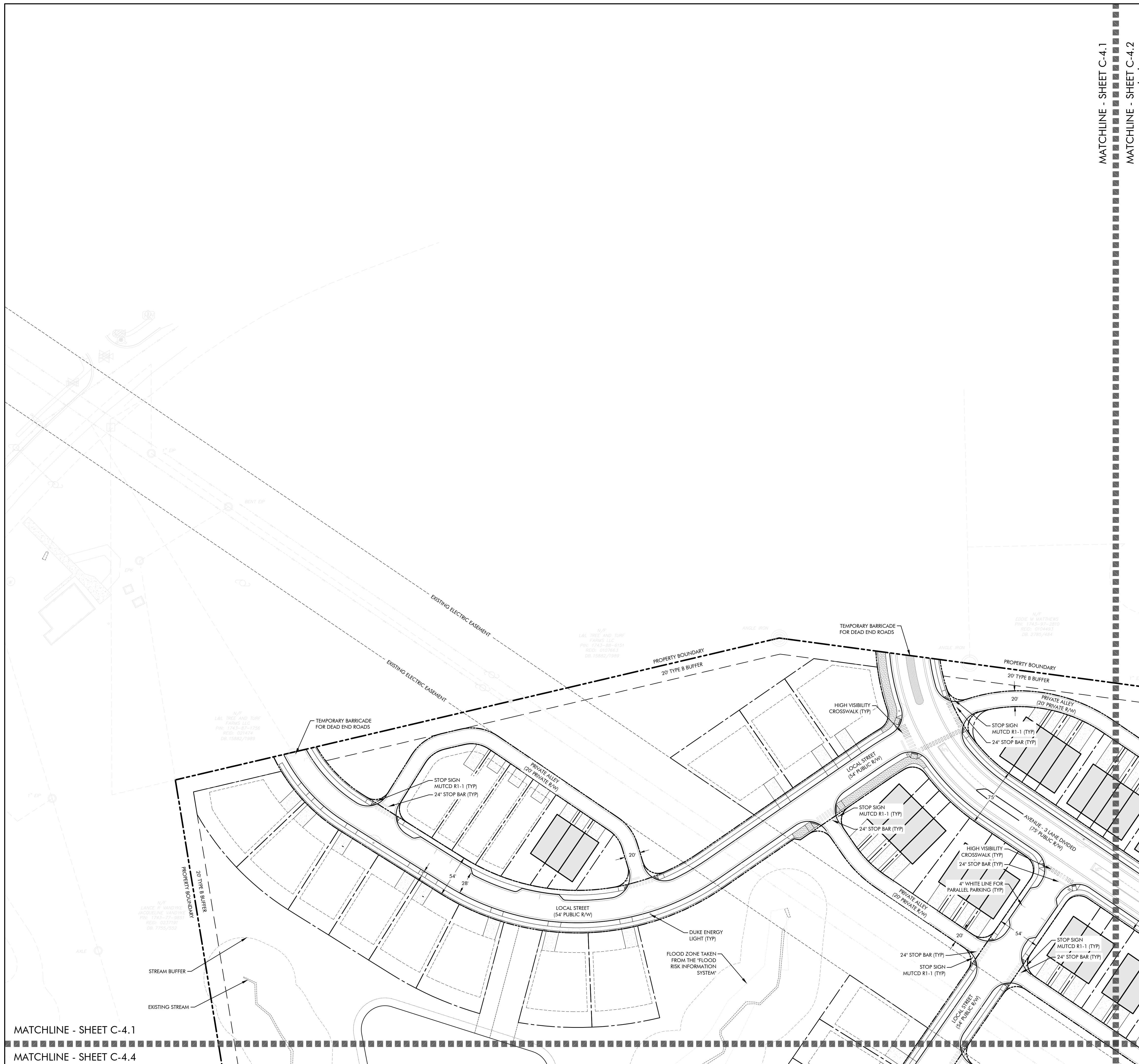


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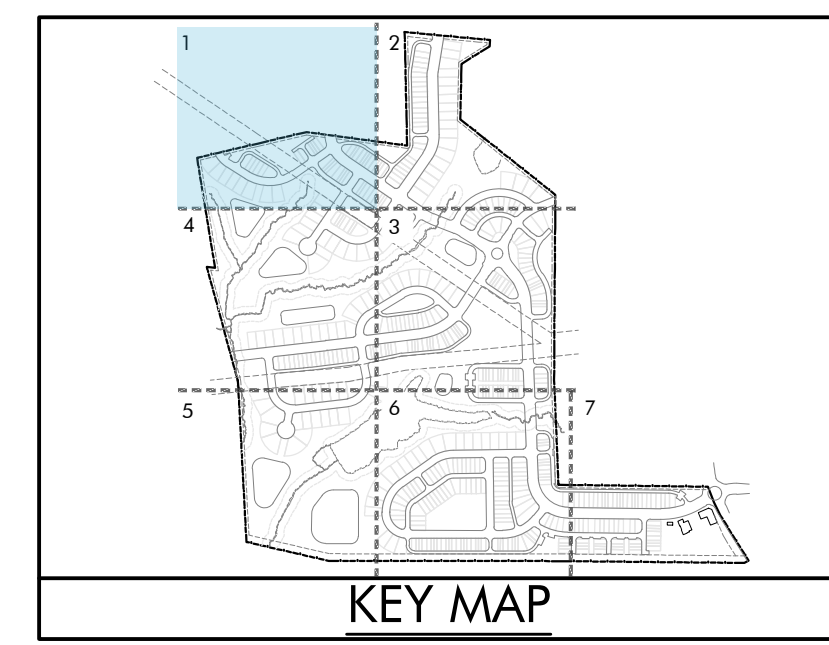
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Project No: 22-RDU-083
Date: 05/03/2024
Sheet No:
C-4.0

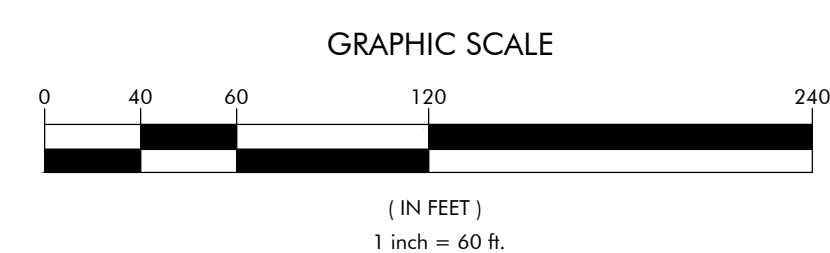
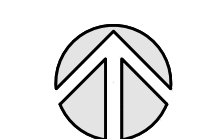


MATCHLINE - SHEET C-4.1
 MATCHLINE - SHEET C-4.4

MATCHLINE - SHEET C-4.1
 MATCHLINE - SHEET C-4.2



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LYNDON OAKS Master Plan Signage & Marking Plan Enlargement 1

Knightsdale, North Carolina

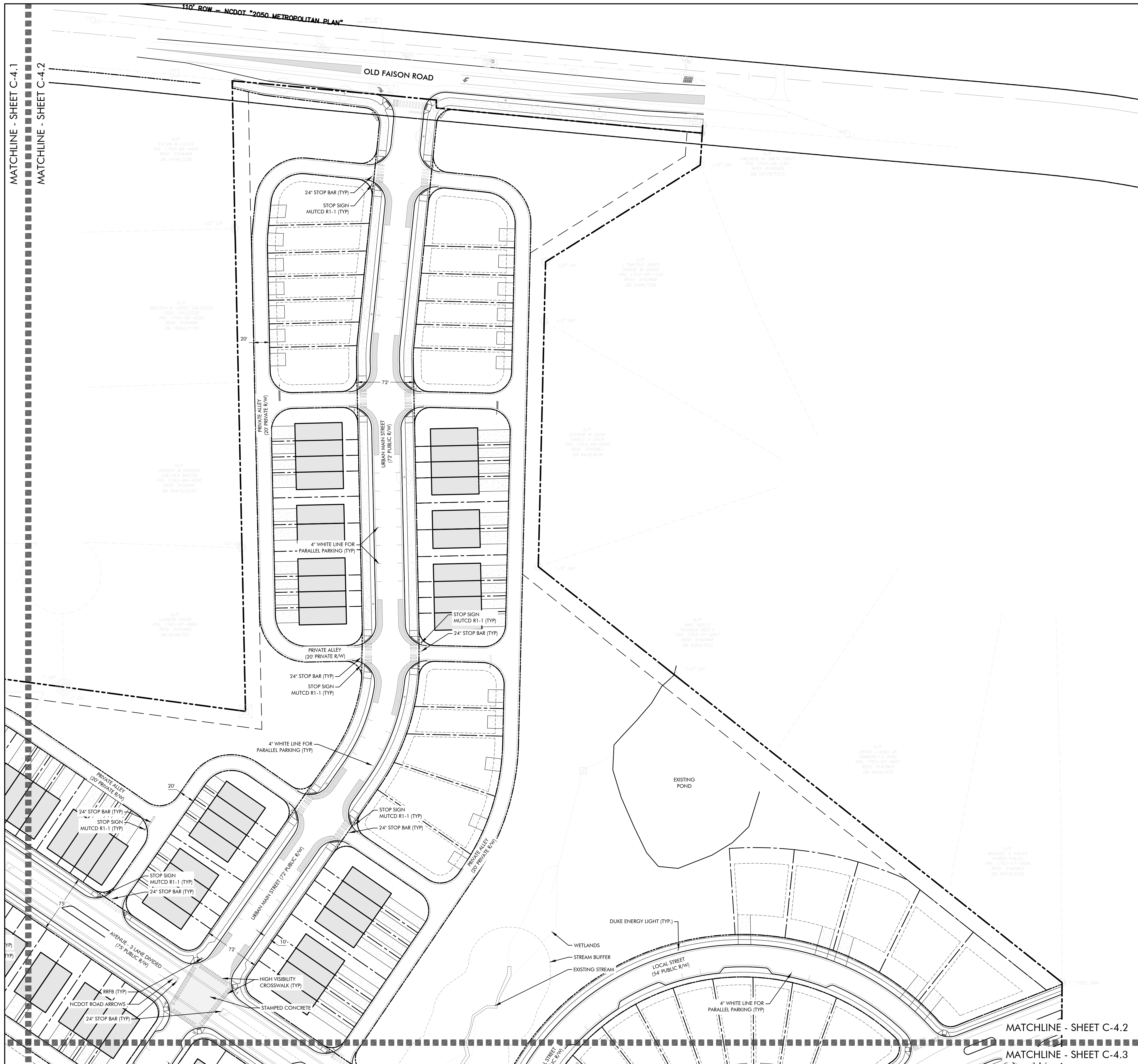
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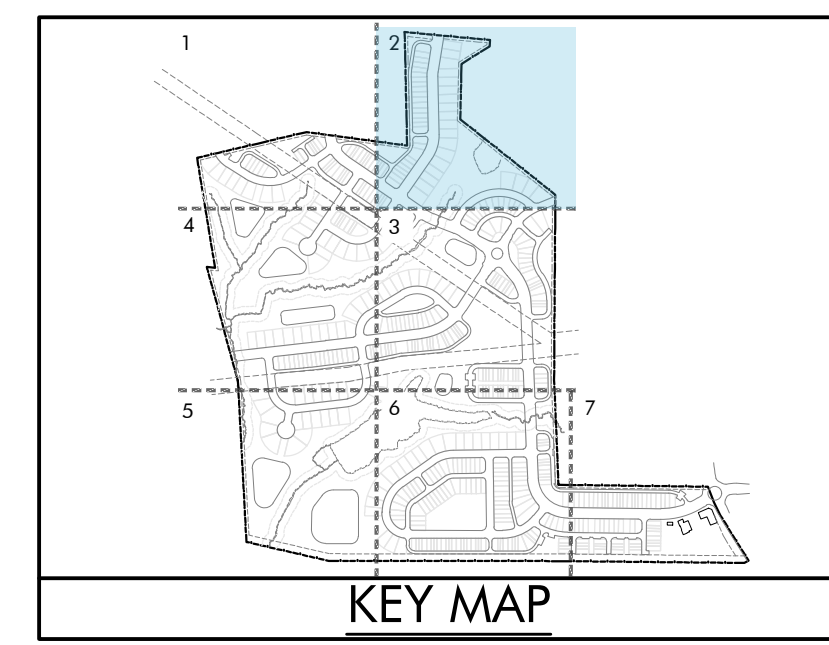
Sheet No:

C-4.1



MATCHLINE - SHEET C-4.1
MATCHLINE - SHEET C-4.2

MATCHLINE - SHEET C-4.2
MATCHLINE - SHEET C-4.3



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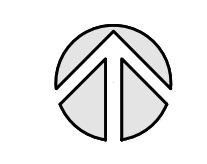
Master Plan

Signage & Marking Plan Enlargement 2

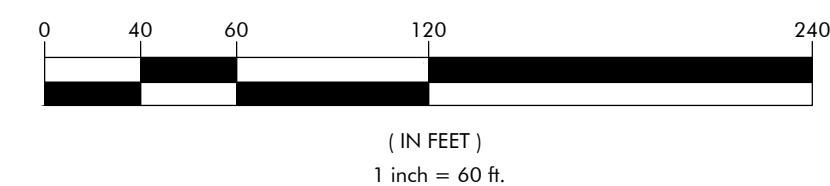
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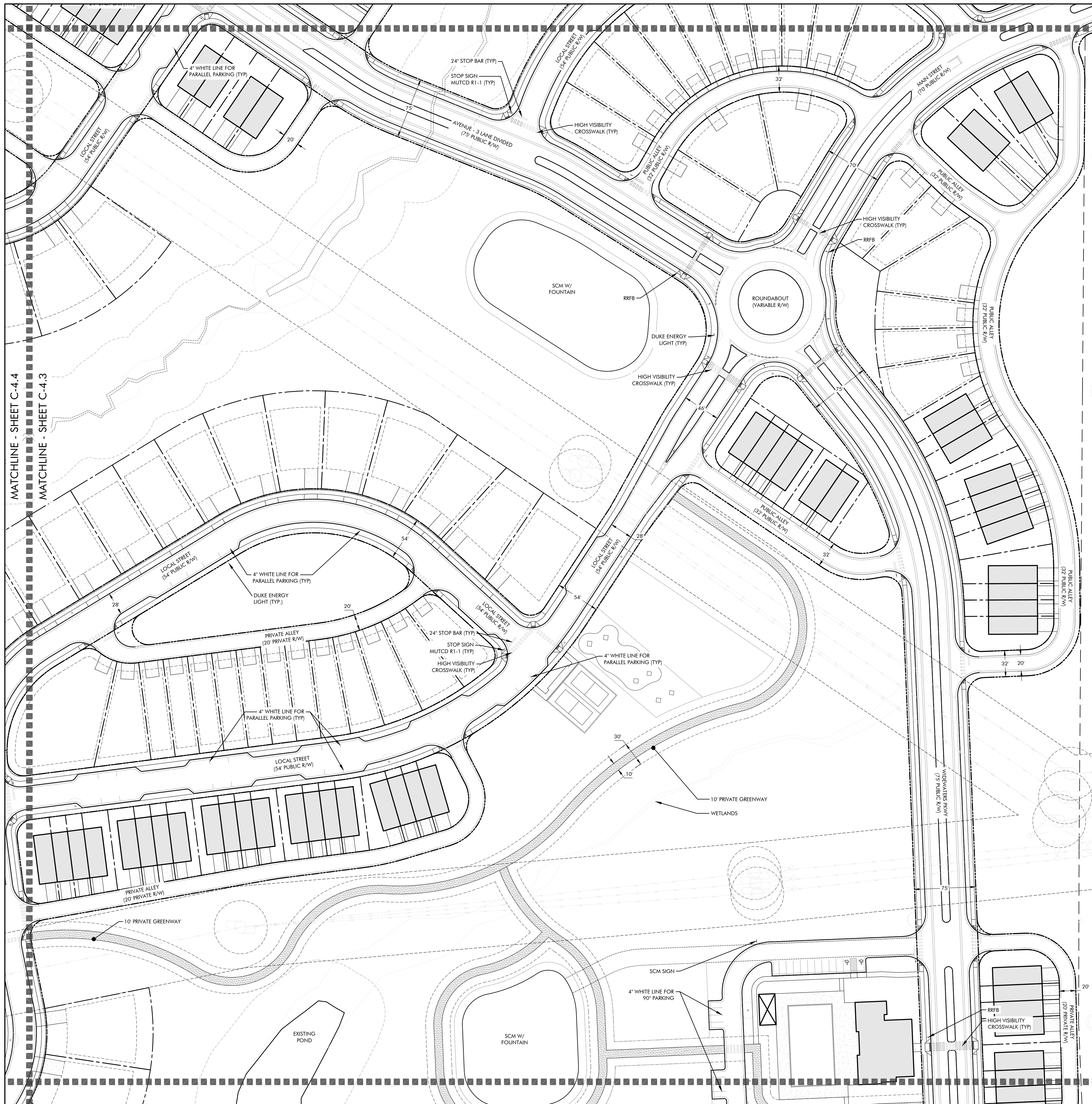
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#3	03/28/2024		

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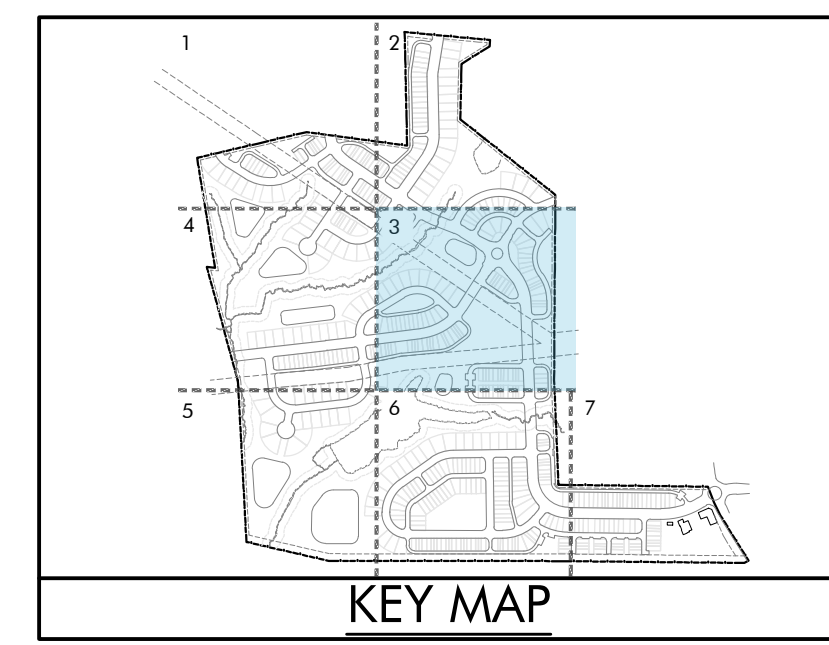
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C-4.2



MATCHLINE - SHEET C-4.2
 MATCHLINE - SHEET C-4.3

MATCHLINE - SHEET C-4.3
 MATCHLINE - SHEET C-4.6



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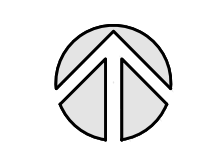
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LYNDON OAKS Master Plan Signage & Marking Plan Enlargement 3

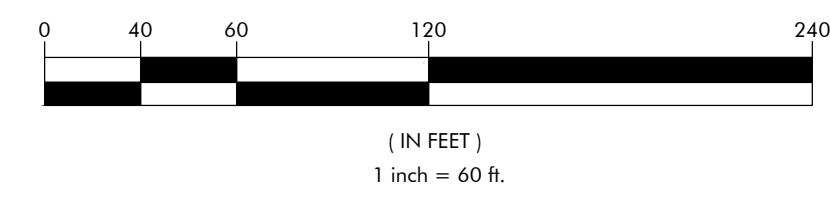
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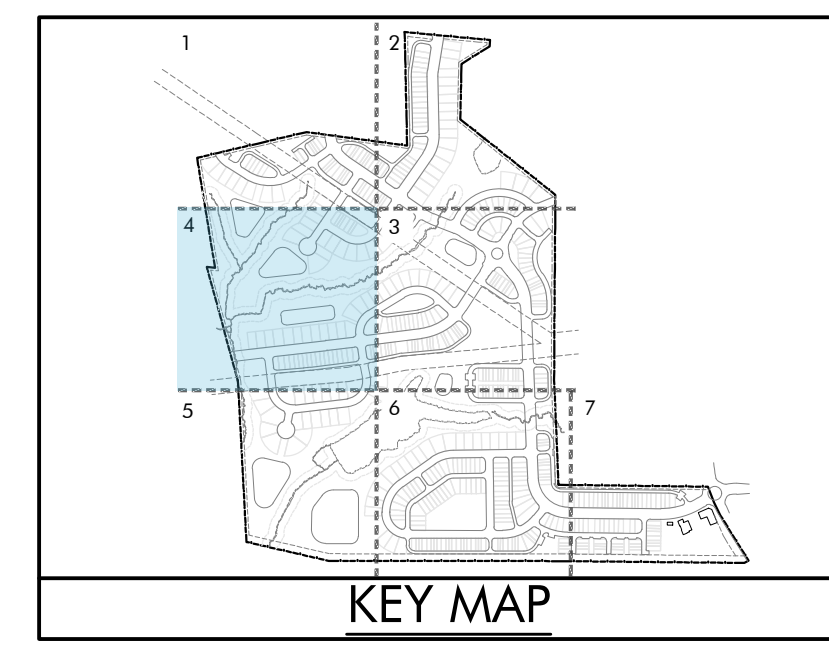
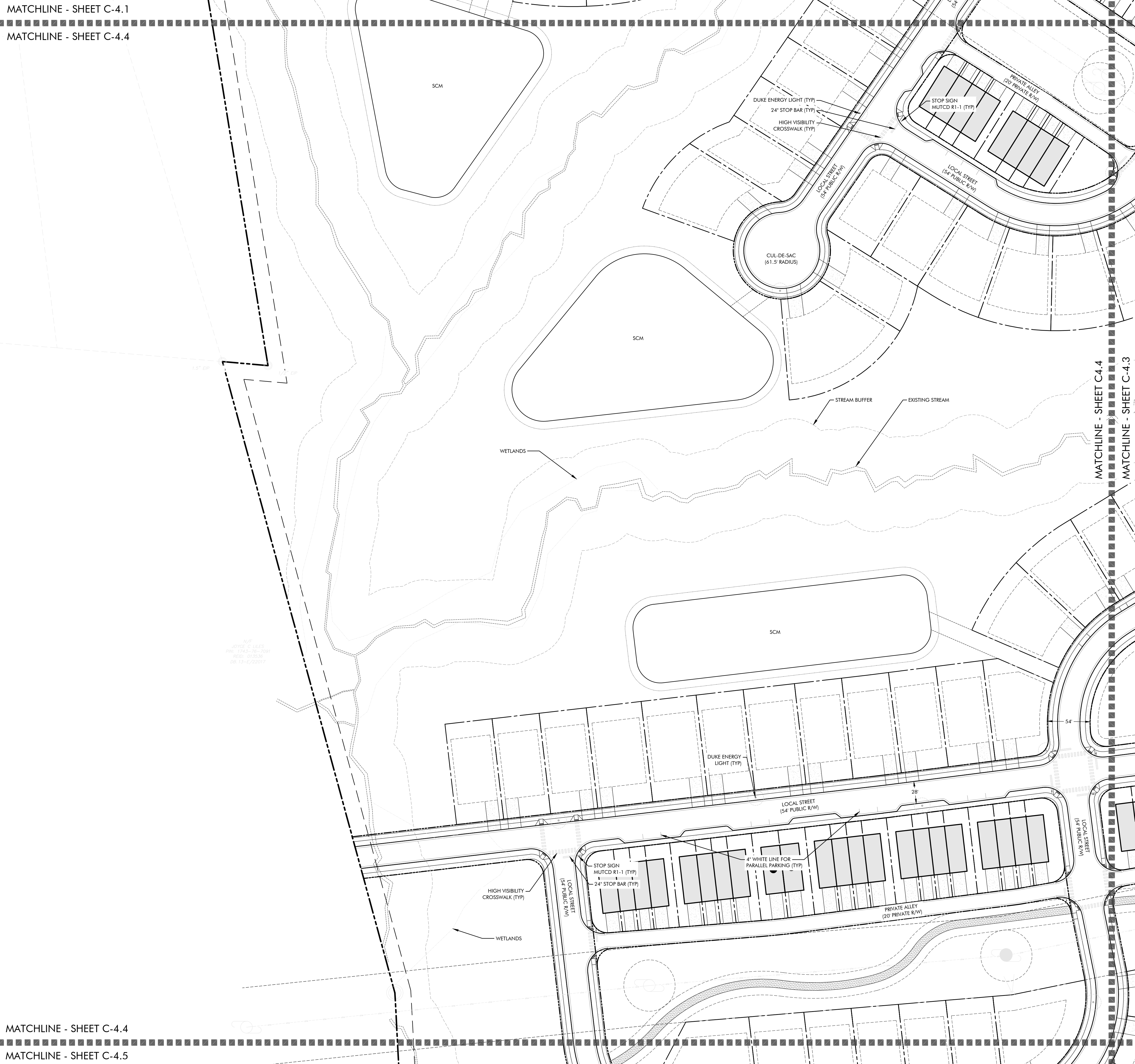
C-4.3

MATCHLINE - SHEET C-4.1

MATCHLINE - SHEET C-4.4

MATCHLINE - SHEET C-4.4

MATCHLINE - SHEET C-4.5



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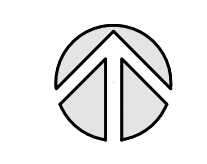
Master Plan

Signage & Marking Plan Enlargement 4

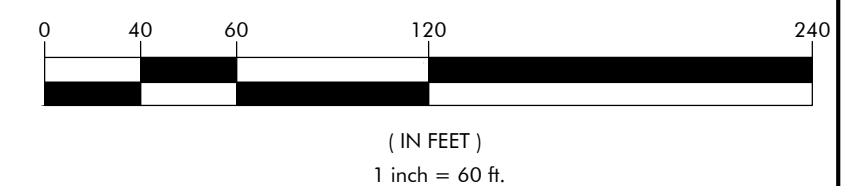
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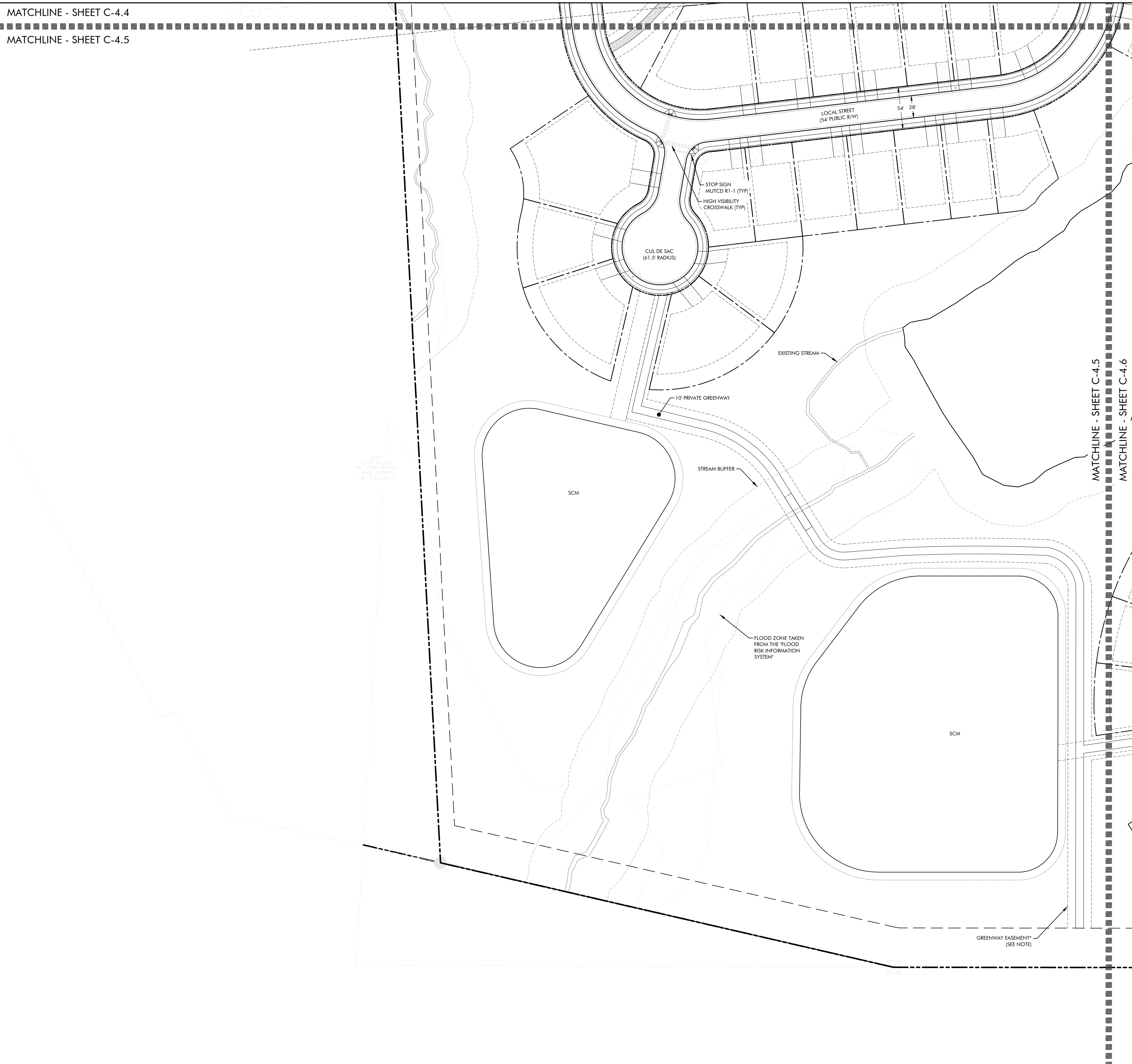
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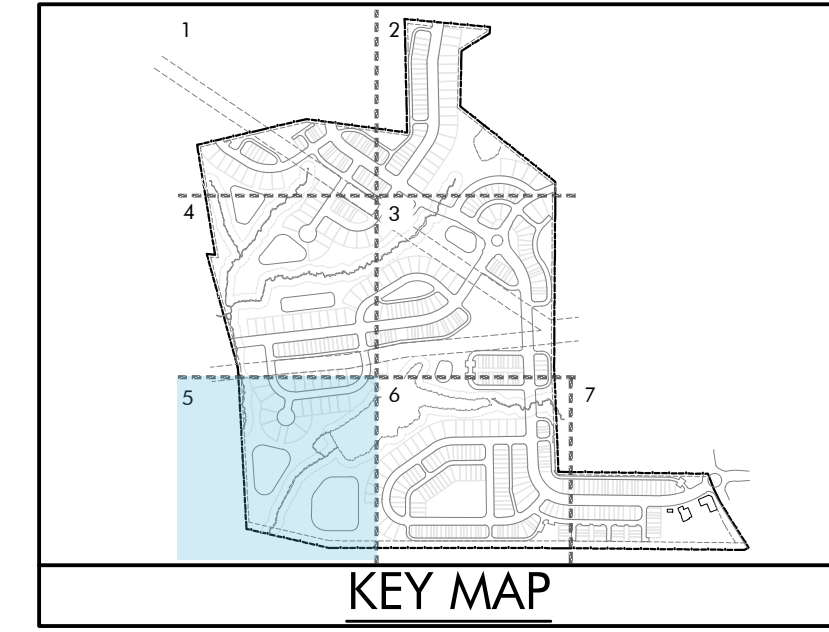
C-4.4

MATCHLINE - SHEET C-4.4

MATCHLINE - SHEET C-4.5



NOTES:
 1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
 2. ALL DISTANCES ARE TO CENTERLINE UNLESS OTHERWISE NOTED.
 3. ALL DISTANCES ARE TO FACE UNLESS OTHERWISE NOTED.
 4. ALL DISTANCES ARE TO CENTERLINE UNLESS OTHERWISE NOTED.



NOTES:
 PER ZMA-2-23 REGARDING EASEMENT FOR FUTURE PEDESTRIAN BRIDGE OVER I-87:
 PRIOR TO APPROVAL OF CONSTRUCTION DRAWINGS FOR THE FIRST PHASE OF DEVELOPMENT, THE APPLICANT WILL OFFER A MINIMUM OF 100 SQUARE FEET OF SITE AREA AS A NON-EXCLUSIVE, PERMANENT EASEMENT AREA TO BE DEDICATED TO THE TOWN FOR CONSTRUCTION, MAINTENANCE, AND REPAIR OF FOOTINGS FOR A FUTURE PEDESTRIAN BRIDGE OVER I-87. TOGETHER WITH SUCH TEMPORARY CONSTRUCTION EASEMENTS AS NECESSARY FOR COMPLETION OF SUCH WORK, IF ACCEPTED BY THE TOWN TRANSPORTATION DIRECTOR, SUCH EASEMENT AREA SHALL BE SHOWN ON THE CONSTRUCTION DRAWINGS AND SUBDIVISION PLAT AND GRANTED TO THE TOWN PURSUANT TO A RECORDED DEED OF EASEMENT.



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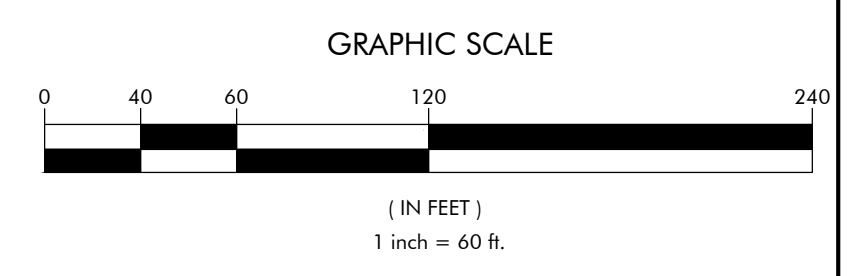
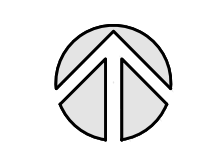
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LYNDON OAKS
Master Plan
Signage & Marking Plan Enlargement 5
 Knightdale, North Carolina



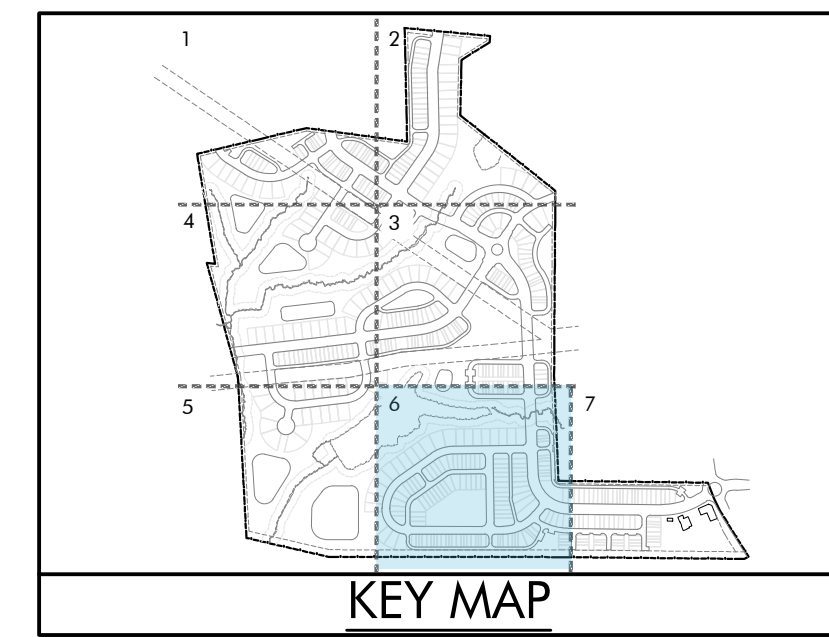
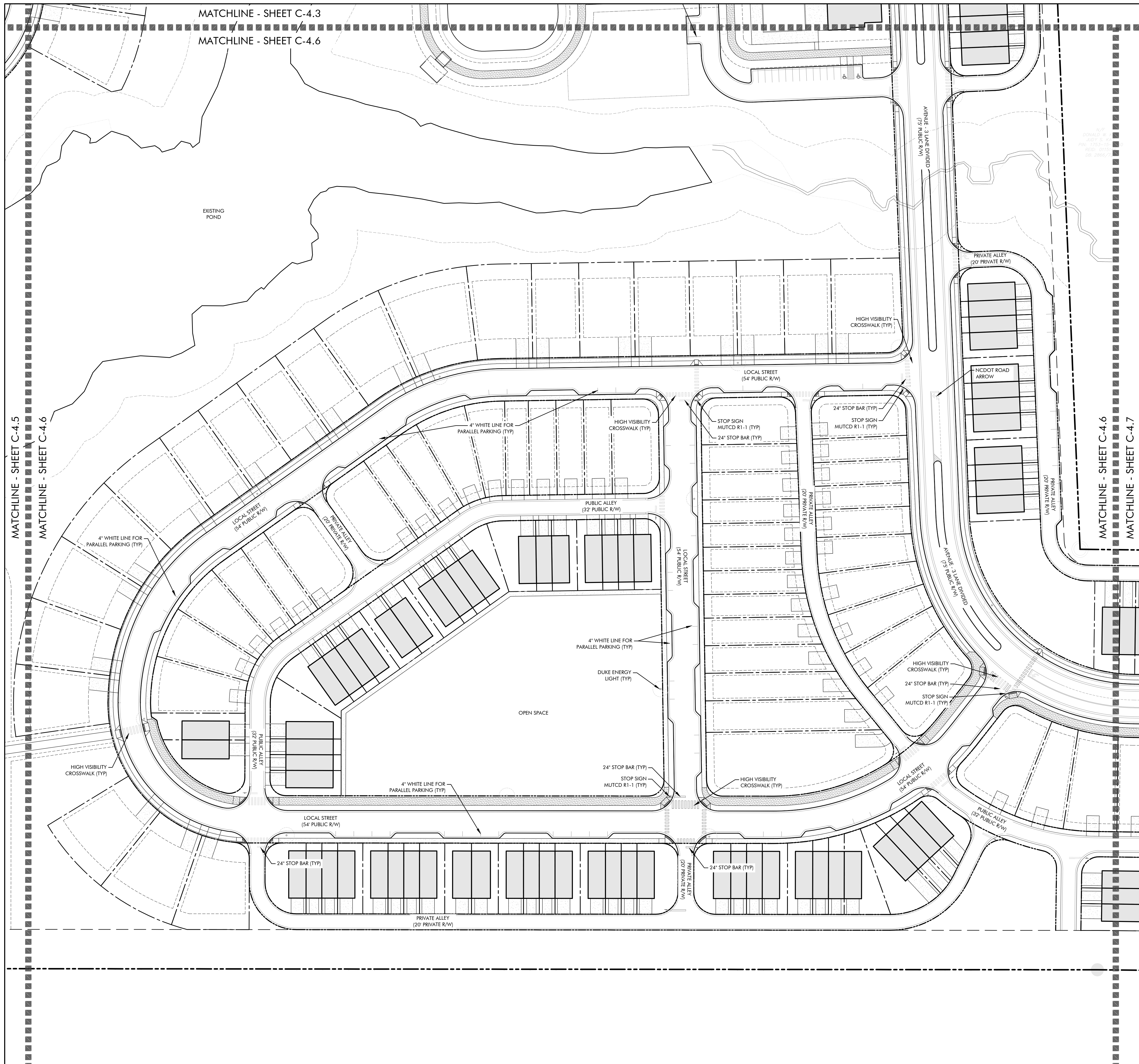
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C-4.5



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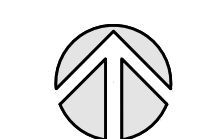
Master Plan

Signage & Marking Plan Enlargement 6

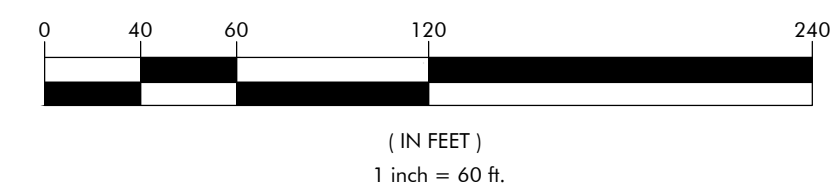
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NO.	DATE	BY	REVISIONS:
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#3	03/28/2024		

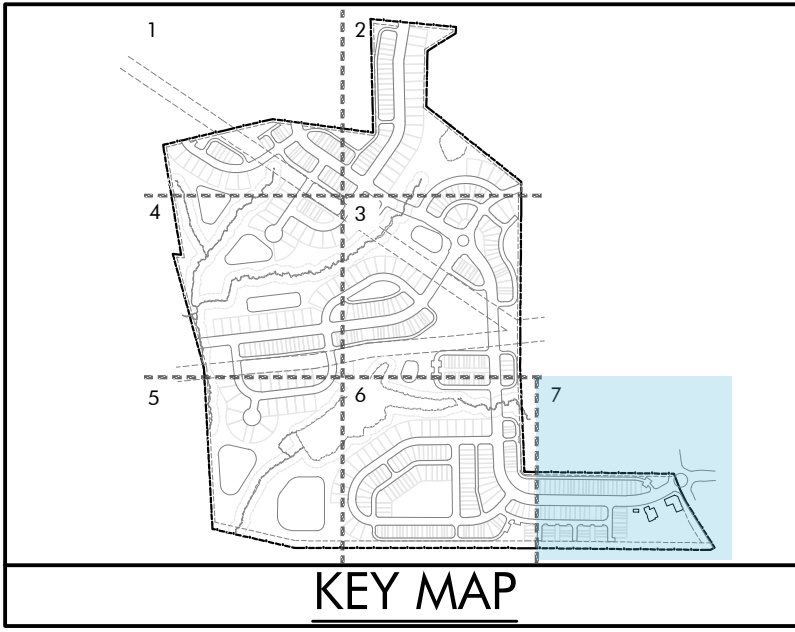
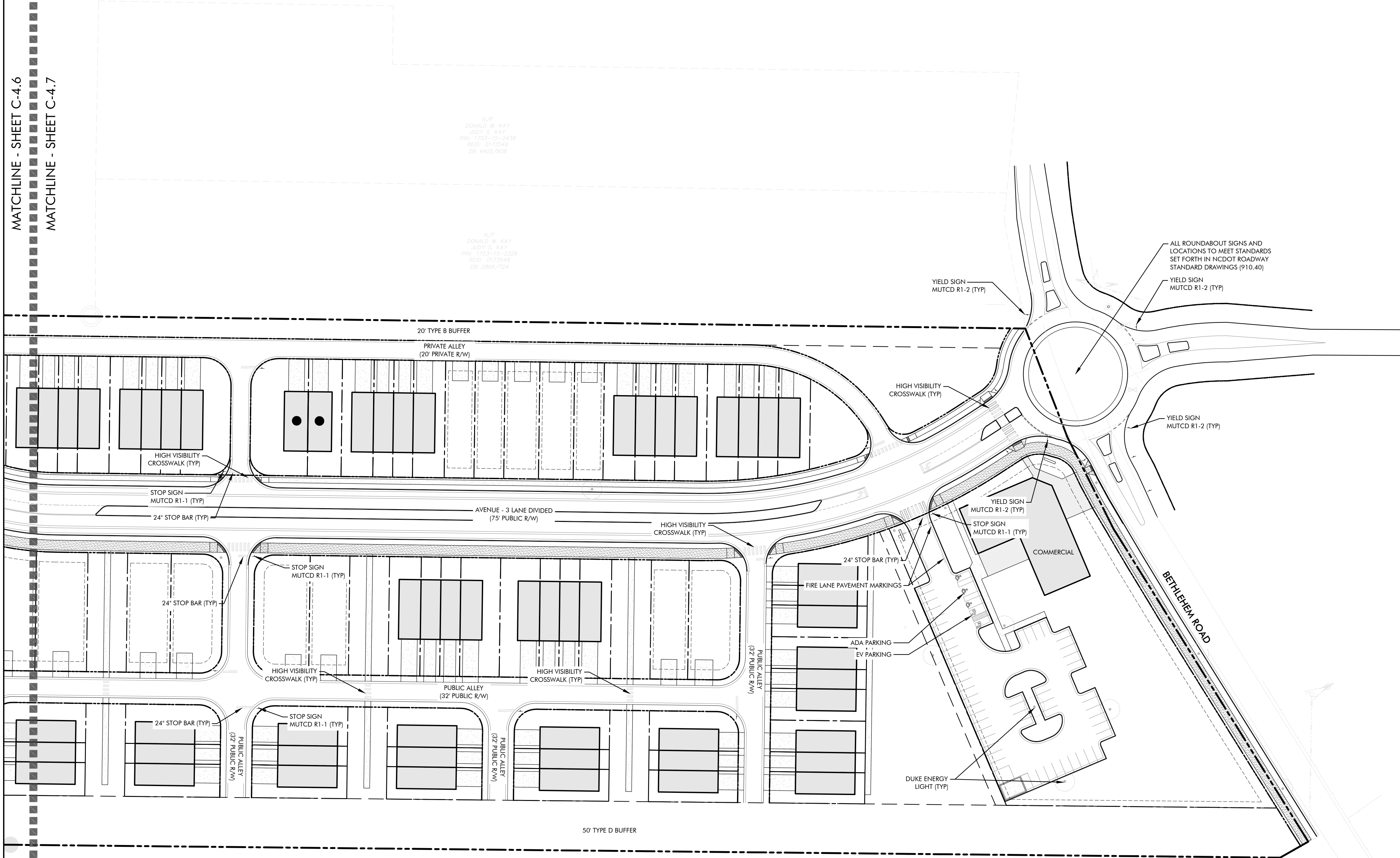
Project No: 22-RDU-083

Date: 05/03/2024

Sheet No:

C-4.6

MATCHLINE - SHEET C-4.6
 MATCHLINE - SHEET C-4.7



NOTES:
 1. ROUNDABOUT DESIGN TO BE FURTHER STUDIED AT TIME OF CONSTRUCTION DOCUMENTS.
 2. ROUNDABOUT TO MEET ALL NCDOT AND TOWN OF KNIGHTDALE STANDARDS.



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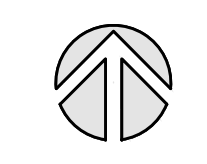
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LYNDON OAKS Master Plan Signage & Marking Plan Enlargement 7

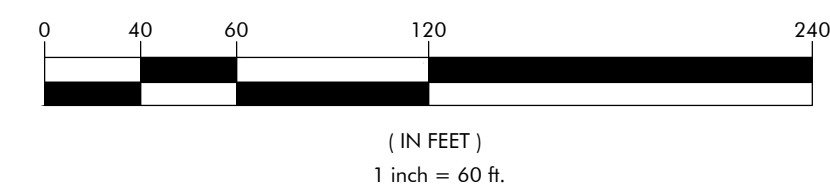
Knightsdale, North Carolina



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GRAPHIC SCALE



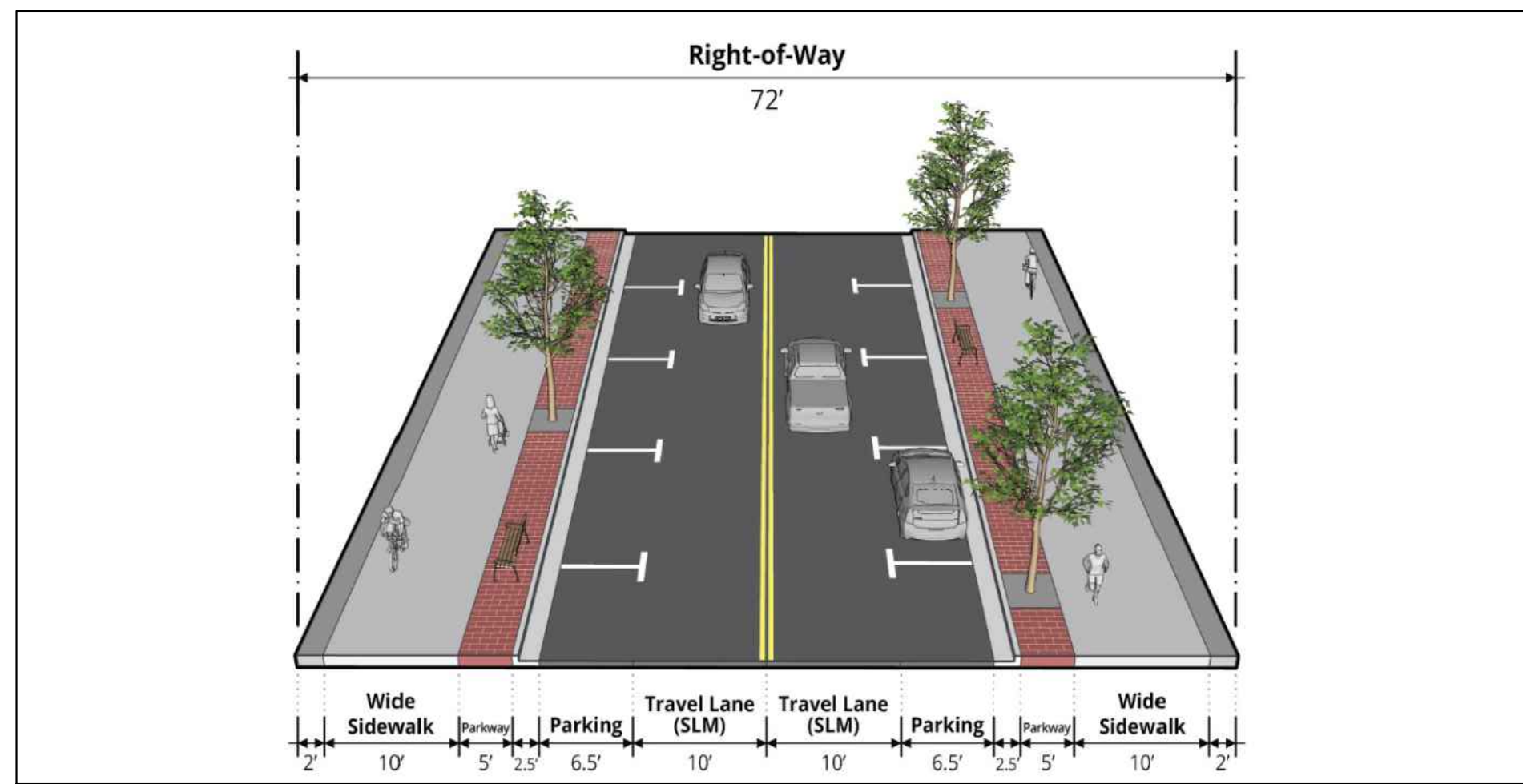
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#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083

Date: 05/03/2024

Sheet No:

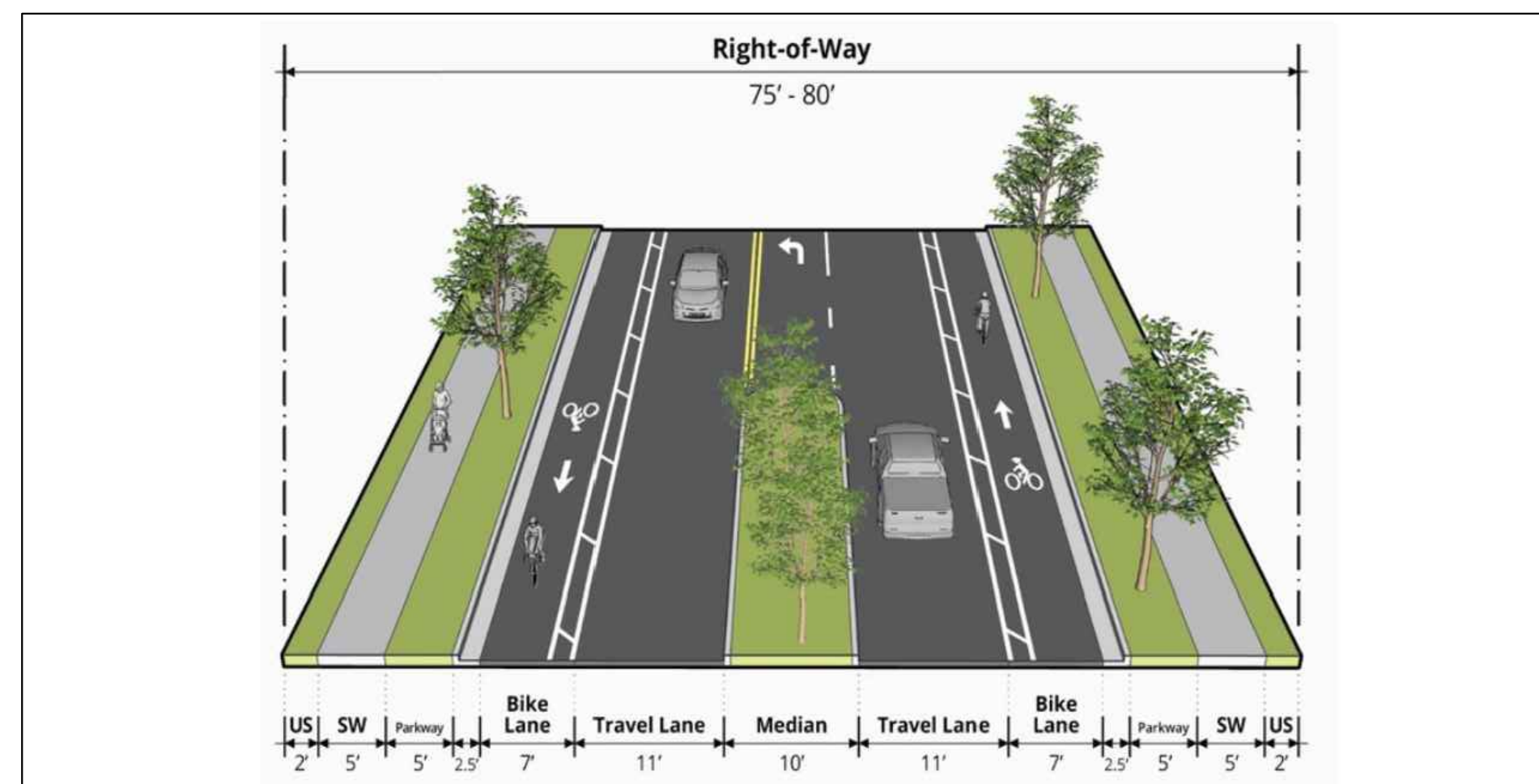
C-4.7



URBAN MAIN STREET - WIDE SIDEWALK

SCALE: NTS

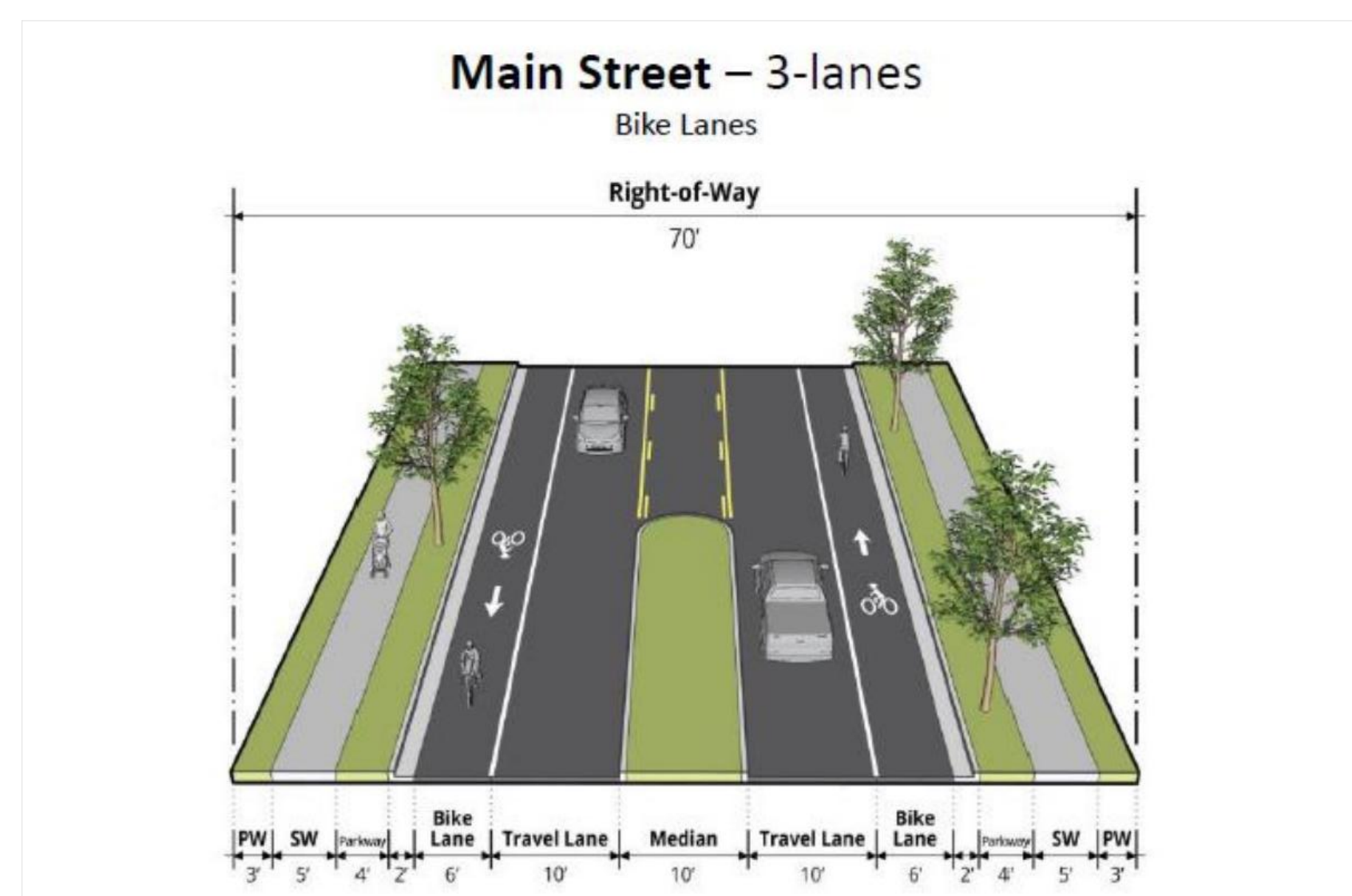
SECTION



AVENUE - BUFFERED BIKE LANES

SCALE: NTS

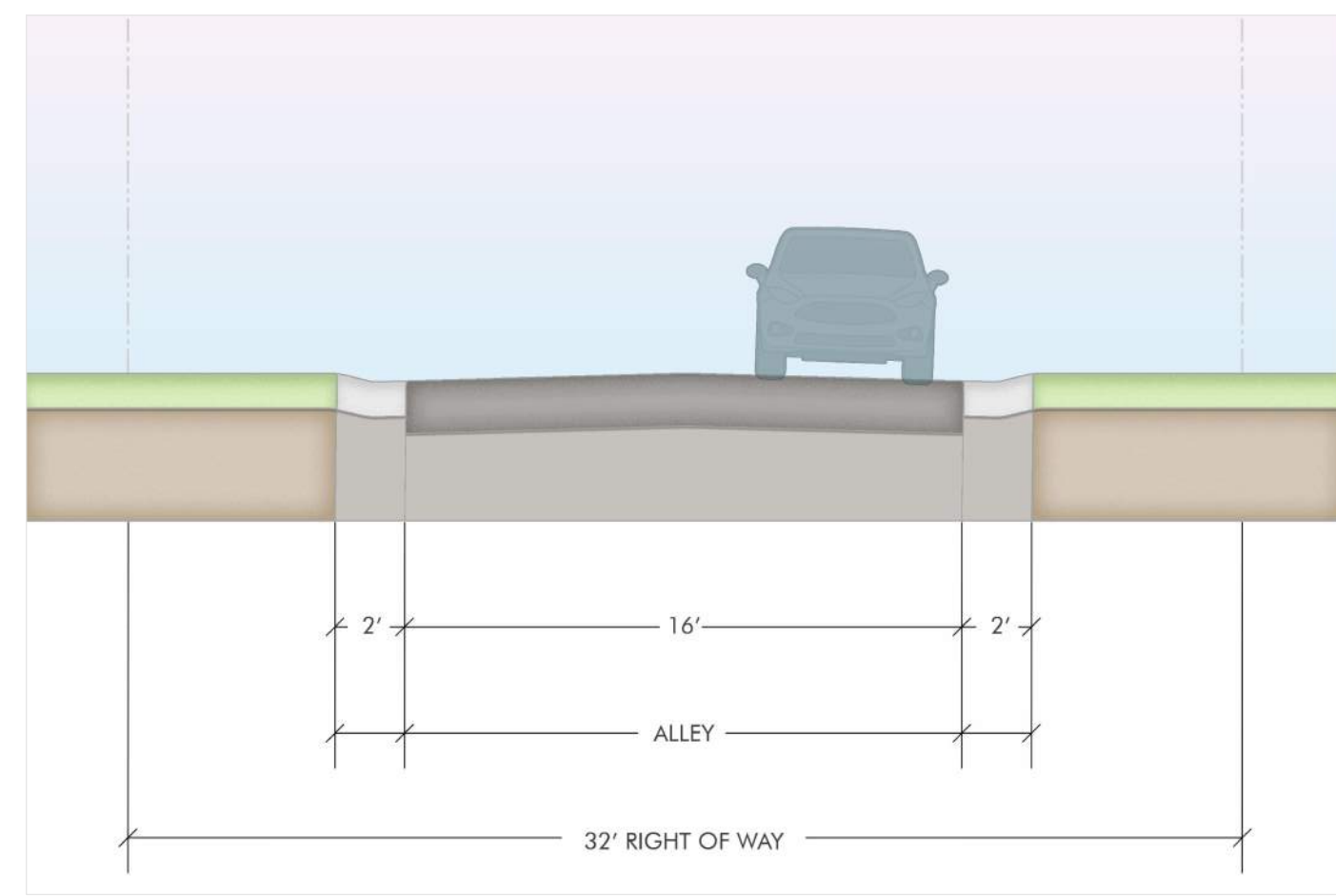
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70' MAIN STREET SECTION

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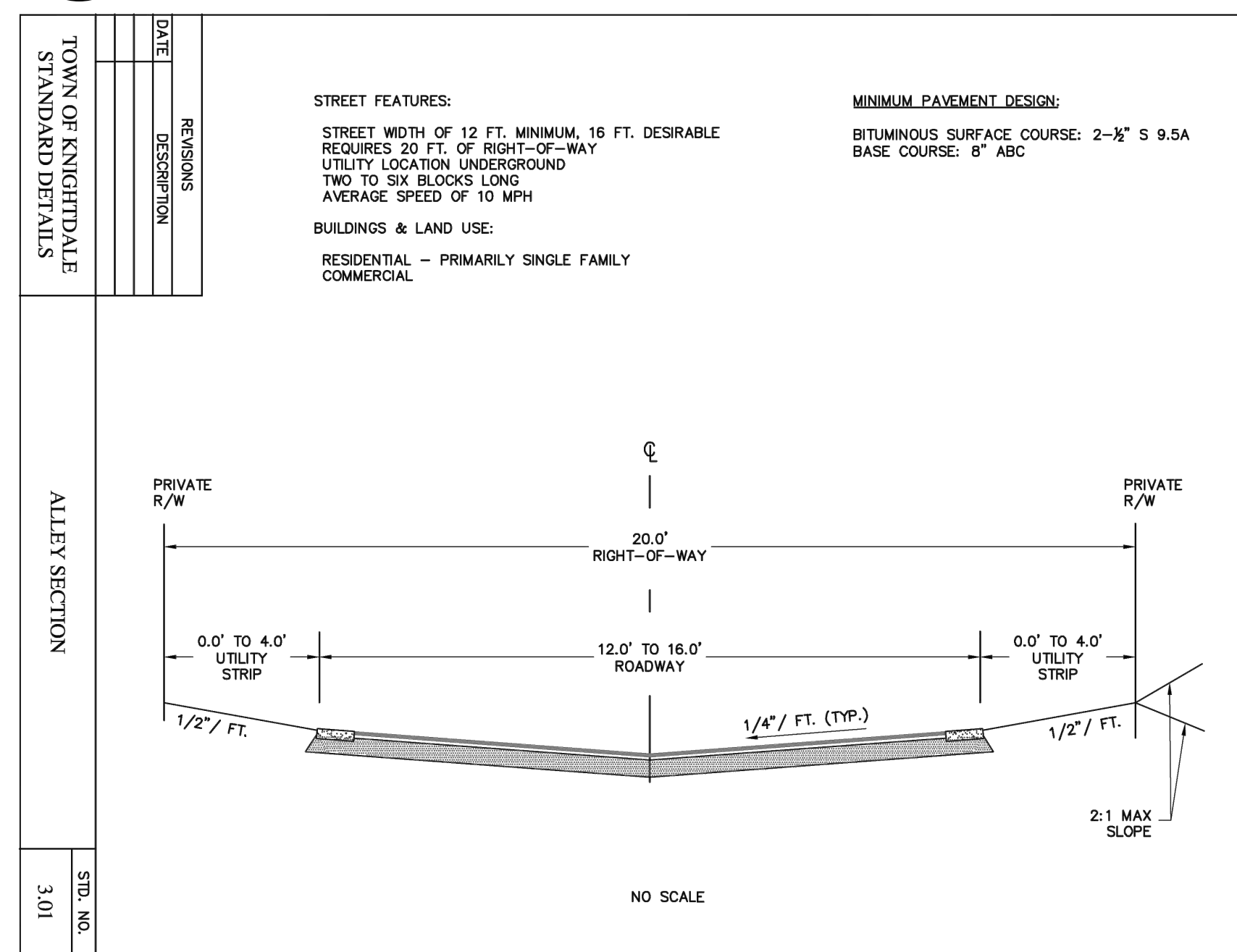
SECTION



32' PUBLIC ALLEY STREET SECTION

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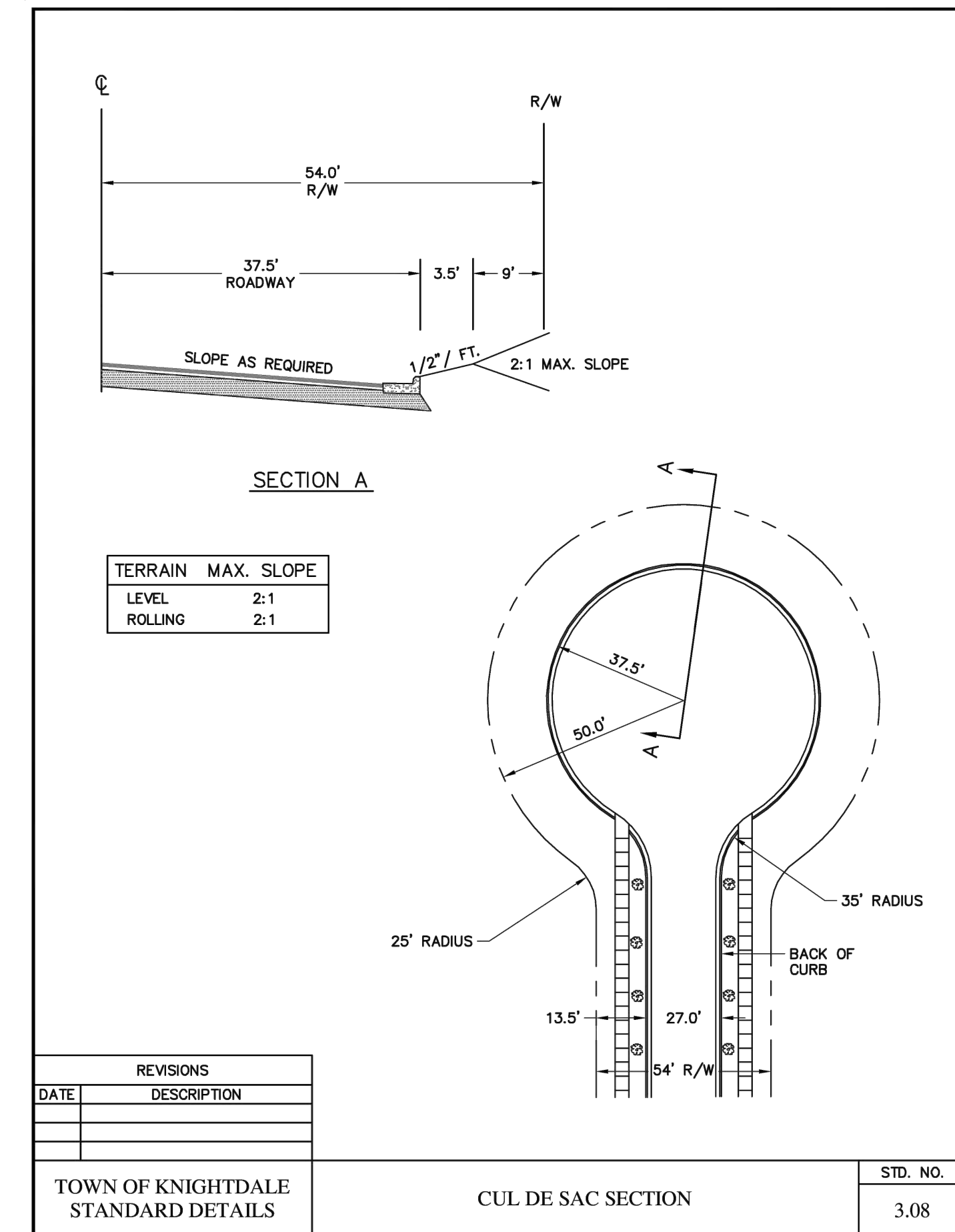
SECTION



20' PRIVATE ALLEY

SCALE: NTS

SECTION



CUL-DE-SAC

SCALE: NTS

SECTION



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+ 919.275.5022
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nc firm no: P-2671 sc cca no: C-03044

PRELIMINARY DRAWING
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05/03/2024

D.R. Horton

7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

LYNDON OAKS
Master Plan
Street Sections
Knightdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

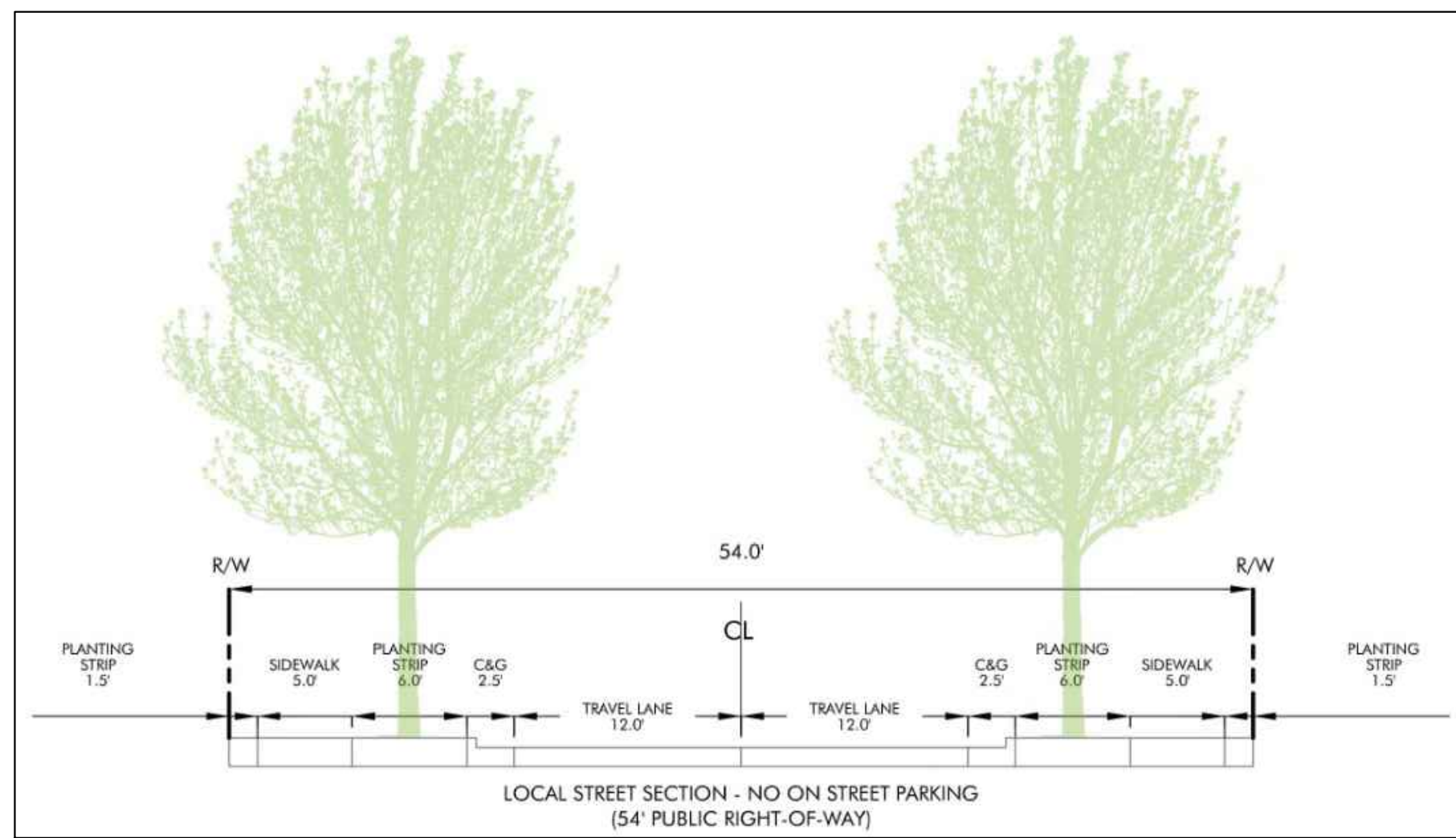
Project No: 22-RDU-083

Date: 05/03/2024

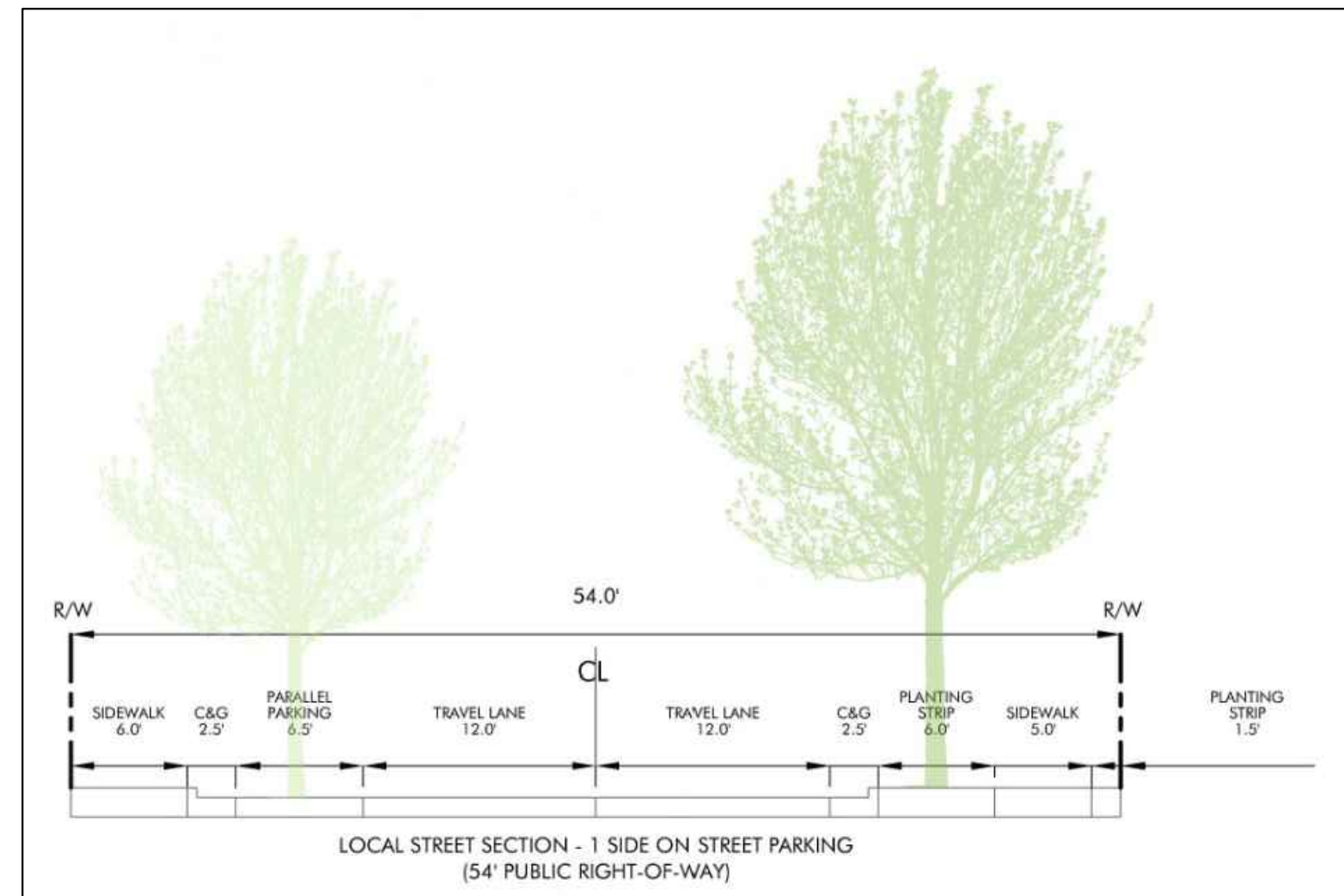
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C-4.8

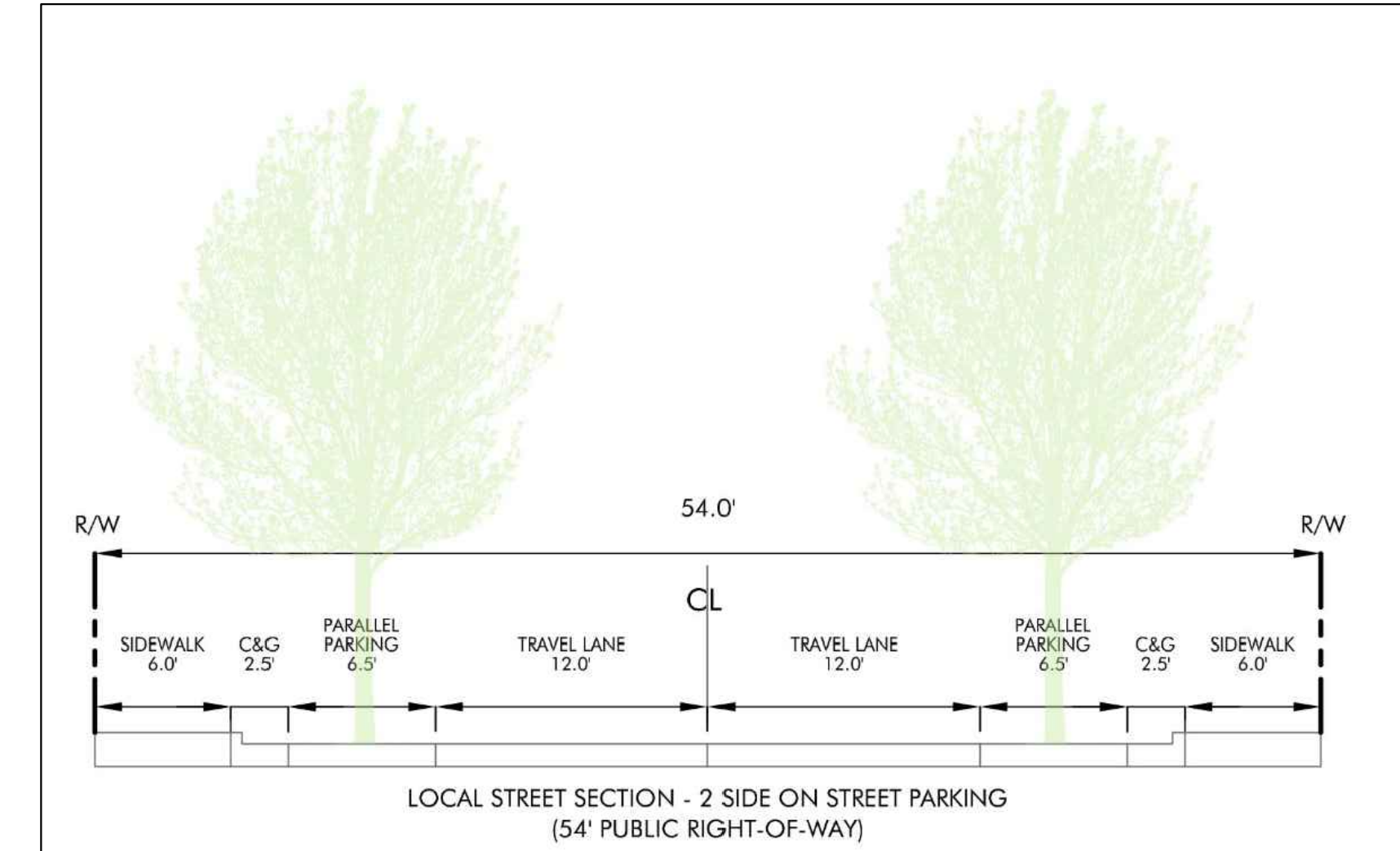
NOTE:
TOWN OF KNIGHTDALE IS CURRENTLY UPDATING THEIR STANDARD DETAILS AND THE STREET SECTIONS SHOWN ON TOWN OF KNIGHTDALE TITLEBLOCKS WILL BE UPDATED TO MEET NEW REQUIREMENTS PER THE UDO.
STREET SECTIONS USED IN PLAN REFLECT THE DIMENSIONS SET FORTH IN THE UDO.



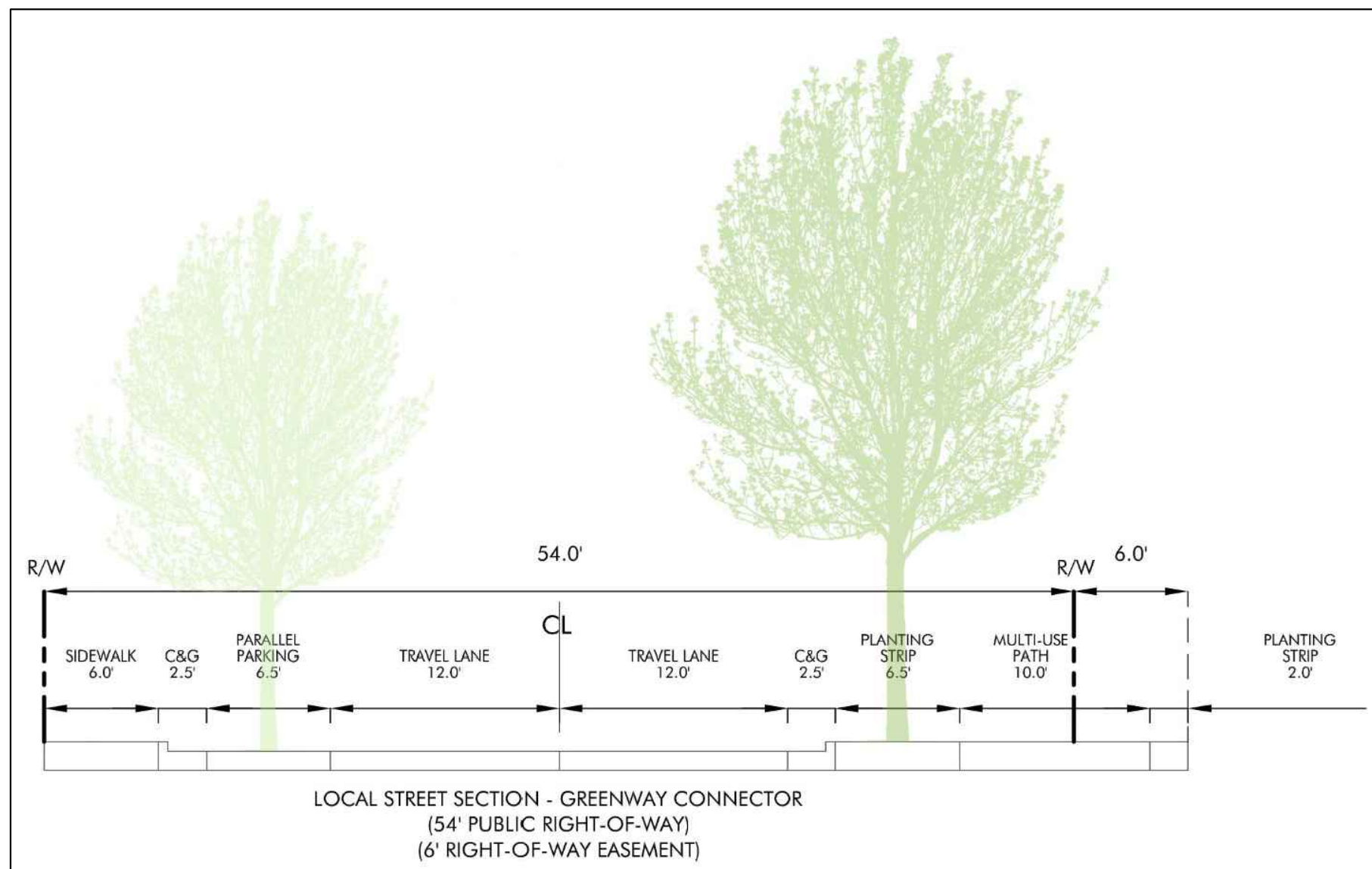
LOCAL STREET PARKING - NO ON STREET PARKING SECTION
SCALE: NTS



LOCAL STREET PARKING - 1 SIDE ON STREET PARKING SECTION
SCALE: NTS



LOCAL STREET PARKING - 2 SIDE ON STREET PARKING SECTION
SCALE: NTS



LOCAL STREET PARKING - GREENWAY CONNECTOR SECTION
SCALE: NTS

NOTE:
TOWN OF KNIGHTDALE IS CURRENTLY UPDATING THEIR STANDARD DETAILS AND THE STREET SECTIONS SHOWN ON TOWN OF KNIGHTDALE TITLEBLOCKS WILL BE UPDATED TO MEET NEW REQUIREMENTS PER THE UDO.
STREET SECTIONS USED IN PLAN REFLECT THE DIMENSIONS SET FORTH IN THE UDO.



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LYNDON OAKS
Master Plan
Street Sections
Knightdale, North Carolina

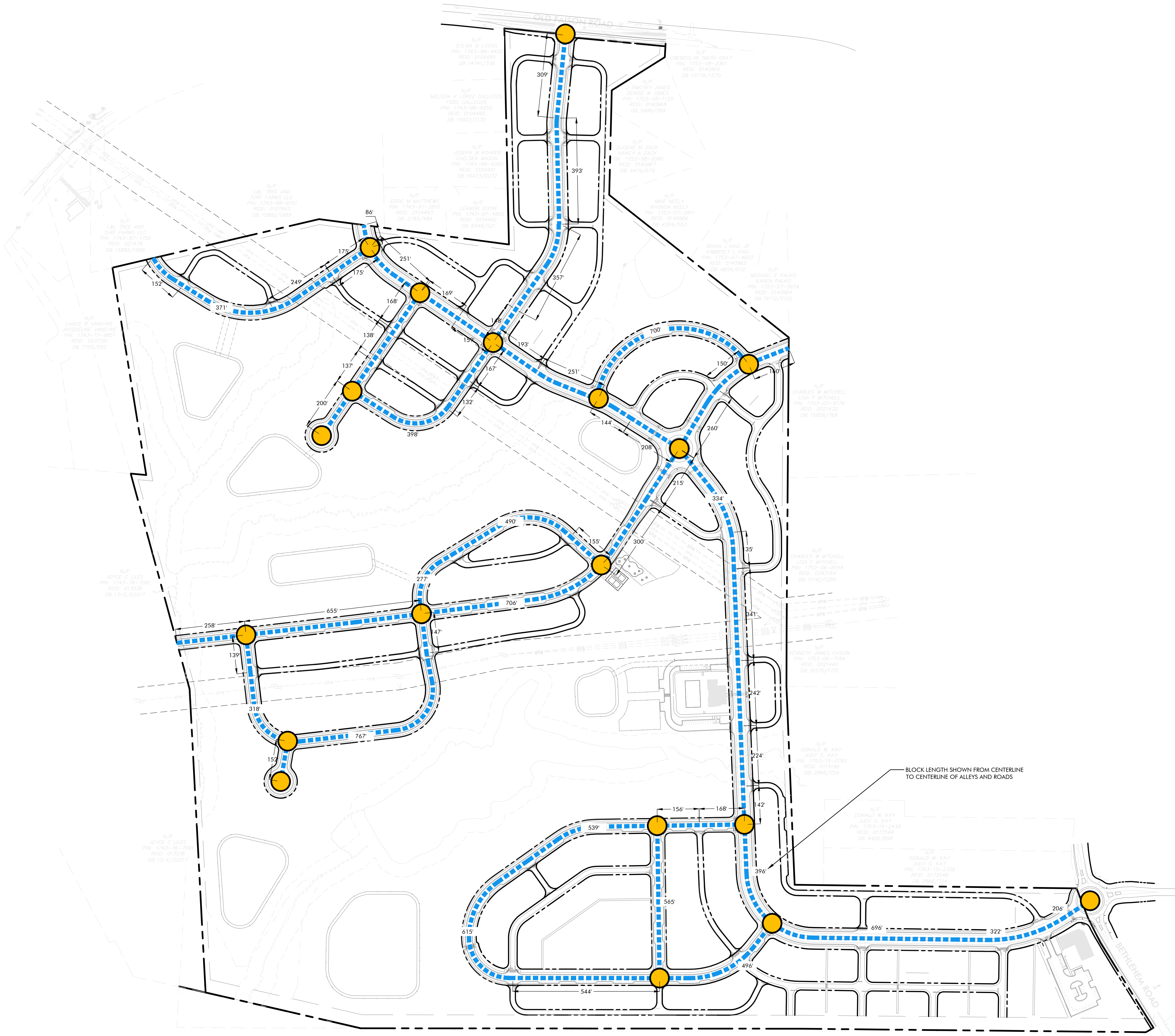
NO.	DATE	BY	REVISIONS:
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#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083

Date: 05/03/2024

Sheet No:

C-4.81



CONNECTIVITY INDEX

LINKS: 28

NODES: 19

MINIMUM REQUIRED: 1.40
 PROVIDED: 1.47
 $\frac{28}{19} = 1.47$

* CONNECTIVITY INDEX IS CALCULATED ACCORDING TO TOWN OF KNIGHTDALE LDD SEC. 11.3.E.7



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 Raleigh, NC 27601
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 FOR REVIEW BY PROFESSIONAL

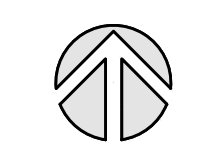
05/03/2024

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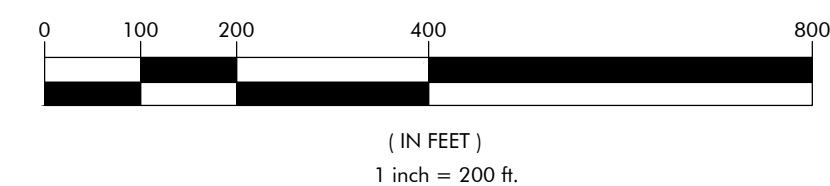
LYNDON OAKS
Master Plan
 Connectivity Index
 Knightdale, North Carolina



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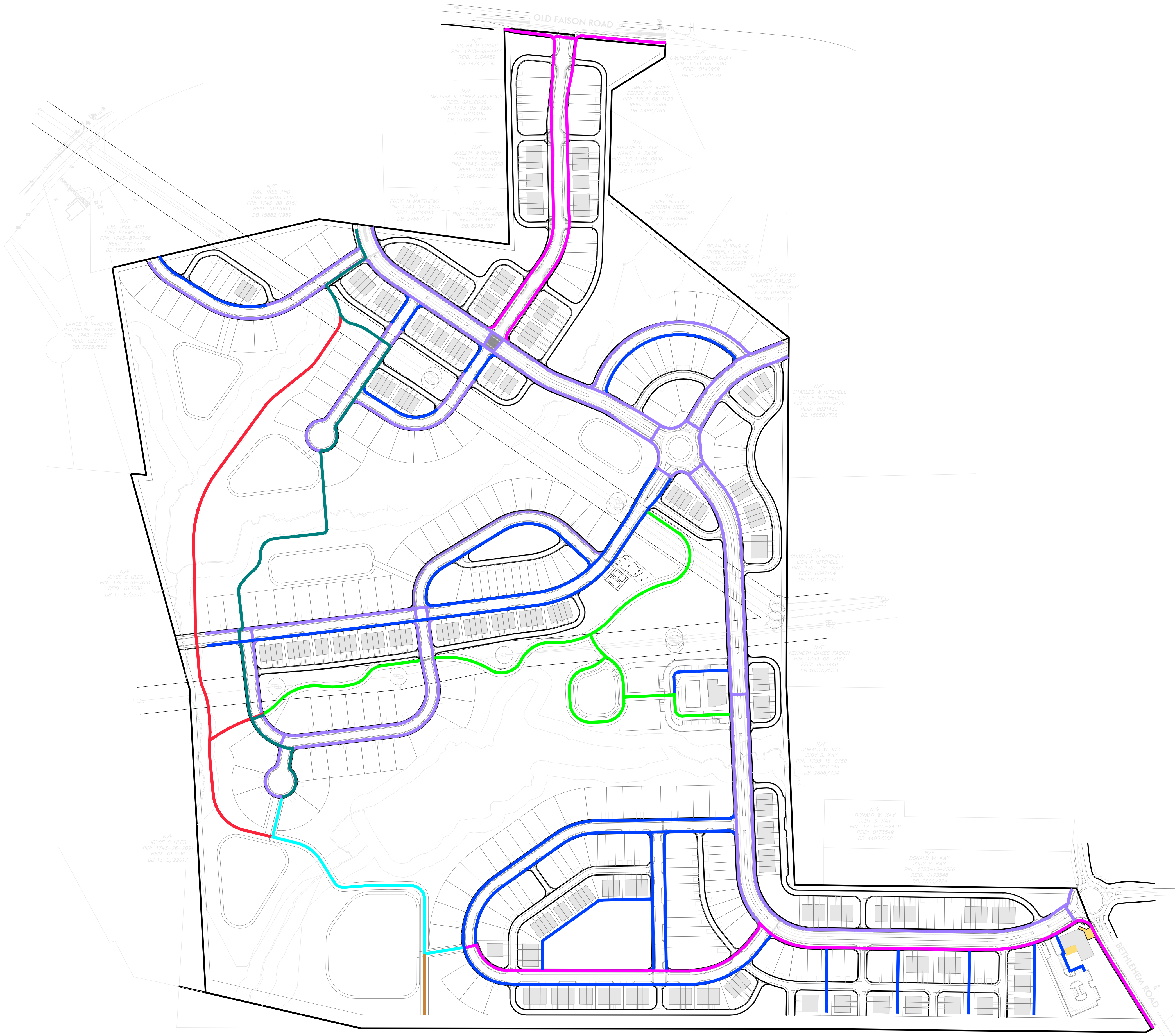
GRAPHIC SCALE



NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
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#3	03/28/2024		

Project No: 22-RDU-083
 Date: 05/03/2024

Sheet No:
C-4.9



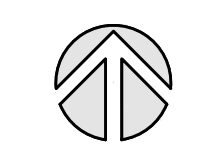
PEDESTRIAN CIRCULATION

- 10' ASPHALT SIDEWALK
- 5' CONCRETE SIDEWALK*
- 5' CONCRETE SIDEWALK*
- 10' PRIVATE TRAIL
- 10' PRIVATE TRAIL W/PUBLIC ACCESS
- 10' PUBLIC TRAIL OPTION A
- 10' PUBLIC TRAIL OPTION B
- TRAIL STUB FOR FUTURE CONNECTION
- PEDESTRIAN PLAZA

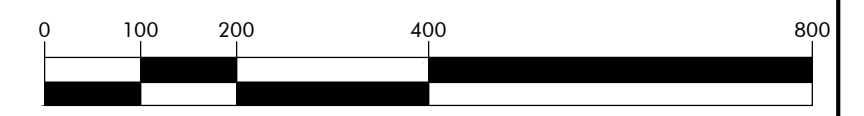
*NOTE: STREET SECTIONS CALL FOR 5' WIDE CONCRETE SIDEWALK, BUT WHERE SIDEWALK ABUTS CURB ADJACENT TO PARALLEL PARKING SPACES, SIDEWALK SHALL BE 6' WIDE.



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GRAPHIC SCALE



(IN FEET)
1 inch = 200 ft.



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Raleigh, NC 27615

LYNDON OAKS Master Plan

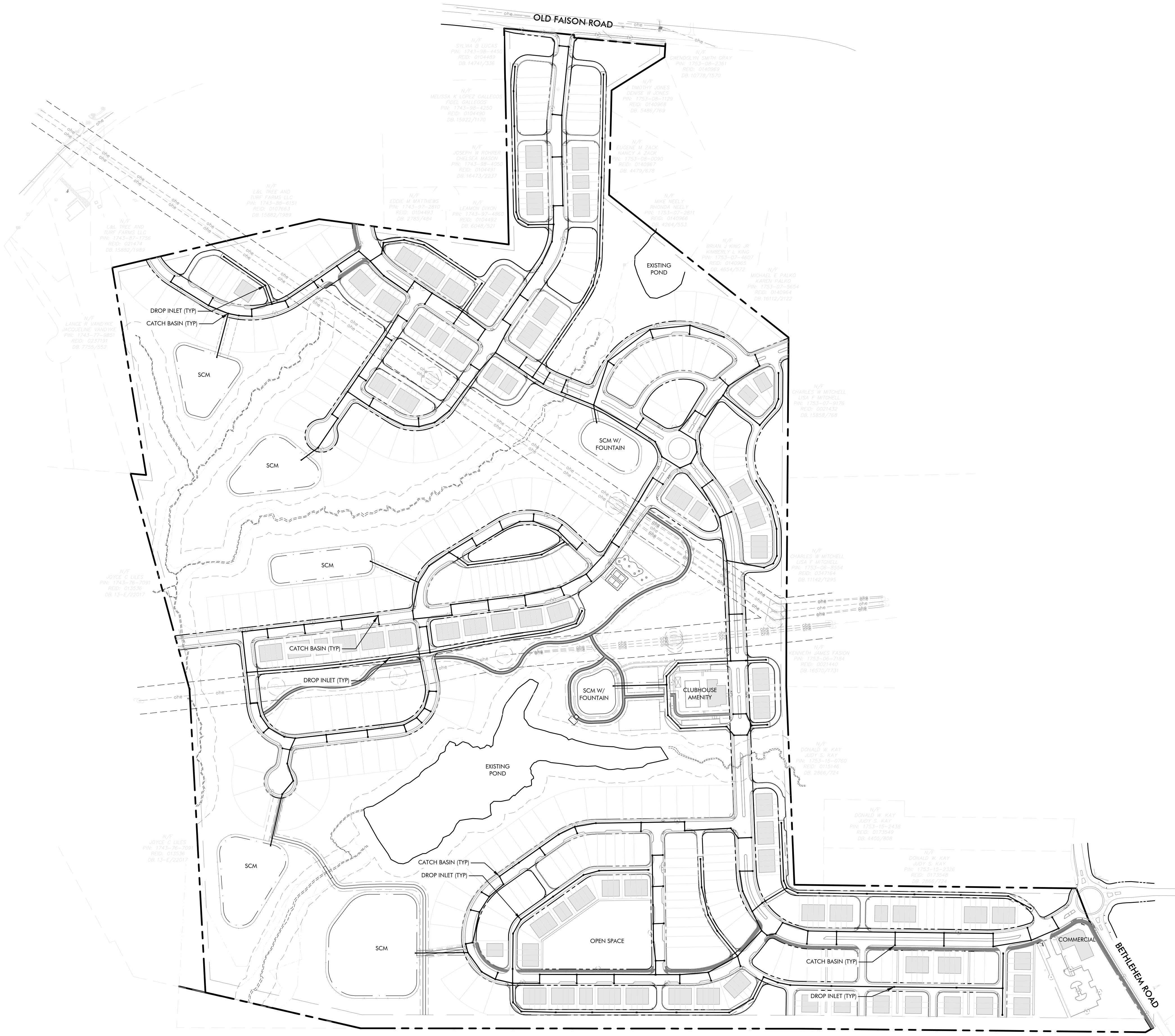
Pedestrian Circulation
Knightdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

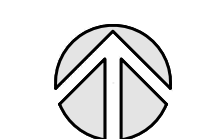
Project No: 22-RDU-083
Date: 05/03/2024

Sheet No:

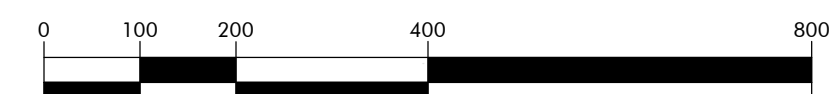
C-4.10



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GRAPHIC SCALE



(IN FEET)
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LYNDON OAKS Master Plan

Overall Storm Drainage Plan

Knightsdale, North Carolina

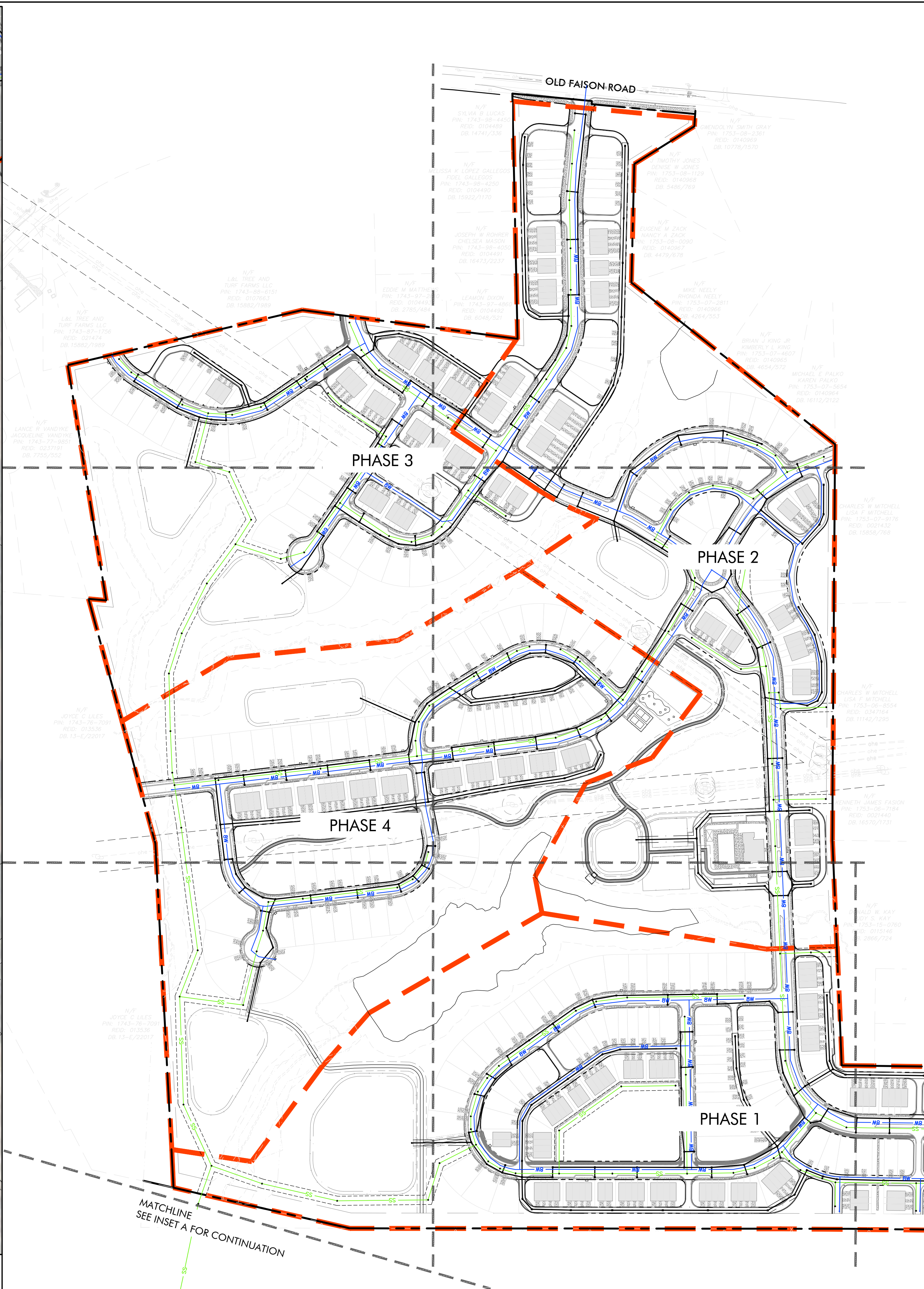
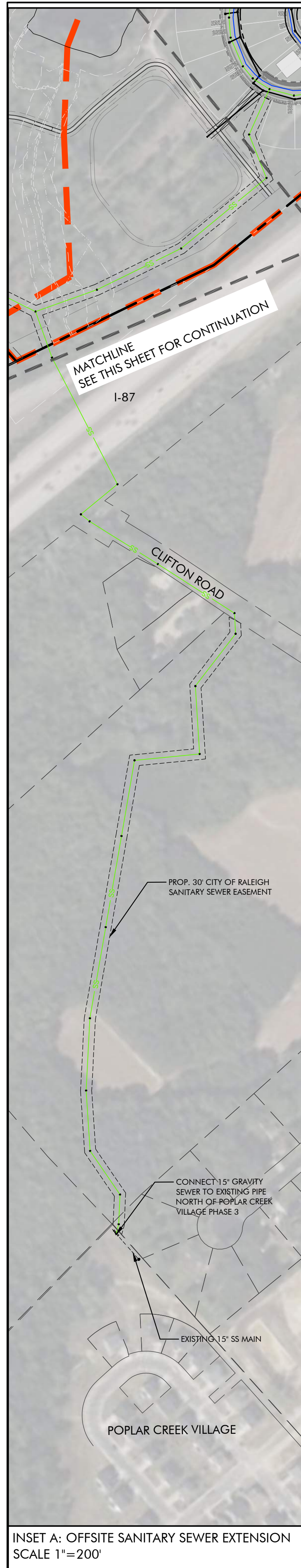
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#2	02/05/2024		
#3	03/26/2024		

Project No: 22-RDU-083

Date: 05/03/2024

Sheet No:

C-5.0



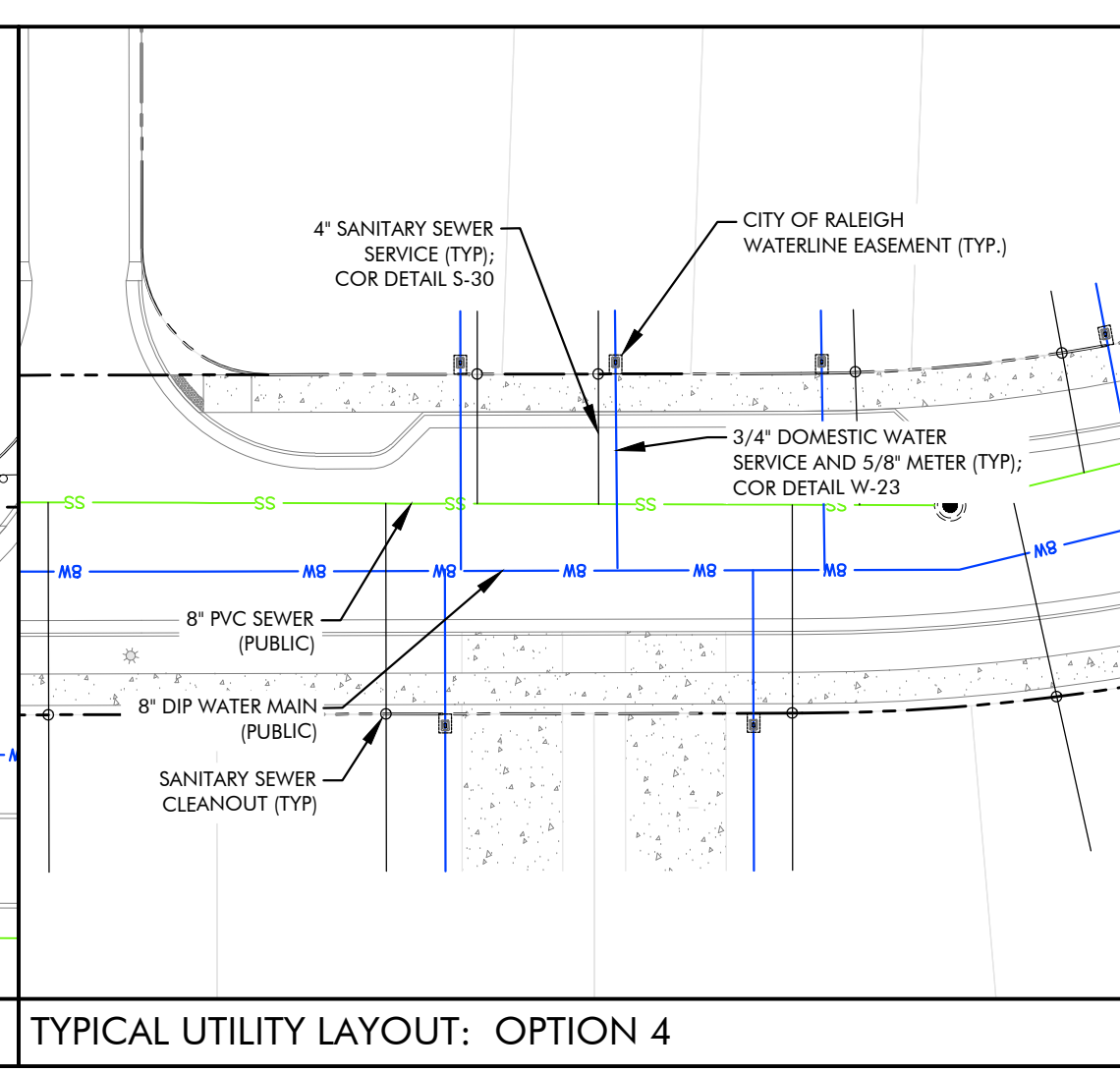
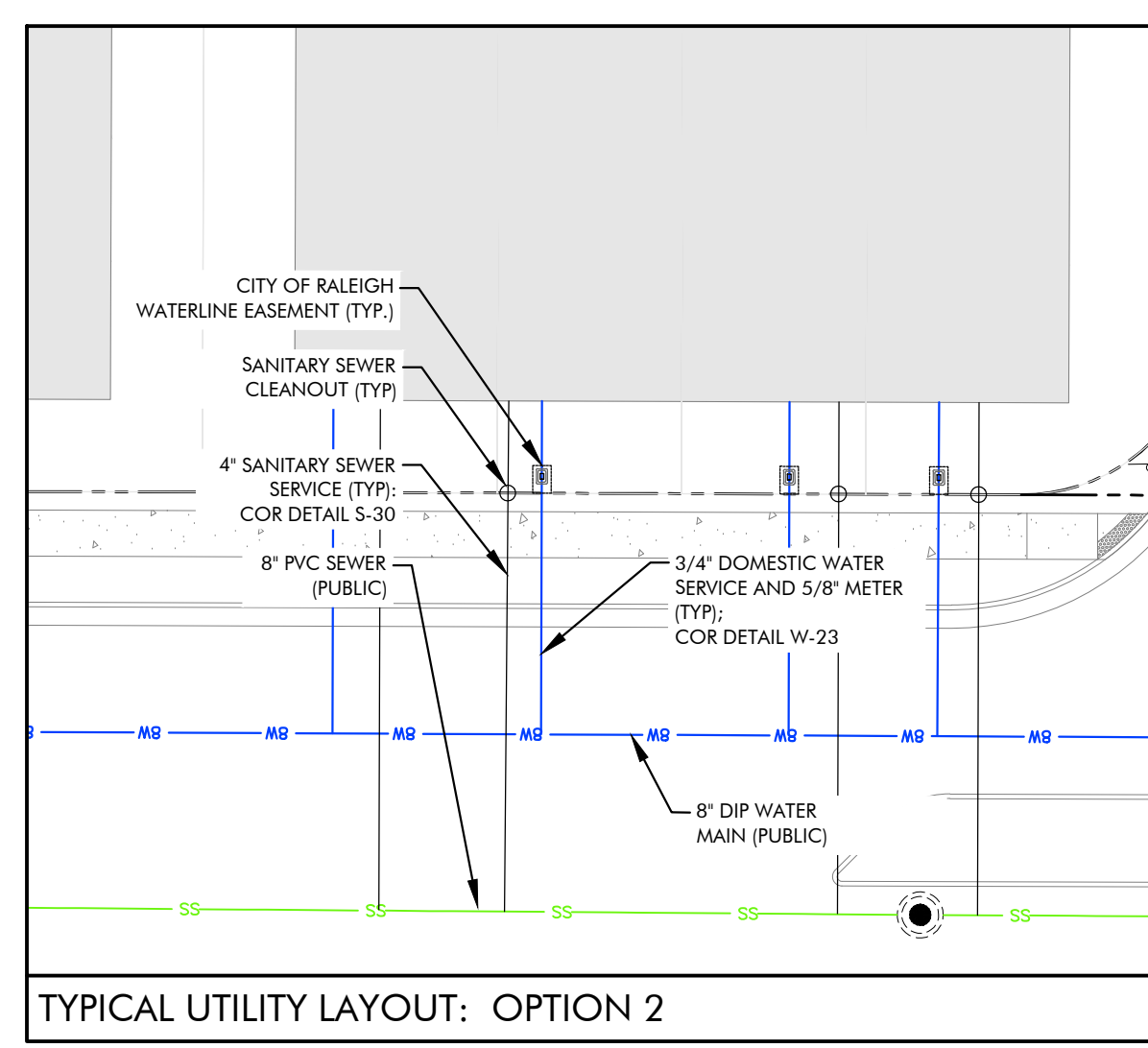
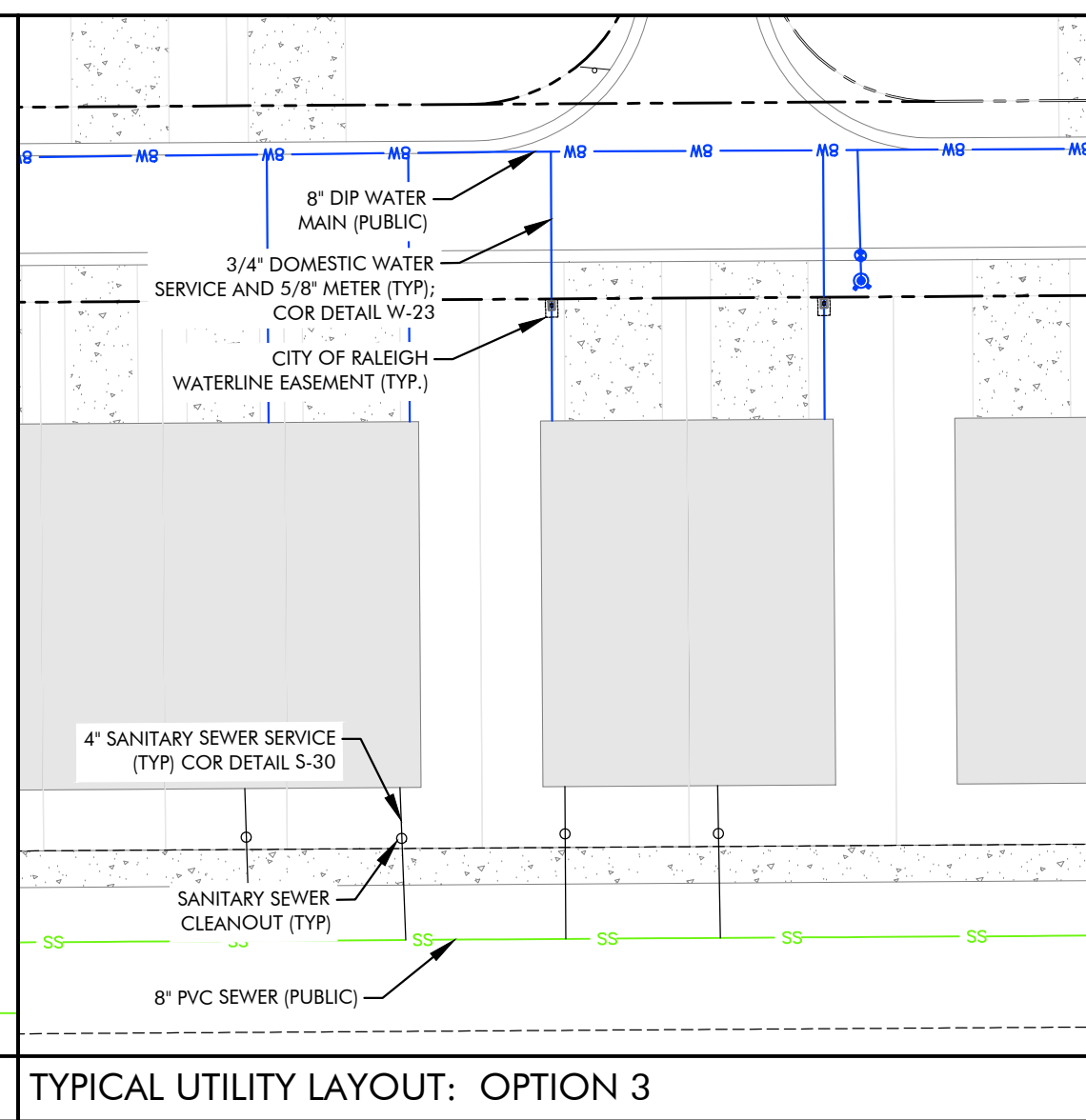
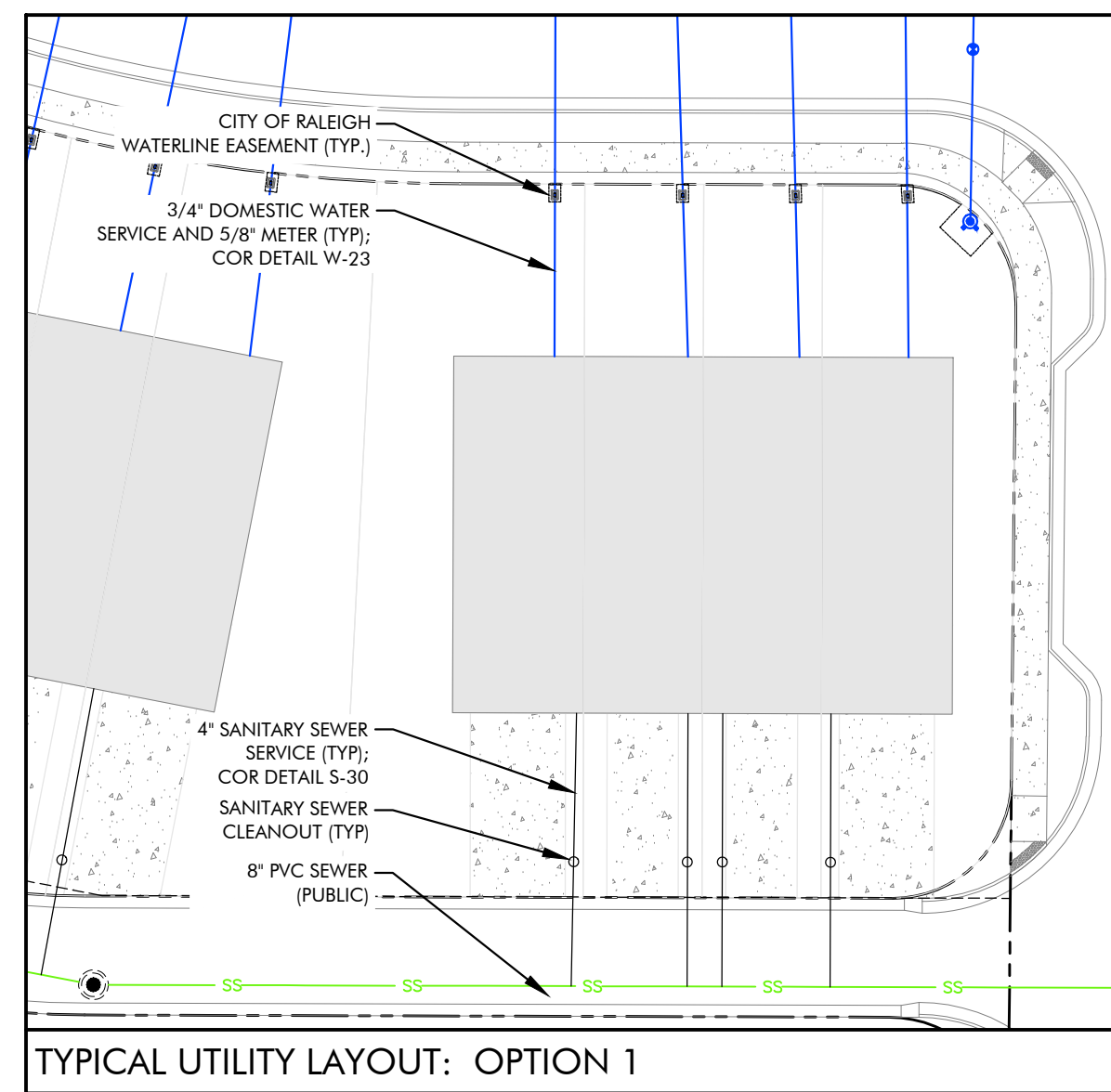
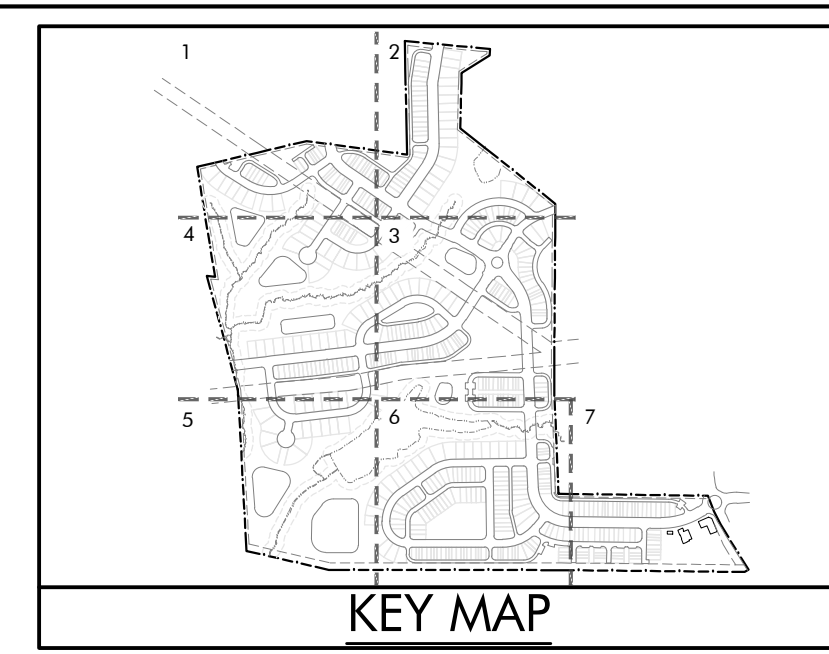
NOTES

SANITARY SEWER LINES WILL MAINTAIN AT LEAST 10' OF HORIZONTAL SEPARATION FROM WATER LINES

WATER LINES WILL MAINTAIN AT LEAST 5' OF HORIZONTAL SEPARATION FROM STORM LINES

STORM LINES WILL MAINTAIN AT LEAST 5' OF HORIZONTAL SEPARATION FROM SANITARY SEWER LINES

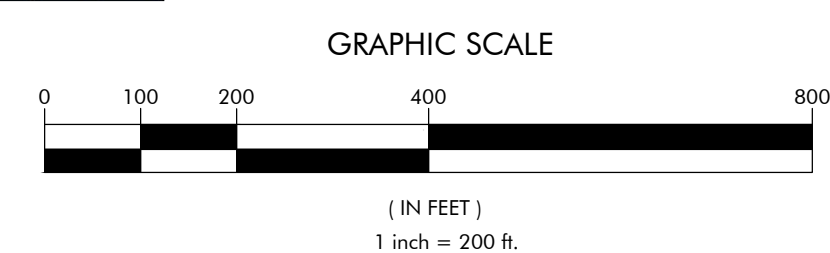
FIRE HYDRANTS AND ASSOCIATED CONNECTIONS WILL FOLLOW THE STANDARDS SET FORTH IN THE CITY OF RALEIGH PUBLIC UTILITIES HANDBOOK



WATER ALLOCATION POLICY

MAJOR SUBDIVISION	PTS.
MAJOR SUBDIVISION BASE POINTS	15 pts.
CONSERVATION OF NATURAL HABITAT	7 pts.
CONSTRUCT A FOUNTAIN WITHIN THE BMP	4 pts.
ON-STREET PARKING	4 pts.
RESIDENTIAL ARCHITECTURAL STANDARDS	15 pts.
3,000' OR MORE OF A 6-FOOT WIDE PATH	3 pts.
RESORT STYLE POOL	2 pts.
DECK/PATIO (2,000 SF MIN.)	2 pts.
CLUBHOUSE (NO MEETING SPACE, BATHROOMS & CHANGING ROOMS ONLY)	3 pts.
IFEMA CERTIFIED PLAYGROUND EQUIPMENT	4 pts.
RICKLEBALL COURTS	5 pts.
OUTDOOR DISPLAY OF PUBLIC ART	4 pts.
TOTAL	68 pts.

SINGLE-USE RETAIL	PTS.
SINGLE USE RETAIL BASE POINTS	41 pts.
CONSTRUCTION OF GATEWAY/LANDSCAPING	5 pts.
DECK/PATIO (2,000 SF MIN.)	2 pts.
OUTDOOR DISPLAY OF PUBLIC ART	4 pts.
TOTAL	52 pts.



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nc firm no: P-2671 sc coo no: C-03044

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05/03/2024

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LYNDON OAKS

Master Plan

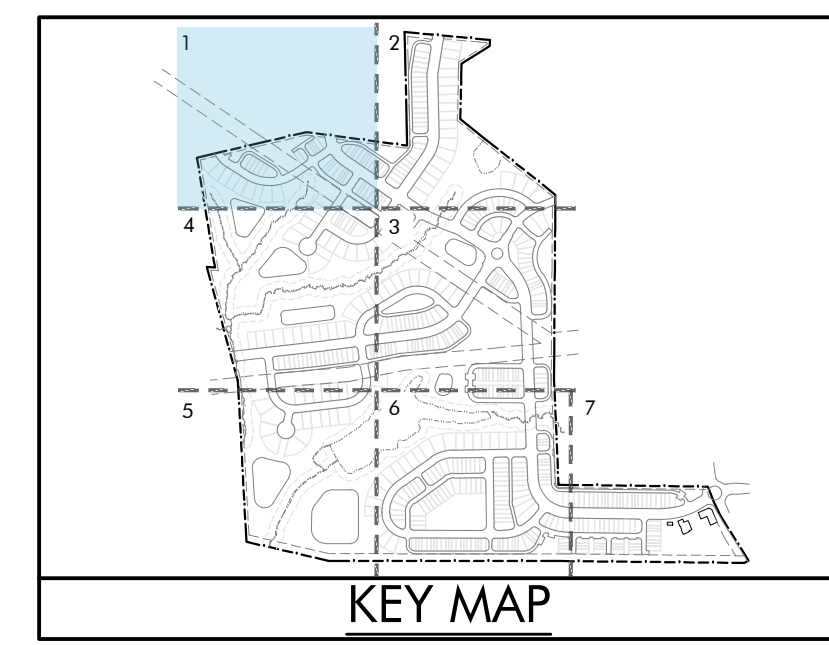
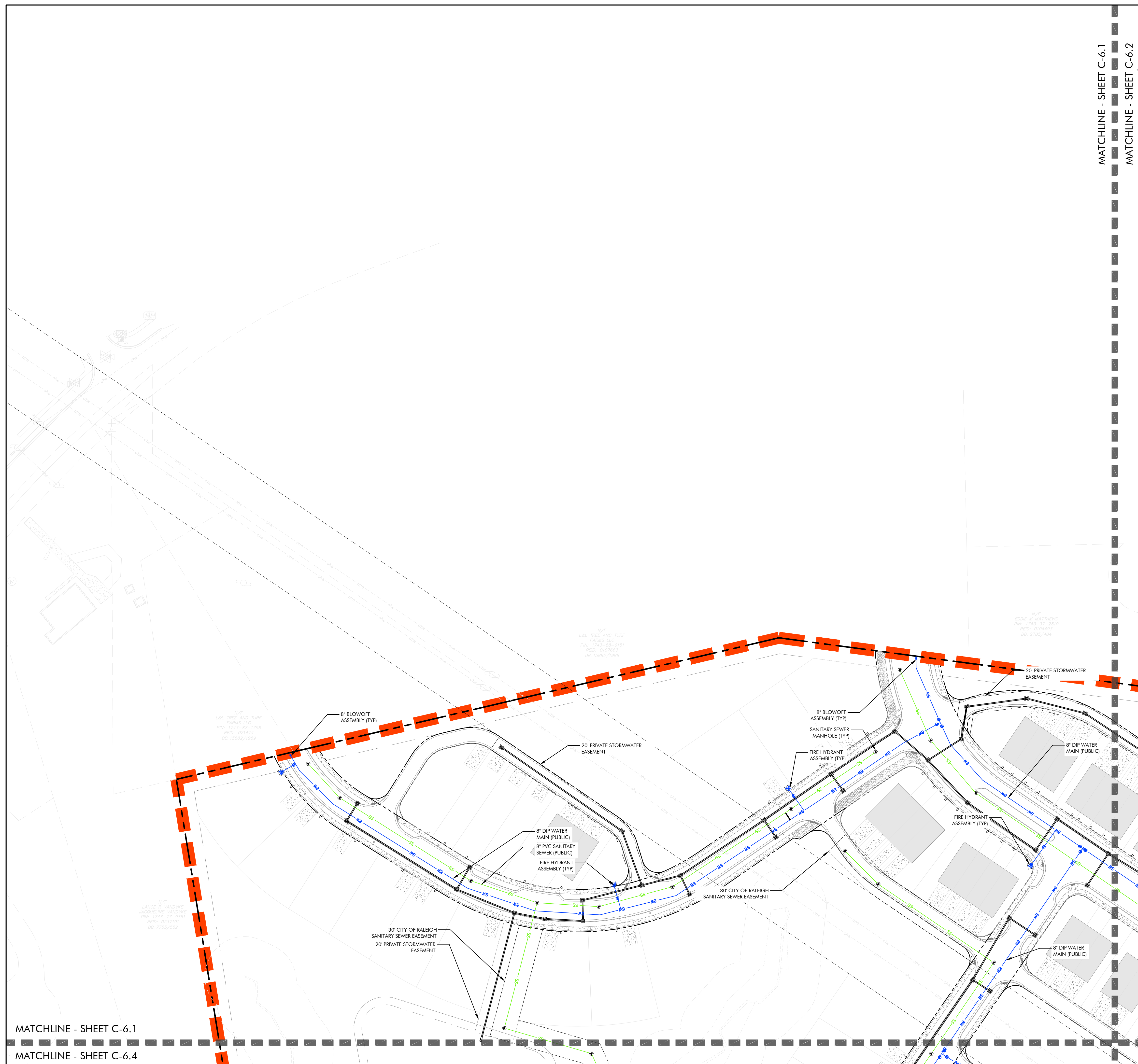
Overall Utility Plan

Knightsdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	09/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083
Date: 05/03/2024
Sheet No:

C-6.0



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Raleigh, NC 27615

- STANDARD UTILITY NOTES (AS APPLICABLE):**
- ALL MATERIALS & CONSTRUCTION METHODS SHALL BE IN ACCORDANCE WITH CITY OF RALEIGH DESIGN STANDARDS, DETAILS & SPECIFICATIONS (REFERENCE: CORPUD HANDBOOK, CURRENT EDITION)
 - UTILITY SEPARATION REQUIREMENTS:
 - a) A DISTANCE OF 10' SHALL BE MAINTAINED BETWEEN SANITARY SEWER & ANY PRIVATE OR PUBLIC WATER SUPPLY SOURCE SUCH AS AN IMPOUNDED RESERVOIR USED AS A SOURCE OF DRINKING WATER. IF ADEQUATE LATERAL SEPARATION CANNOT BE ACHIEVED, FERROUS SANITARY SEWER PIPE SHALL BE SPECIFIED & INSTALLED TO WATERLINE SPECIFICATIONS. HOWEVER, THE MINIMUM SEPARATION SHALL NOT BE LESS THAN 2' FROM A PRIVATE WELL OR 5' FROM A PUBLIC WELL
 - b) WHEN INSTALLING WATER &/OR SEWER MAINS, THE HORIZONTAL SEPARATION BETWEEN UTILITIES SHALL BE 10'. IF THIS SEPARATION CANNOT BE MAINTAINED DUE TO EXISTING CONDITIONS, THE VARIATION ALLOWED IS THE WATER MAIN IN A SEPARATE TRENCH WITH THE ELEVATION OF THE WATER MAIN AT LEAST 18" ABOVE THE TOP OF THE SEWER AND MUST BE APPROVED BY THE PUBLIC UTILITIES DIRECTOR. ALL DISTANCES ARE MEASURED FROM OUTSIDE DIAMETER TO OUTSIDE DIAMETER
 - c) WHERE IT IS IMPOSSIBLE TO OBTAIN PROPER SEPARATION, OR ANYTIME A SANITARY SEWER PASSES OVER A WATERMAIN, DIP MATERIALS OR STEEL ENCASEMENT EXTENDED 10' ON EACH SIDE OF CROSSING MUST BE SPECIFIED & INSTALLED TO WATERLINE SPECIFICATIONS
 - d) 5.0' MINIMUM HORIZONTAL SEPARATION IS REQUIRED BETWEEN ALL SANITARY SEWER & STORM SEWER FACILITIES, UNLESS DIP MATERIAL IS SPECIFIED FOR SANITARY SEWER
 - e) MAINTAIN 18" MIN. VERTICAL SEPARATION AT ALL WATERMAIN & RCP STORM DRAIN CROSSINGS; MAINTAIN 18" MIN. VERTICAL SEPARATION AT ALL SANITARY SEWER & RCP STORM DRAIN CROSSINGS. WHERE ADEQUATE SEPARATIONS CANNOT BE ACHIEVED, SPECIFY DIP MATERIALS & A CONCRETE CRADLE HAVING 6" MIN. CLEARANCE (PER CORPUD DETAILS W-41 & S-49)
 - f) ALL OTHER UNDERGROUND UTILITIES SHALL CROSS WATER & SEWER FACILITIES WITH 18" MIN. VERTICAL SEPARATION REQUIRED.
 - ANY NECESSARY FIELD REVISIONS ARE SUBJECT TO REVIEW & APPROVAL OF AN AMENDED PLAN &/OR PROFILE BY THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT PRIOR TO CONSTRUCTION
 - DEVELOPER SHALL PROVIDE 30 DAYS ADVANCE WRITTEN NOTICE TO OWNER FOR ANY WORK REQUIRED WITH AN EXISTING CITY OF RALEIGH UTILITY EASEMENT TRAVERSING PRIVATE PROPERTY.
 - CONTRACTOR SHALL MAINTAIN CONTINUOUS WATER & SEWER SERVICE TO EXISTING RESIDENCES & BUSINESSES THROUGHOUT CONSTRUCTION OF PROJECT. ANY NECESSARY SERVICE INTERRUPTIONS SHALL BE PRECEDED BY A 24 HOUR ADVANCE NOTICE TO THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT
 - 3.0' MINIMUM COVER IS REQUIRED ON ALL WATER MAINS & SEWER FORCEMAINS. 4.0' MINIMUM COVER IS REQUIRED ON ALL REUSE MAINS
 - IT IS THE DEVELOPER'S RESPONSIBILITY TO ABANDON OR REMOVE EXISTING WATER & SEWER SERVICES NOT BEING USED IN REDEVELOPMENT OF A SITE UNLESS OTHERWISE DIRECTED BY THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT. THIS INCLUDES ABANDONING TAP AT MAIN & REMOVAL OF SERVICE FROM ROW OR EASEMENT PER CORPUD HANDBOOK PROCEDURE.
 - INSTALL 3/4" COPPER WATER SERVICES WITH METERS LOCATED AT ROW OR WITHIN A 2'X2' WATERLINE EASEMENT IMMEDIATELY ADJACENT. NOTE: IT IS THE APPLICANT'S RESPONSIBILITY TO PROPERLY SIZE THE WATER SERVICE FOR EACH CONNECTION TO PROVIDE ADEQUATE FLOW & PRESSURE.
 - INSTALL 4" PVC SEWER SERVICES @ 1.0% MINIMUM GRADE WITH CLEANOUTS LOCATED AT ROW OR EASEMENT LINE & SPACED EVERY 75 LINEAR FEET MAXIMUM
 - PRESSURE REDUCING VALVES ARE REQUIRED ON ALL WATER SERVICES EXCEEDING 80 PSI; BACKWATER VALVES ARE REQUIRED ON ALL SANITARY SEWER SERVICES HAVING BUILDING DRAINS LOWER THAN 1.0' ABOVE THE NEXT UPSTREAM MANHOLE
 - ALL ENVIRONMENTAL PERMITS APPLICABLE TO THE PROJECT MUST BE OBTAINED FROM NCDWQ, USACE &/OR FEMA FOR ANY RIPARIAN BUFFER, WETLAND &/OR FLOODPLAIN IMPACTS (RESPECTIVELY) PRIOR TO CONSTRUCTION.
 - NC DOT / RAILROAD ENCROACHMENT AGREEMENTS ARE REQUIRED FOR ANY UTILITY WORK (INCLUDING MAIN EXTENSIONS & SERVICE TAPS) WITHIN STATE OR RAILROAD ROW PRIOR TO CONSTRUCTION
 - GREASE INTERCEPTOR / OIL WATER SEPARATOR SIZING CALCULATIONS & INSTALLATION SPECIFICATIONS SHALL BE APPROVED BY THE CORPUD FOG PROGRAM COORDINATOR PRIOR TO ISSUANCE OF A BUILDING PERMIT. CONTACT STEPHEN CALVERLEY AT (919) 996-2334 OR STEPHEN.CALVERLEY@RALEIGHNC.GOV FOR MORE INFORMATION
 - CROSS-CONNECTION CONTROL PROTECTION DEVICES ARE REQUIRED BASED ON DEGREE OF HEALTH HAZARD INVOLVED AS LISTED IN APPENDIX B OF THE RULES GOVERNING PUBLIC WATER SYSTEMS IN NORTH CAROLINA.
 - THE DEVICES SHALL MEET AMERICAN SOCIETY OF SANITARY ENGINEERING (ASSE) STANDARDS OR BE ON THE UNIVERSITY OF SOUTHERN CALIFORNIA APPROVAL LIST.
 - THE DEVICE AND INSTALLATION SHALL MEET THE GUIDELINES OF APPENDIX A - GUIDELINES AND REQUIREMENTS FOR THE CROSS CONNECTION PROGRAM IN RALEIGH'S SERVICE AREA.
 - THE DEVICES SHALL BE INSTALLED AND TESTED (BOTH INITIAL AND PERIODIC TESTING THEREAFTER) IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS OR THE LOCAL CROSS-CONNECTION CONTROL PROGRAM, WHICHEVER IS MORE STRINGENT. CONTACT STEPHEN CALVERLEY AT (919) 996-2334 OR STEPHEN.CALVERLEY@RALEIGHNC.GOV FOR MORE INFORMATION.
 - NOTICE FOR PROJECTS WITH REPLACE OR OVERSIZED MAINS: IF THE CITY'S REIMBURSEMENT FOR AN OVERSIZED MAIN OR URBAN MAIN REPLACEMENT PROJECT IS \$250,000 OR GREATER, THE PROJECT MUST BE PUBLICLY BID.

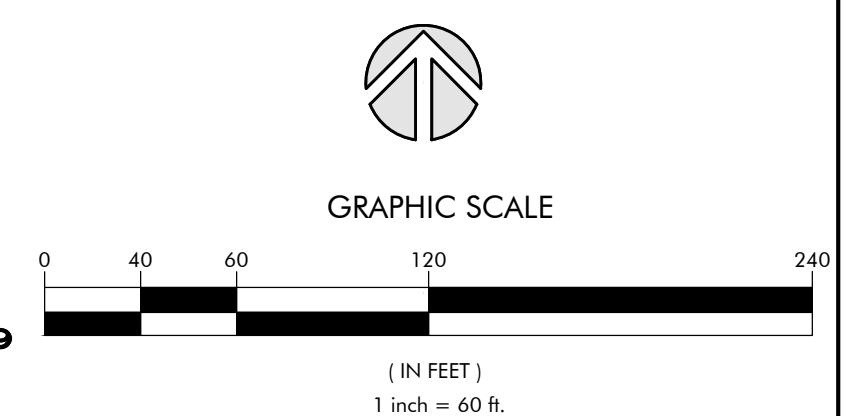
NOTES

SANITARY SEWER LINES WILL MAINTAIN AT LEAST 10' OF HORIZONTAL SEPARATION FROM WATER LINES

WATER LINES WILL MAINTAIN AT LEAST 5' OF HORIZONTAL SEPARATION FROM STORM LINES

STORM LINES WILL MAINTAIN AT LEAST 5' OF HORIZONTAL SEPARATION FROM SANITARY SEWER LINES

FIRE HYDRANTS AND ASSOCIATED CONNECTIONS WILL FOLLOW THE STANDARDS SET FORTH IN THE CITY OF RALEIGH PUBLIC UTILITIES HANDBOOK



MATCHLINE - SHEET C-6.1
MATCHLINE - SHEET C-6.4

LYNDON OAKS Master Plan Utility Plan Enlargement 1

Knightsdale, North Carolina

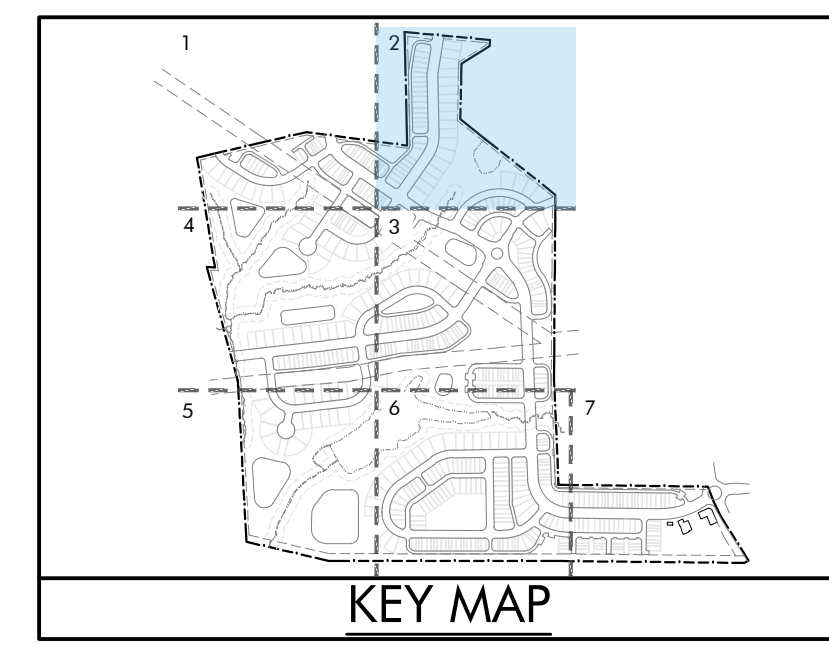
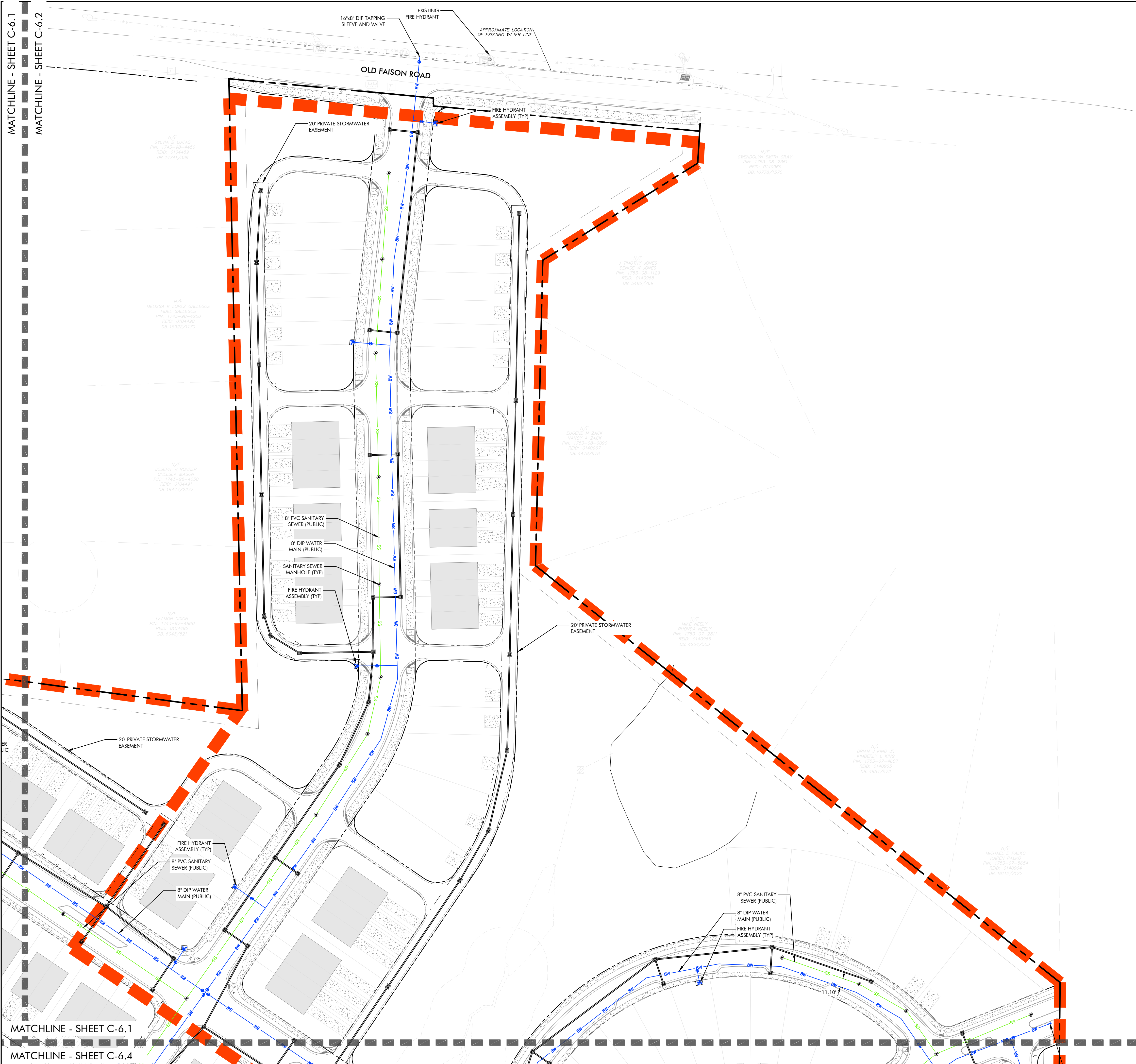
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Project No: 22-RDU-083
Date: 05/03/2024
Sheet No:

C-6.1

MATCHLINE - SHEET C-6.1
MATCHLINE - SHEET C-6.2

MATCHLINE - SHEET C-6.1
MATCHLINE - SHEET C-6.4



STANDARD UTILITY NOTES (AS APPLICABLE):

- ALL MATERIALS & CONSTRUCTION METHODS SHALL BE IN ACCORDANCE WITH CITY OF RALEIGH DESIGN STANDARDS, DETAILS & SPECIFICATIONS (REFERENCE: CORPUD HANDBOOK, CURRENT EDITION)
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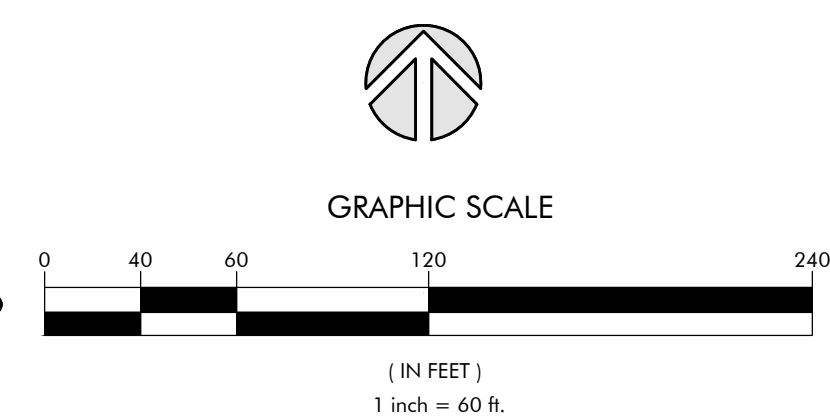
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nc firm no: P-2671 sc coo no: C-03044
PRELIMINARY DRAWING
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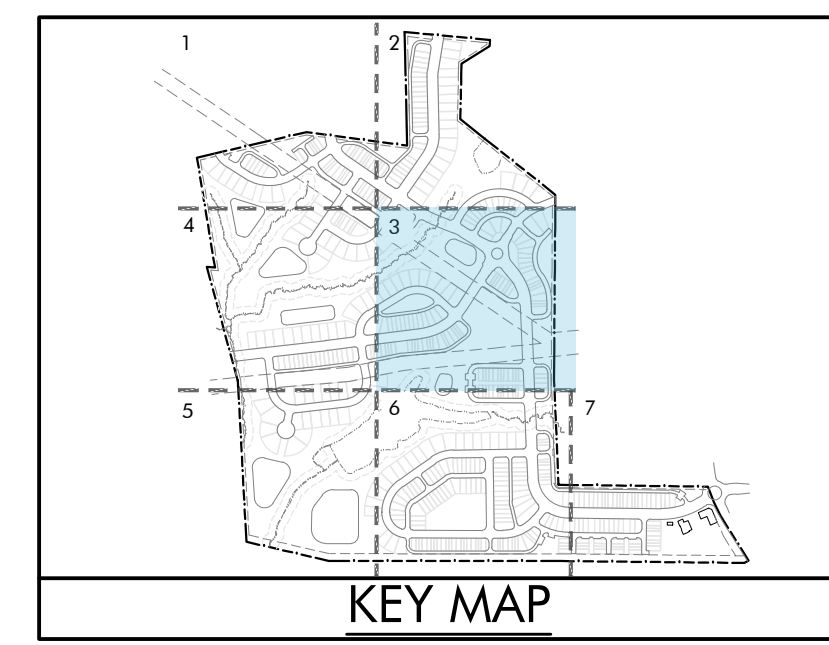
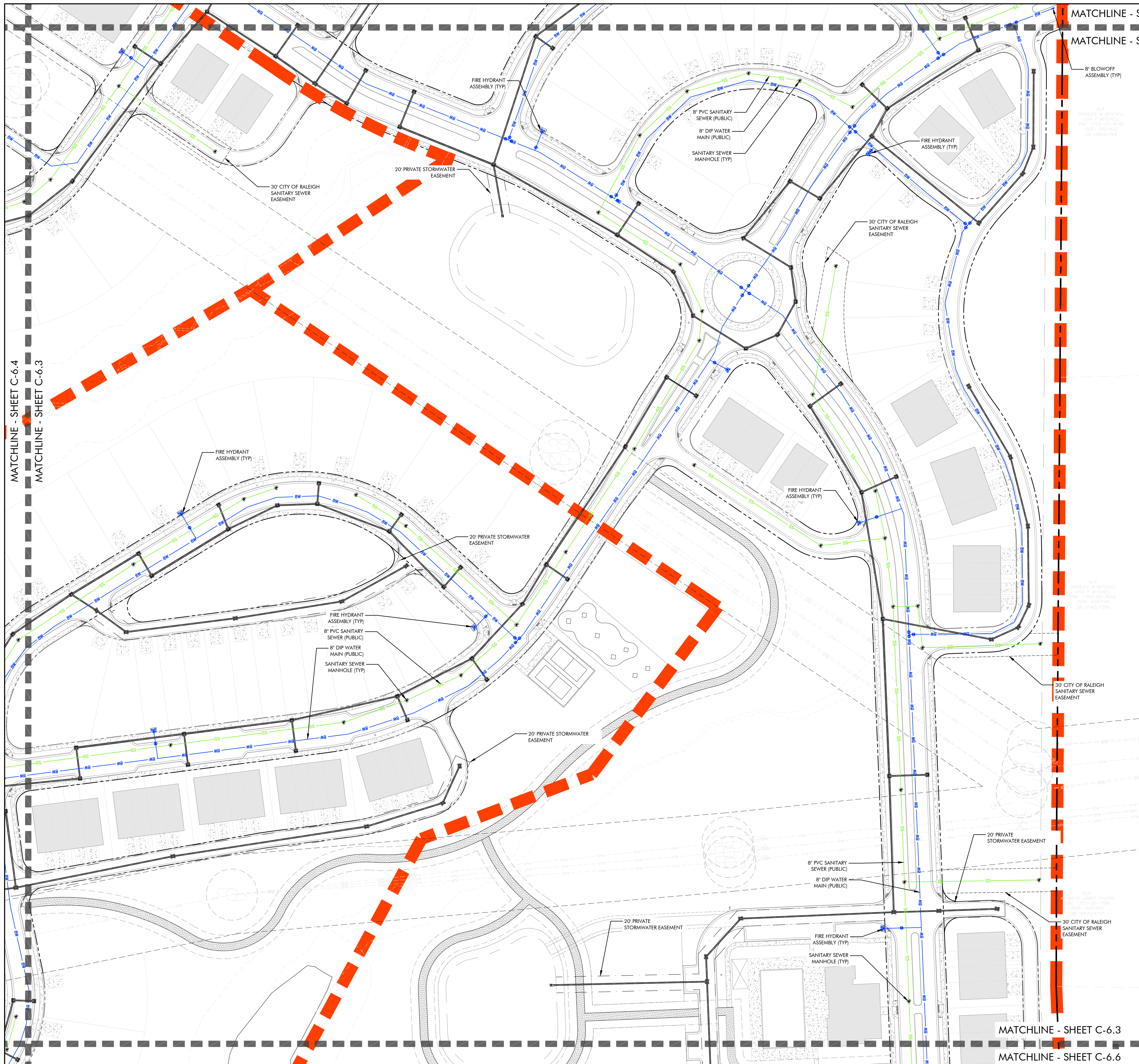
D.R. Horton
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

**LYNDON OAKS
Master Plan
Utility Plan Enlargement 2**
Knightdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
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Project No: 22-RDU-083
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C-6.2



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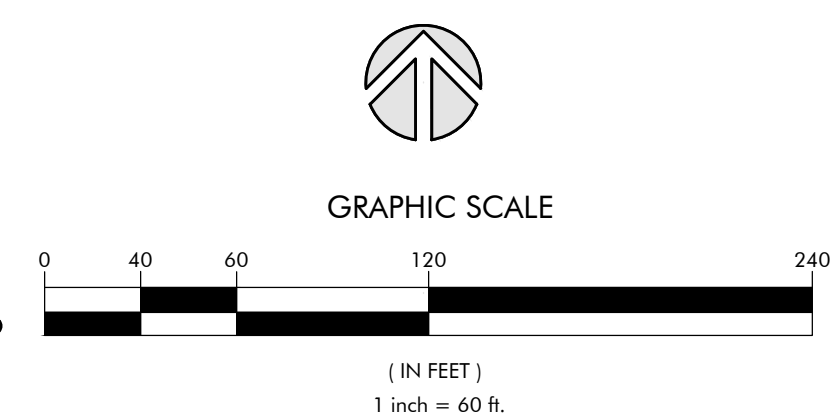
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nc firm no: P-2671 sc coo no: C-03044
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LYNDON OAKS
Master Plan
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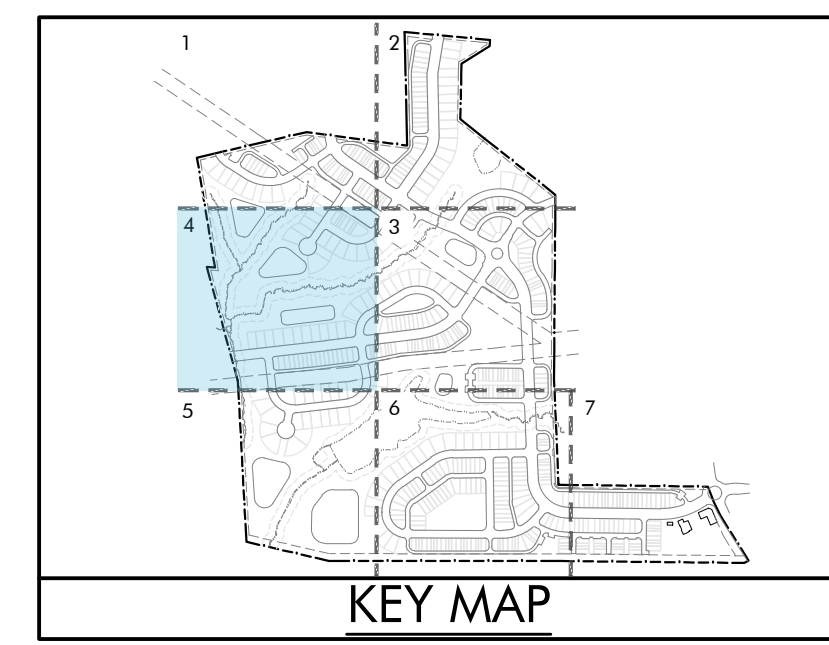
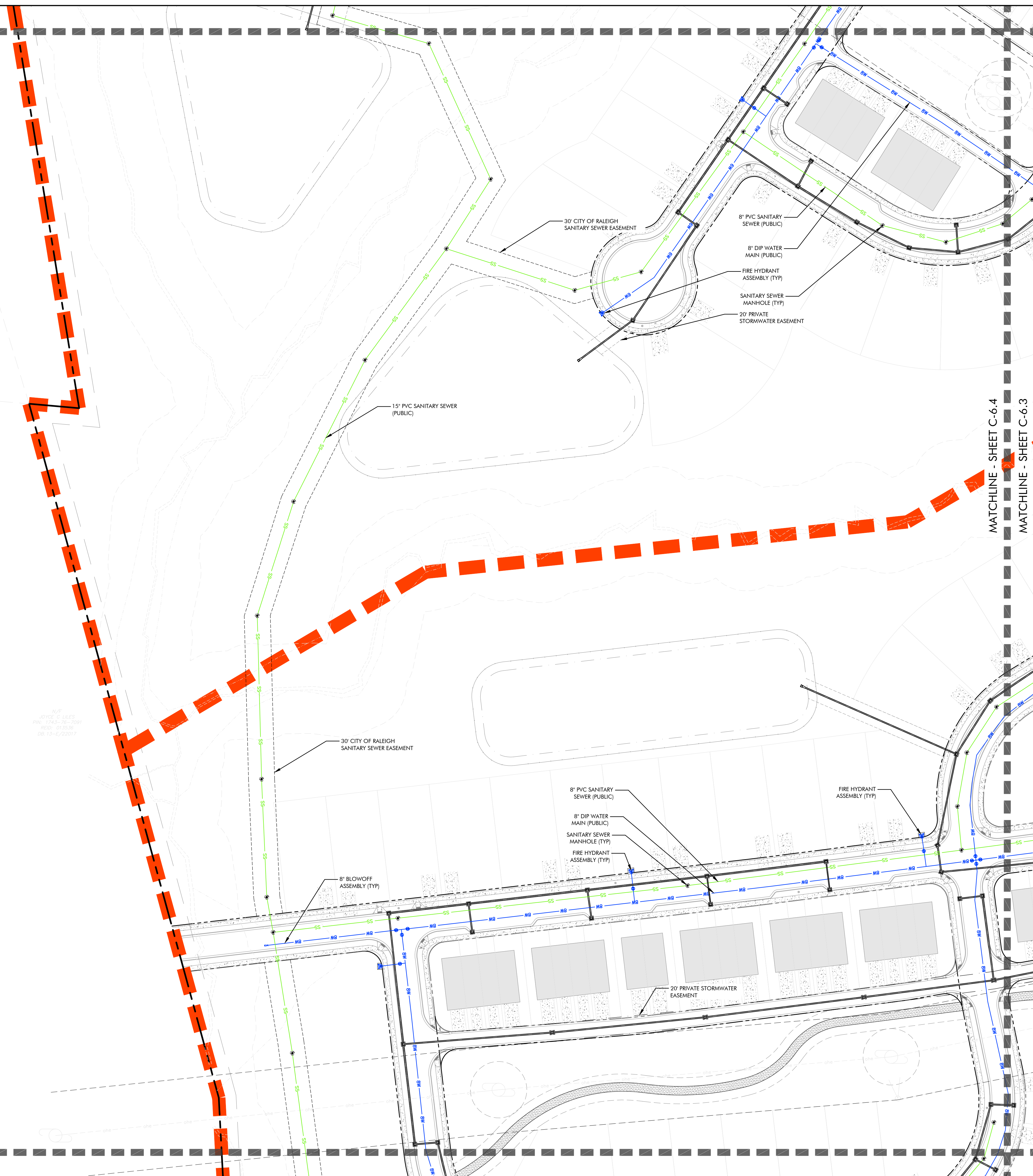
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Project No: 22-RDU-083
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Sheet No:
C-6.3

MATCHLINE - SHEET C-6.1
 MATCHLINE - SHEET C-6.4

MATCHLINE - SHEET C-6.4
 MATCHLINE - SHEET C-6.5



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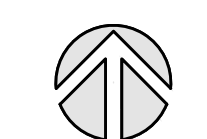
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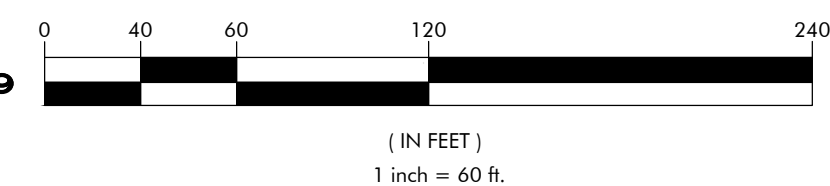
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 urbandesignpartners.com

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**LYNDON OAKS
 Master Plan
 Utility Plan Enlargement 4**
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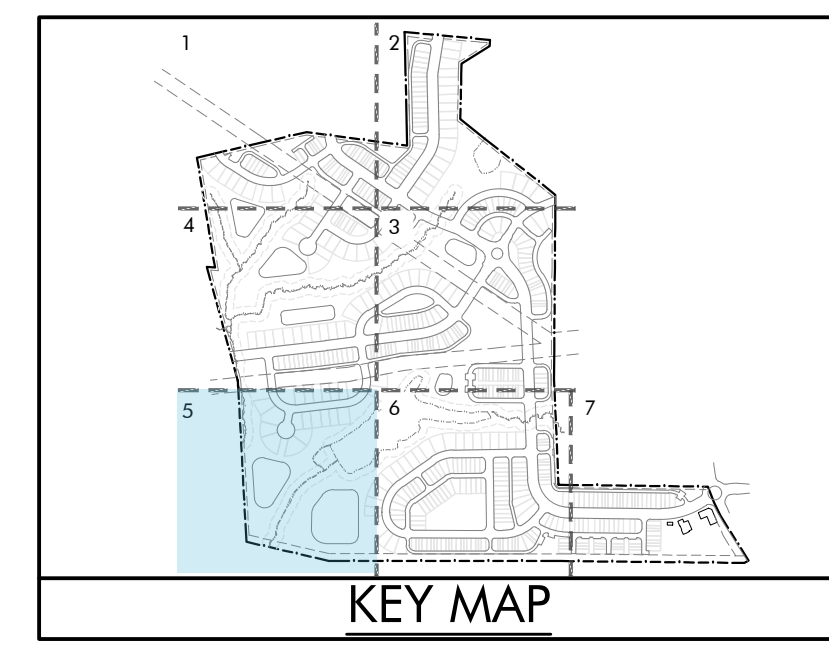
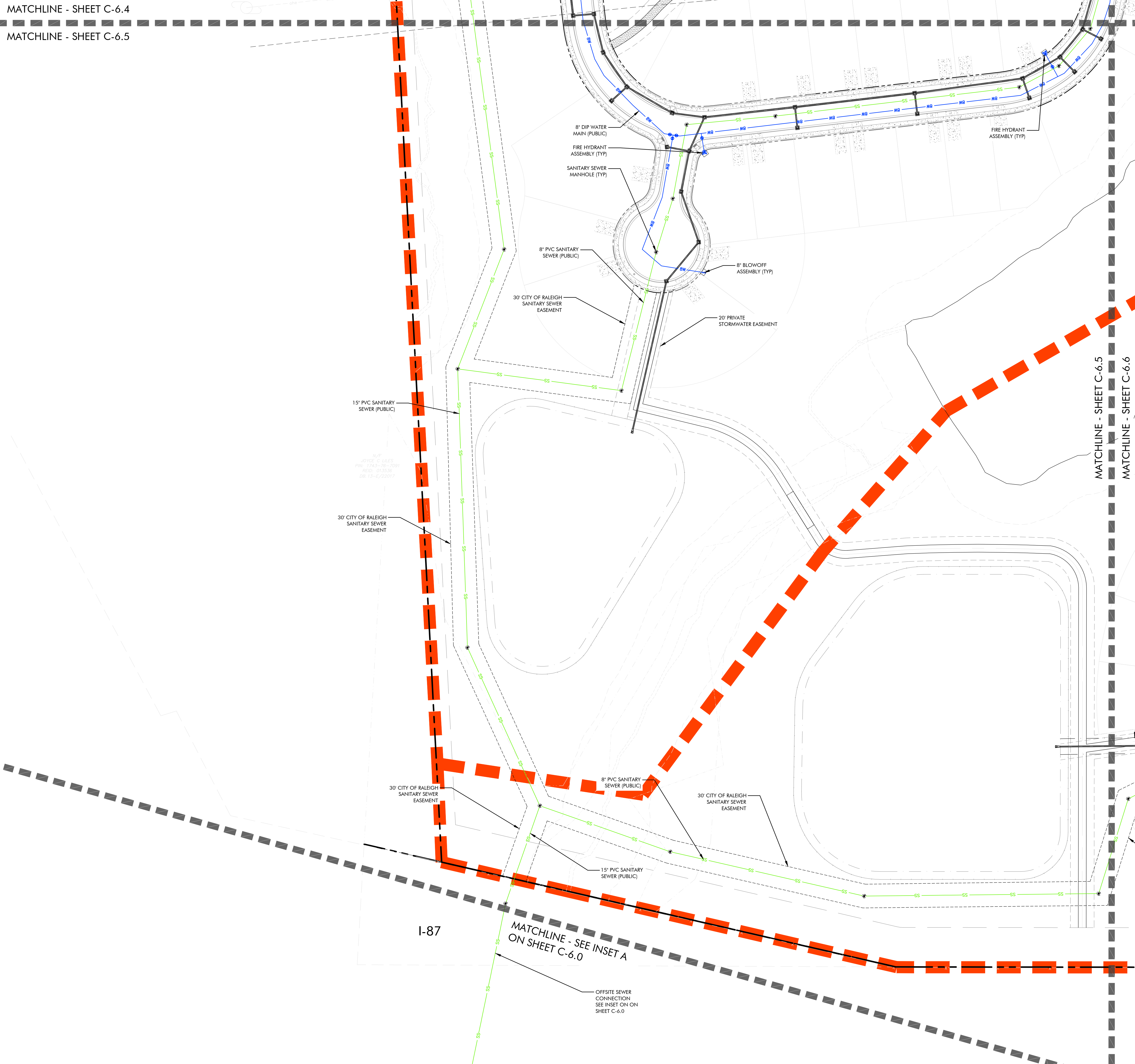
Project No: 22-RDU-083

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Sheet No:

C-6.4

MATCHLINE - SHEET C-6.4
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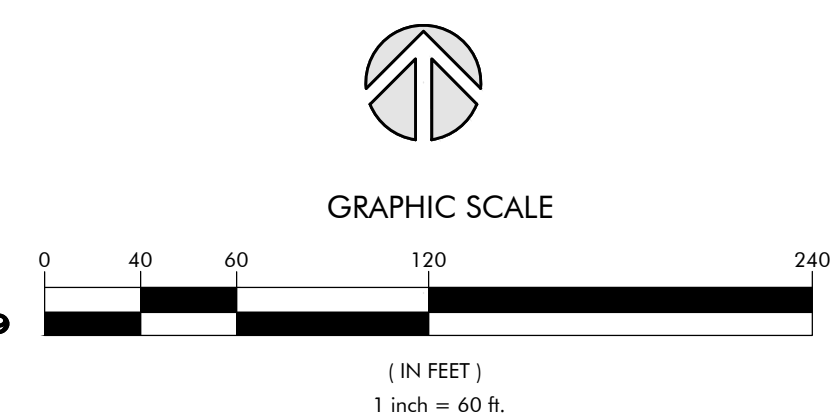
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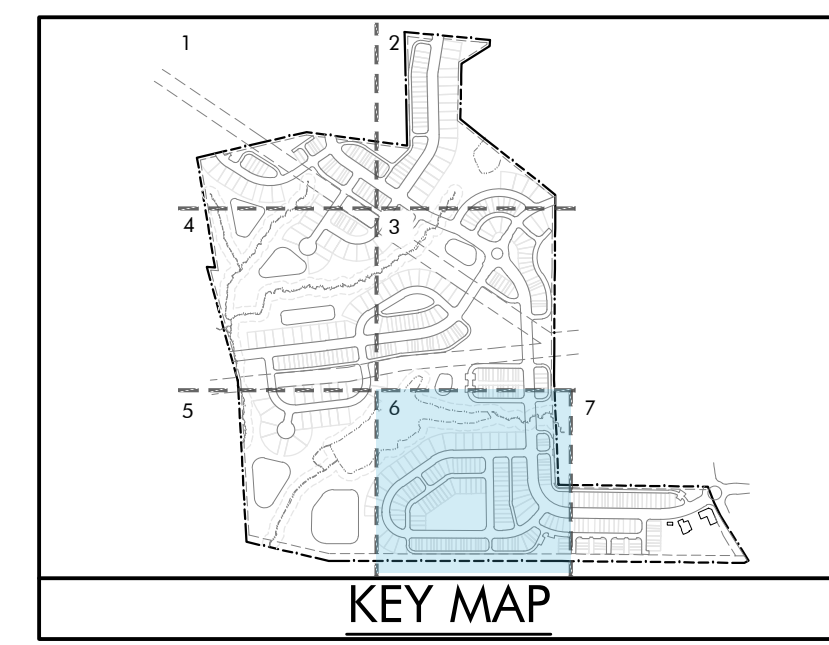
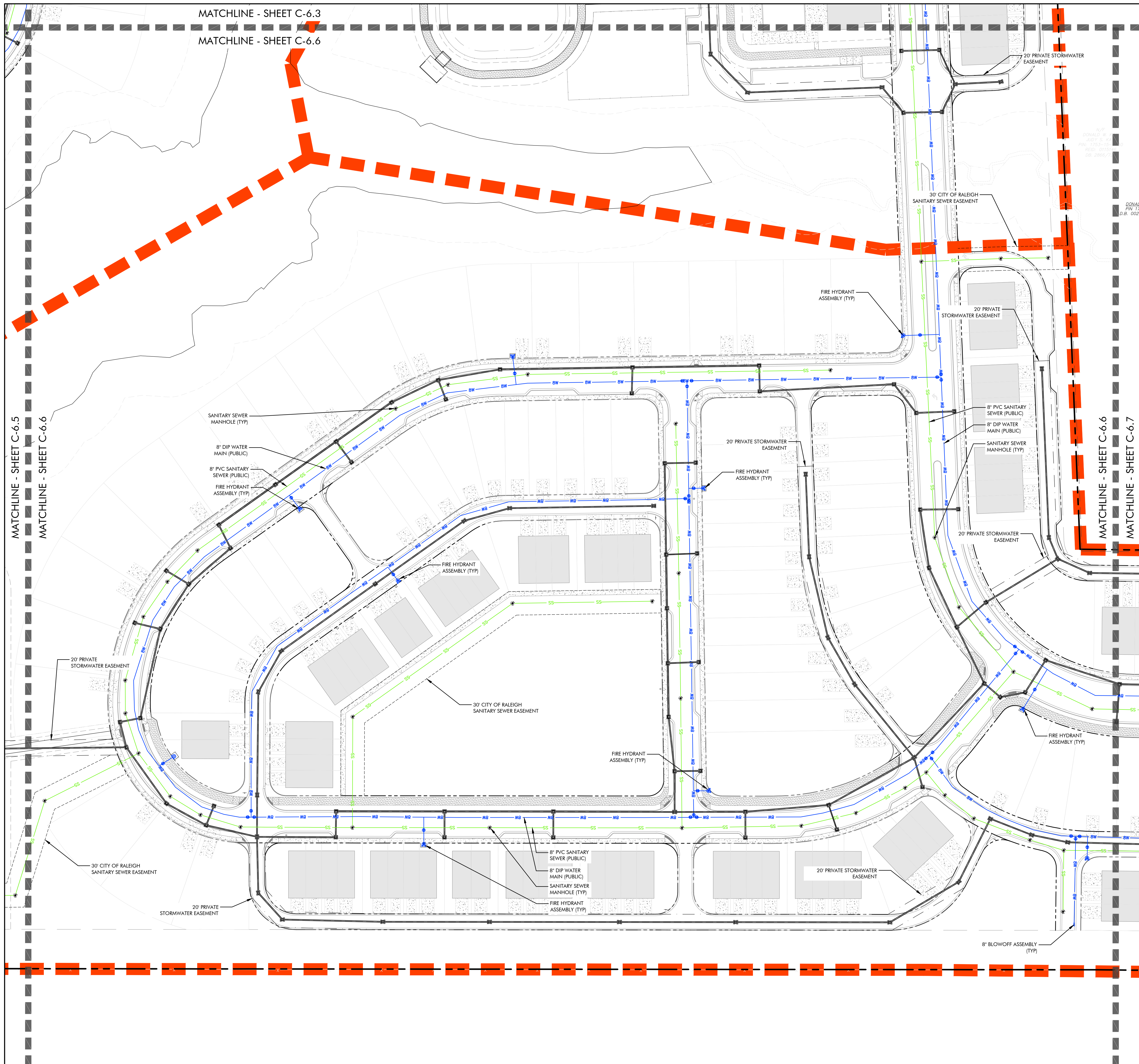
nc firm no: P-2671 sc coo no: C-03044
 PRELIMINARY DRAWING
 FOR REVIEW PURPOSES ONLY
 05/03/2024
 D.R. Horton
 7208 Falls of Neuse Rd. Ste. 201
 Raleigh, NC 27615

**LYNDON OAKS
 Master Plan
 Utility Plan Enlargement 5**
 Knightdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	05/03/2024		
#3	05/28/2024		

Project No: 22-RDU-083
 Date: 05/03/2024
 Sheet No:

C-6.5



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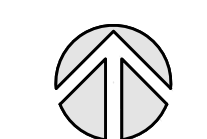
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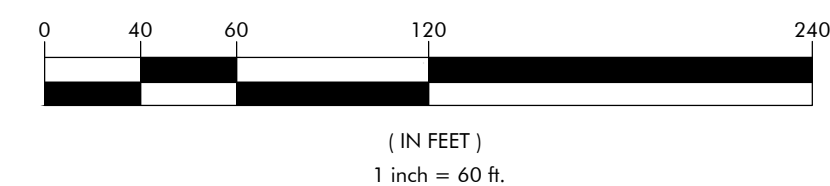
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GRAPHIC SCALE



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Raleigh, NC 27601
919.275.5022
urbandesignpartners.com

nc firm no: P-2671 sc coo no: C-03044

05/03/2024

D.R. Horton
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

LYNDON OAKS
Master Plan
Utility Plan Enlargement 6
Knightdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083

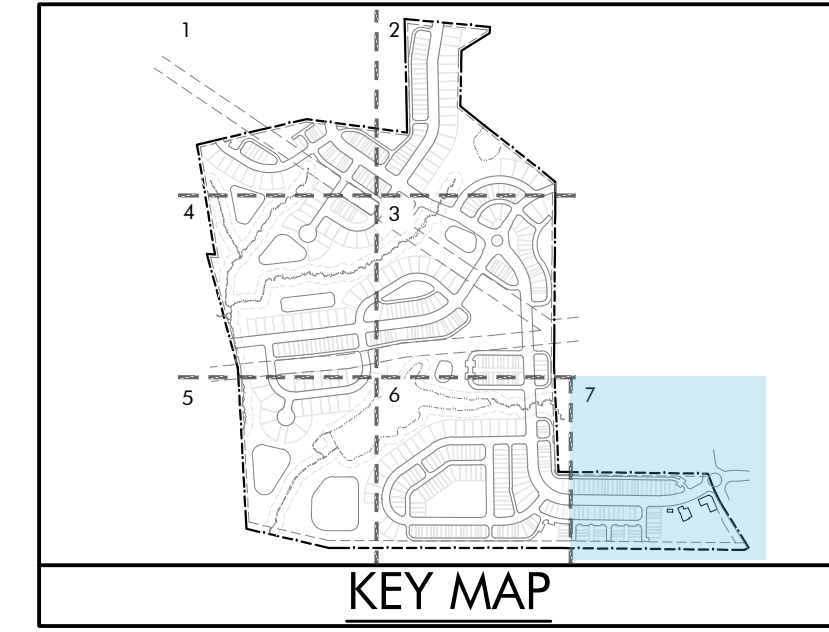
Date: 05/03/2024

Sheet No:

C-6.6

MATCHLINE - SHEET C-6.6
MATCHLINE - SHEET C-6.7

SINGLE-USE RETAIL	
SINGLE USE RETAIL BASE POINTS	41 pts.
CONSTRUCTION OF GATEWAY LANDSCAPING	5 pts.
DECK/PATIO (2,000 SF MIN)	2 pts.
OUTDOOR DISPLAY OF PUBLIC ART	4 pts.
TOTAL	52 pts.

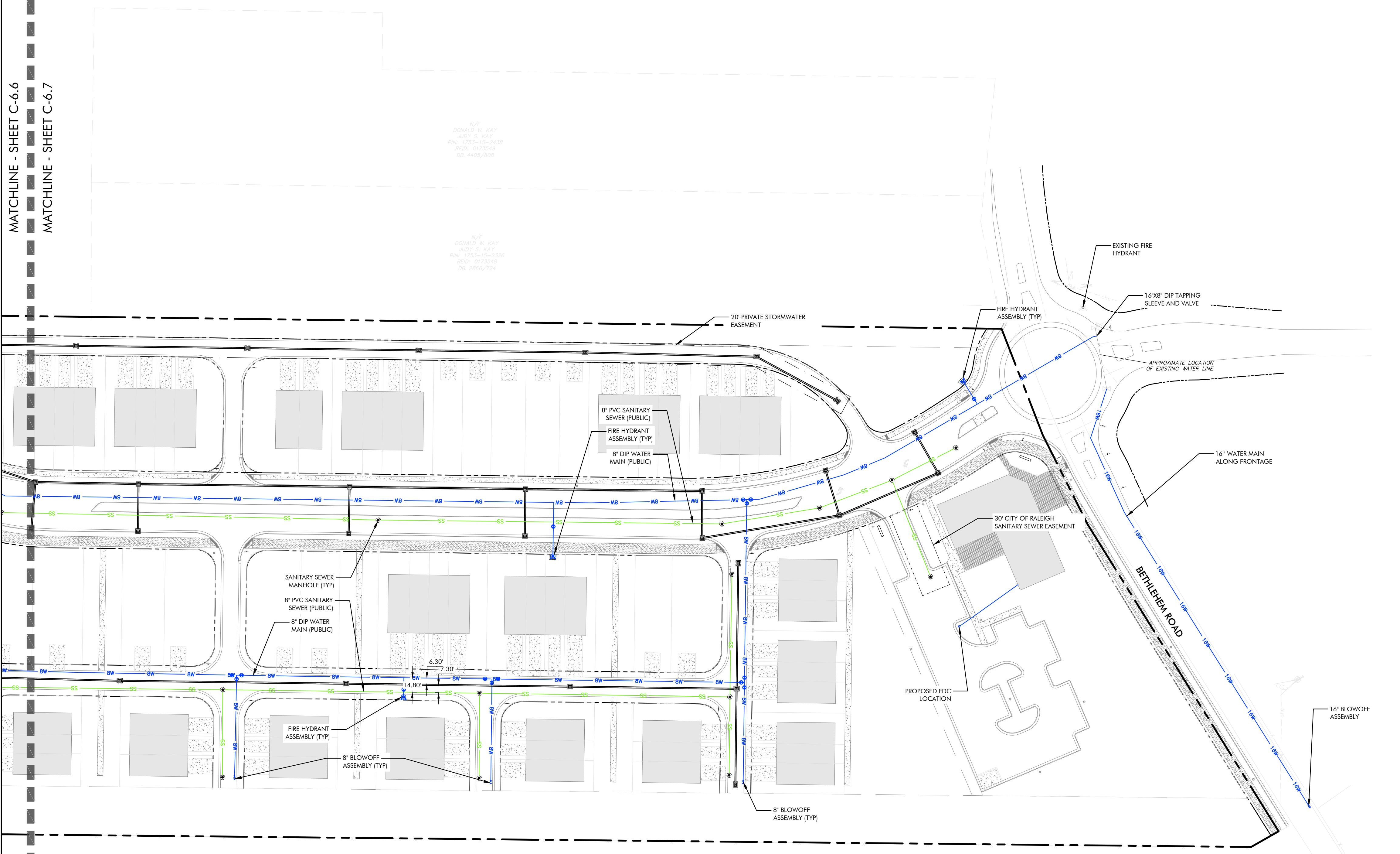


nc firm no: P-2671 sc coo no: C-03044

PRELIMINARY DRAWING
FOR REVIEW PURPOSES ONLY

05/03/2024

D.R. Horton
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615



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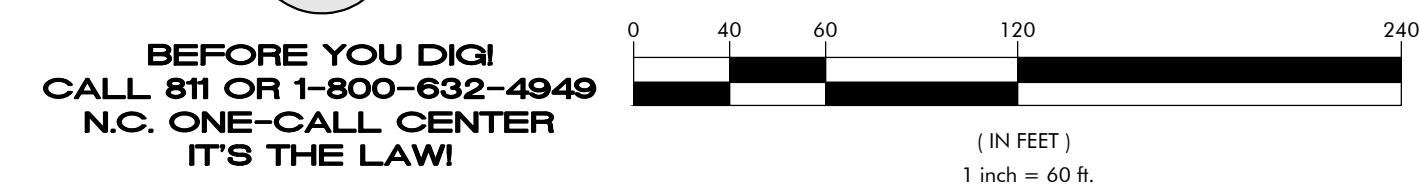
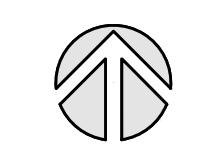
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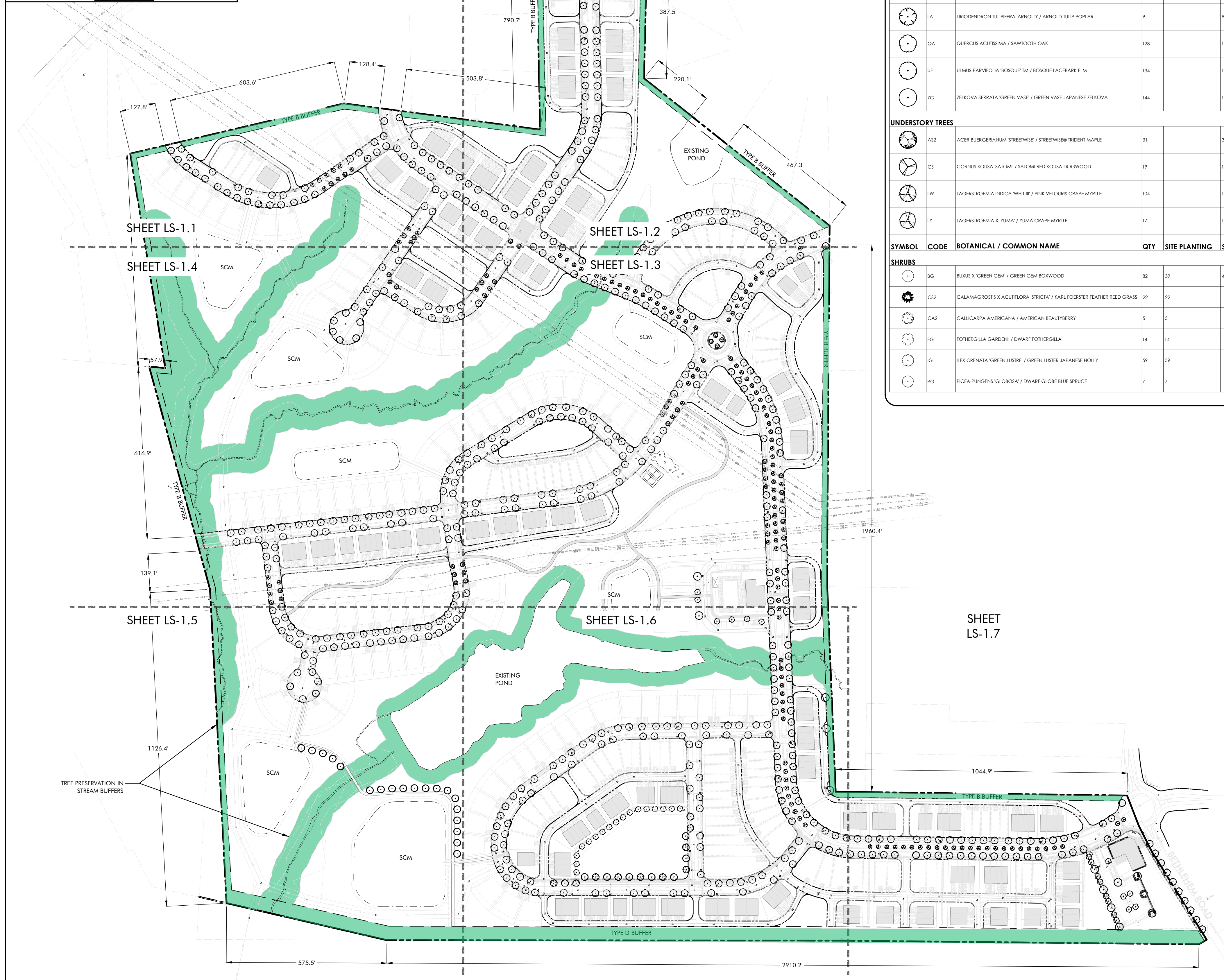
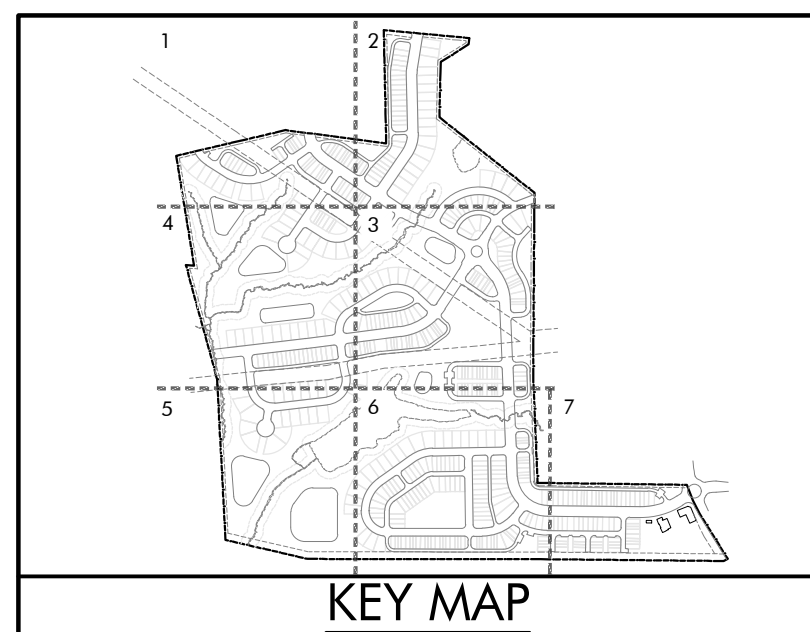


LYNDON OAKS
Master Plan
Utility Plan Enlargement 7
Knightdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
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Project No: 22-RDU-083
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Sheet No:

C-6.7



PLANT SCHEDULE

SYMBOL	CODE	BOTANICAL / COMMON NAME	QTY	SITE PLANTING	STREET TREES	CONT	CAL	SIZE	MATURE HEIGHT
CANOPY TREES									
AD		ACER RUBRUM 'OCTOBER GLORY' / OCTOBER GLORY RED MAPLE	34	19	15	B & B	2'CAL	8' HT	25'-11"
AB		ACER SACCHARUM 'BOHFIRE' / BOHFIRE SUGAR MAPLE	149		149	B & B	2'CAL	8' HT	
CA		CARPINUS CAROLINIANA / AMERICAN HORNBEAM	36	19	17	B & B	2'CAL	8' HT	15'-11"
GK		GYMNOCLADUS DIOICA / KENTUCKY COFFEETREE	13	13		B & B	2'CAL	8' HT	40'-11"
LA		LIRIODENDRON TUJIFERA 'ARNOOLD' / ARNOOLD TULIP POPLAR	9		9	B & B	2'CAL	8' HT	40'-65R. HT.
QA		QUERCUS ACUTISSIMA / SAWTOOTH OAK	128		128	B & B	2'CAL	8' HT	40'-65R. HT.
UF		ULMUS PARVIFOLIA 'BOSQUE TM' / BOSQUE LACEBARK ELM	134		134	B & B	2'CAL	8' HT	
ZG		ZELKOVA SERRATA 'GREEN VASE' / GREEN VASE JAPANESE ZELKOVA	144		144	B & B	2'CAL	8' HT	> 65R. HT.
UNDERSTORY TREES									
AS2		ACER BURGERSIANUM 'STREETWISE' / STREETWISE TRIDENT MAPLE	31		31	B & B	1.25'CAL	4' HT	15'-11"
CS		CORNUS KOUSA 'SATOMI' / SATOMI RED KOUSA DOGWOOD	19		19	B & B	1.25'CAL	4' HT	15'-25R. HT.
LW		LAGERSTROEMIA INDICA 'WHITE W' / PINK VELOURIB CRAPE MYRTLE	104		104	B & B	MULTI-STEM (3 STALKS MIN.)	8' HT	6'-11"
LY		LAGERSTROEMIA X 'YUMA' / YUMA CRAPE MYRTLE	17		17	B & B	MULTI-STEM (3 STALKS MIN.)	8' HT	15'-25R. HT.
SHRUBS									
BG		BUXUS 'GREEN GEM' / GREEN GEM BOXWOOD	82	39	43		3 GAL		3'-6R. HT.
CS2		CALAMAGROSTIS X ACUTIFLORA 'STRICTA' / KARL FOERSTER FEATHER REED GRASS	22	22			3 GAL		18'-6' HT.
CA2		CALLICARPA AMERICANA / AMERICAN BEAUTYBERRY	5	5			3 GAL		3'-6R. HT.
FG		FOTHERGILLA GARDENII / DWARF FOTHERGILLA	14	14			3 GAL		18'-36R. HT.
IG		ILEX ORNATA 'GREEN LUSTRE' / GREEN LUSTRE JAPANESE HOLLY	59	59			3 GAL		4'-11"
PG		PICEA PUNGENS 'GLOBOSA' / DWARF GLOBE BLUE SPRUCE	7	7			3 GAL		3'-6R. HT.

- ### NOTES:
- EXISTING PLANT MATERIAL TO BE PRESERVED IN ALL BUFFER AREAS AS PRIORITY TIER 1 TREE SAVE AREAS. WHERE SUFFICIENT MATERIAL DOES NOT MEET REQUIREMENTS, SUPPLEMENTAL PLANT MATERIAL WILL BE REQUIRED TO MEET BUFFER OPACITY STANDARDS.
 - FINAL LOCATION OF SUPPLEMENTAL BUFFER PLANTINGS TO BE COORDINATED WHEN CONSTRUCTION DOCUMENTS ARE DEVELOPED. SEE SHEET LS-2.0 FOR TYPICAL BUFFER SECTIONS.
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 - FULL LANDSCAPE SCHEDULE ON SHEET LS-2.0
 - LANDSCAPE SPECIES, LOCATION, AND QUANTITY IS SUBJECT TO CHANGE AT TIME OF CONSTRUCTION DOCUMENTS.
 - EACH SINGLE-FAMILY OR DUPLEX LOT SHALL CONTAIN A MINIMUM OF ONE (1) CANOPY TREE FOR EVERY TWO THOUSAND (2,000) SQUARE FEET OF LOT AREA OR FRACTION THEREOF UP TO TWENTY THOUSAND (20,000) SQUARE FEET IN LOT AREA. ANY PORTION OF THE RESIDENTIAL LOT OCCUPIED BY A RECORDED UTILITY EASEMENT SHALL NOT BE INCLUDED AS PART OF THE TOTAL LOT AREA. THE LOCATION OF PLANTING TO ACCOUNT FOR PHYSICAL CONDITIONS MAY BE ADJUSTED BY THE LAND USE ADMINISTRATOR.
 - THE USE OF EXISTING TREES MEETING THE FOLLOWING STANDARDS TO SATISFY THIS REQUIREMENT IS ENCOURAGED. EXISTING LARGE SHADE TREES MEASURING MORE THAN SIX (6) INCHES IN DBH MAY BE SECTIONED TOWARDS THIS REQUIREMENT.
 - REQUIRED STREET TREES (UDO SECTION 7.4 (L)) MAY NOT BE COUNTED TOWARDS THE FULFILLMENT OF THE RESIDENTIAL LANDSCAPING REQUIREMENT, APART FROM REQUIRED STREET TREES. ALL OTHER TREES SHALL BE PLANTED WITHIN THE PRIVATE LOT.
 - FOUNDATION PLANTINGS CONSISTING OF EVERGREEN SHRUBS SHALL BE INSTALLED ALONG THE ENTIRE FOUNDATION WALL OF THE BUILDING. PLANT INSTALLATION SHALL BE A MINIMUM OF TWO (2) FEET IN HEIGHT PLANTED AT FOUR (4) FOOT INTERVALS.

- ### LANDSCAPE REQUIREMENTS:
- BUFFER YARD TYPE B (PERIMETER BUFFER)**
- BUFFER TYPE: B
 - WIDTH REQD: 20' MIN.
 - WIDTH PROV: 20'
 - LENGTH: ≥ 12,730 LF TOTAL
 - REQUIRED PLANTINGS:
 • CANOPY TREES: 3 TREES/100 LF
 • UNDERSTORY TREES: 5 TREES/100 LF
 • SHRUBS: 20 SHRUBS/100 LF
 (12,730 LF / 100 LF)(3) = 382 CANOPY TREES REQD
 (12,730 LF / 100 LF)(5) = 637 UNDERSTORY TREES REQD
 (12,730 LF / 100 LF)(20) = 2,546 SHRUBS REQD
 - PROPOSED PLANTINGS: EXISTING PLANT MATERIAL IN BUFFERS TO REMAIN AND BE SUPPLEMENTED TO MEET REQUIREMENTS AS STATED ABOVE
- BUFFER YARD TYPE D (PERIMETER BUFFER)**
- BUFFER TYPE: D
 - WIDTH REQD: 50' MIN.
 - WIDTH PROV: 50'
 - LENGTH: ≥ 3,486 LF
 - MIN WALL HT & OPACITY: A FULLY OPAQUE, MASONRY WALL WITH A MINIMUM HEIGHT OF SIX (6) FEET AND A MAXIMUM HEIGHT OF EIGHT (8) FEET SHALL BE PROVIDED.
 - REQUIRED PLANTINGS:
 • CANOPY TREES: 5 TREES/100 LF
 • UNDERSTORY TREES: 8 TREES/100 LF
 • SHRUBS: 35 SHRUBS/100 LF
 (3,486 LF / 100 LF)(5) = 175 CANOPY TREES REQD
 (3,486 LF / 100 LF)(8) = 279 UNDERSTORY TREES REQD
 (3,486 LF / 100 LF)(35) = 1,221 SHRUBS REQD
 - PROPOSED PLANTINGS: EXISTING PLANT MATERIAL IN BUFFERS TO REMAIN AND BE SUPPLEMENTED TO MEET REQUIREMENTS AS STATED ABOVE
- STREET TREES**
- SPACING: 40' O.C. (CANOPY)
 30' O.C. (UNDERSTORY)
 - SPECIES: SPECIES OF EVERY OTHER TREE ALONG THE STREET FRONTAGE SHALL ALTERNATE
- OVERALL TREE CANOPY/TREE PRESERVATION (UDO SEC 7.4.M)**
- REQUIRED: A TREE COVER AREA EQUAL TO THE PERIMETER OF THE DEVELOPMENT IN FEET MULTIPLIED BY 20 SHALL BE MAINTAINED ON SITE.
 (14,079 LF)(20) = 281,580 SF (6.46 AC) REQUIRED
 6.46 AC / 171.88 AC = 3.7% OF SITE
 - PROVIDED:
 • PERIMETER BUFFERS: 844,604 SF (19.39 AC) TREE PRESERVATION PROV.
 • STREAM BUFFERS: 32,524 SF (0.75 AC) WITHIN PERIMETER BUFFERS*
 • STREAM BUFFERS: 812,980 SF (18.64 AC) WITHIN STREAM BUFFERS
 * SUPPLEMENTAL PERIMETER BUFFER TREES TO BE STUDIED AT TIME OF CD.



NO. DATE BY: REVISIONS:
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 #2 05/03/2024
 #3 05/28/2024

Project No: 22-RDU-083
 Date: 05/03/2024
 Sheet No:

GRAPHIC SCALE
 0 100 200 400 800
 (IN FEET)
 1 inch = 200 ft.

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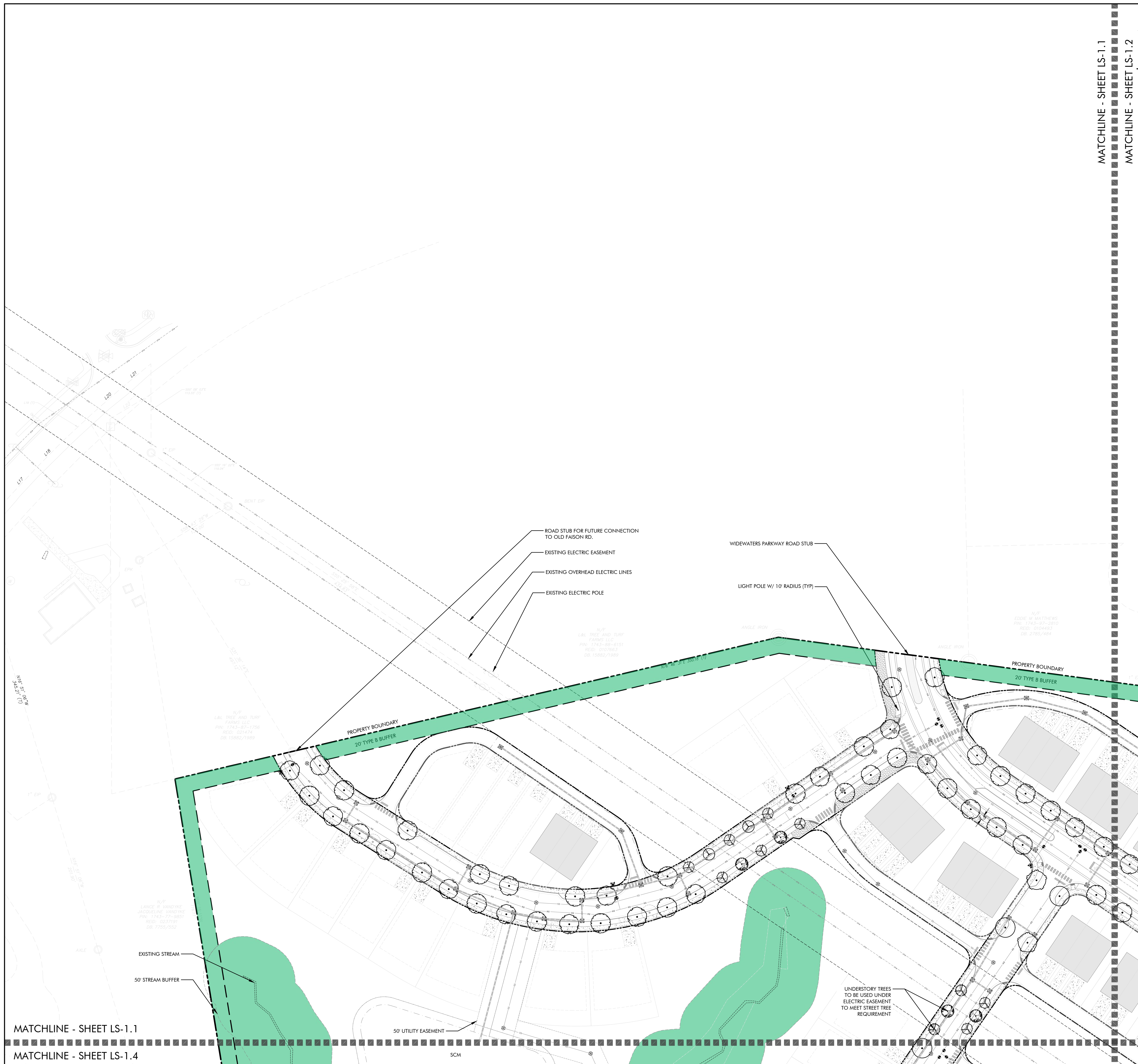
PRELIMINARY DRAWING
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05/03/2024

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LYNDON OAKS
Master Plan
 Overall Landscape Plan
 Knightdale, North Carolina

LS-1.0

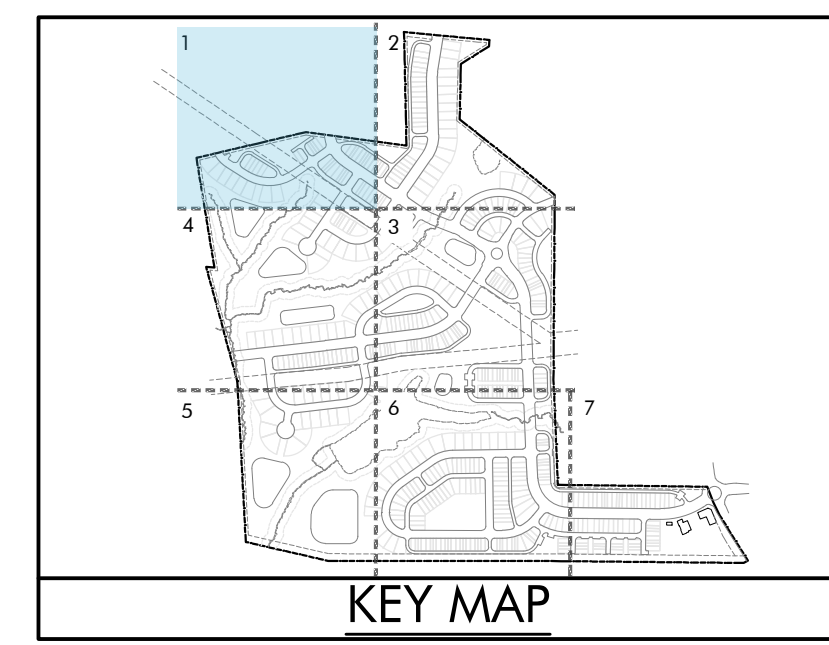


MATCHLINE - SHEET LS-1.1

MATCHLINE - SHEET LS-1.2

MATCHLINE - SHEET LS-1.1

MATCHLINE - SHEET LS-1.4



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LYNDON OAKS

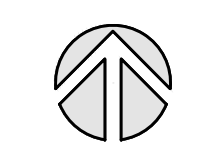
Master Plan

Landscape Plan Enlargement 1

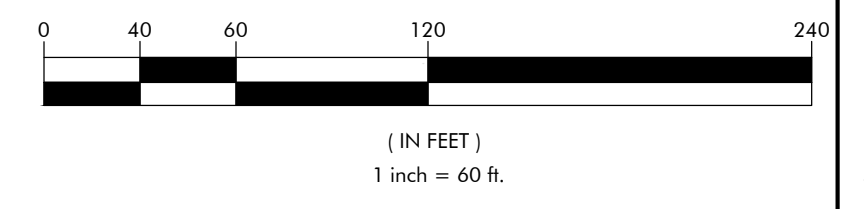
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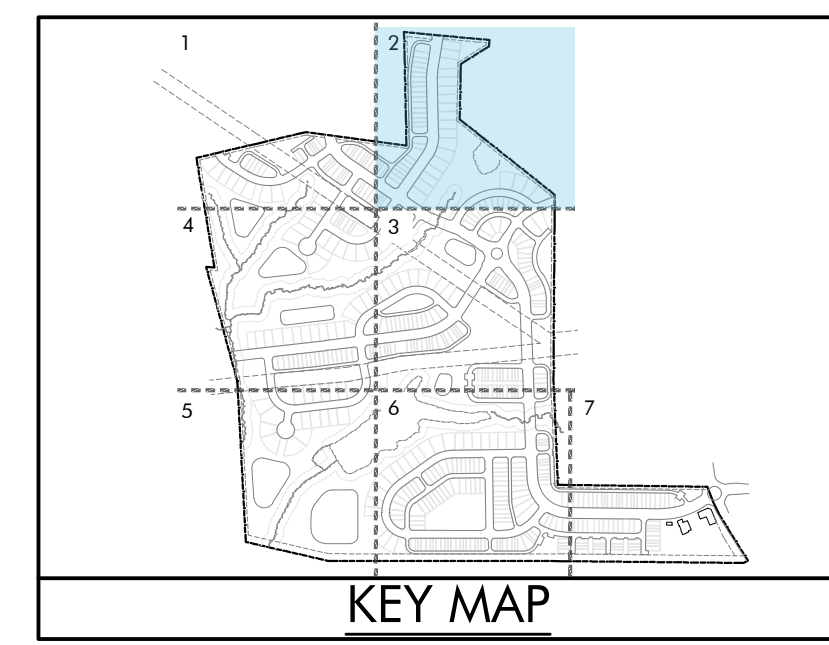
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LS-1.1



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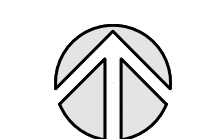
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LYNDON OAKS Master Plan Landscape Plan Enlargement 2

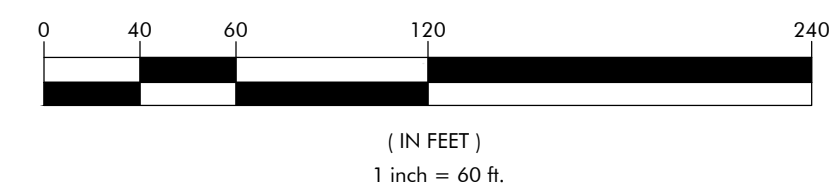
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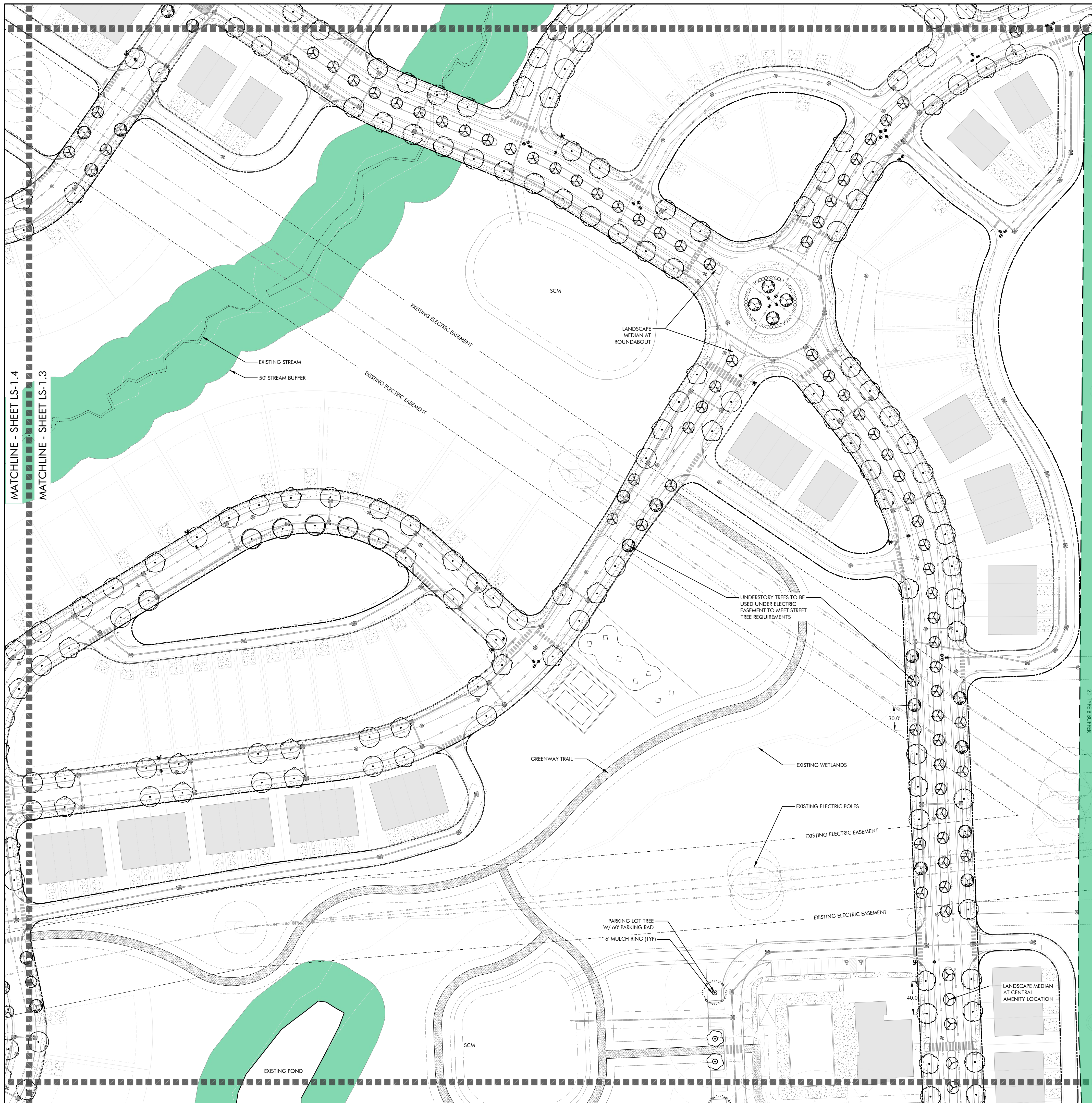
Project No: 22-RDU-083

Date: 05/03/2024

Sheet No:

LS-1.2

MATCHLINE - SHEET LS-1.2
MATCHLINE - SHEET LS-1.3

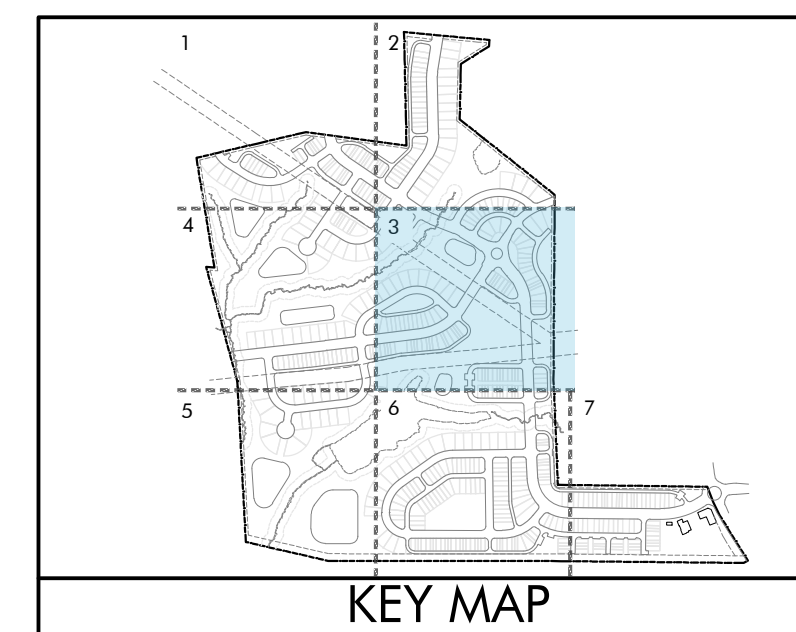


MATCHLINE - SHEET LS-1.2

MATCHLINE - SHEET LS-1.3

MATCHLINE - SHEET LS-1.3

MATCHLINE - SHEET LS-1.6



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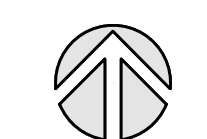
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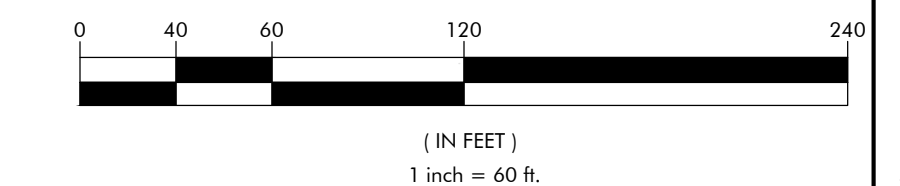
LYNDON OAKS
Master Plan
Landscape Plan Enlargement 3
Knightdale, North Carolina



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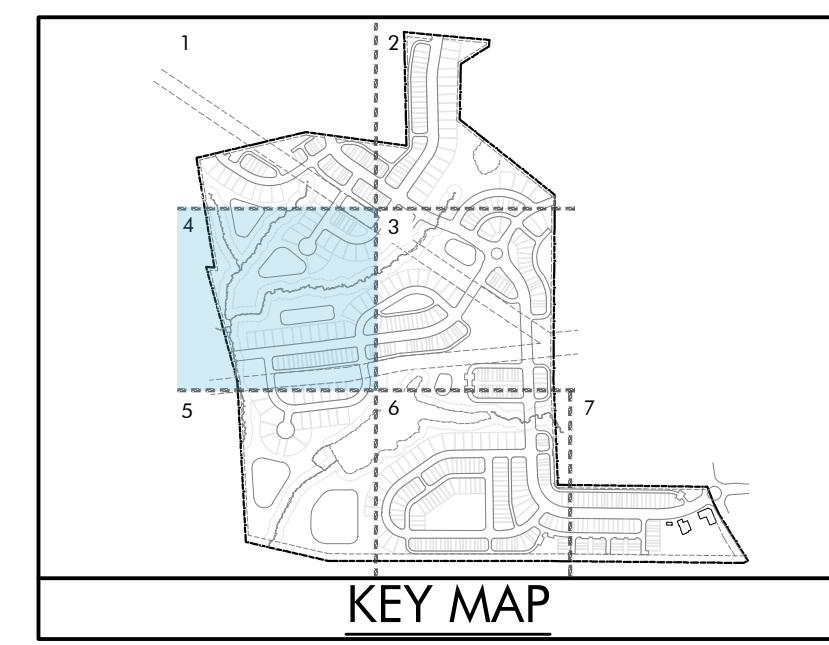
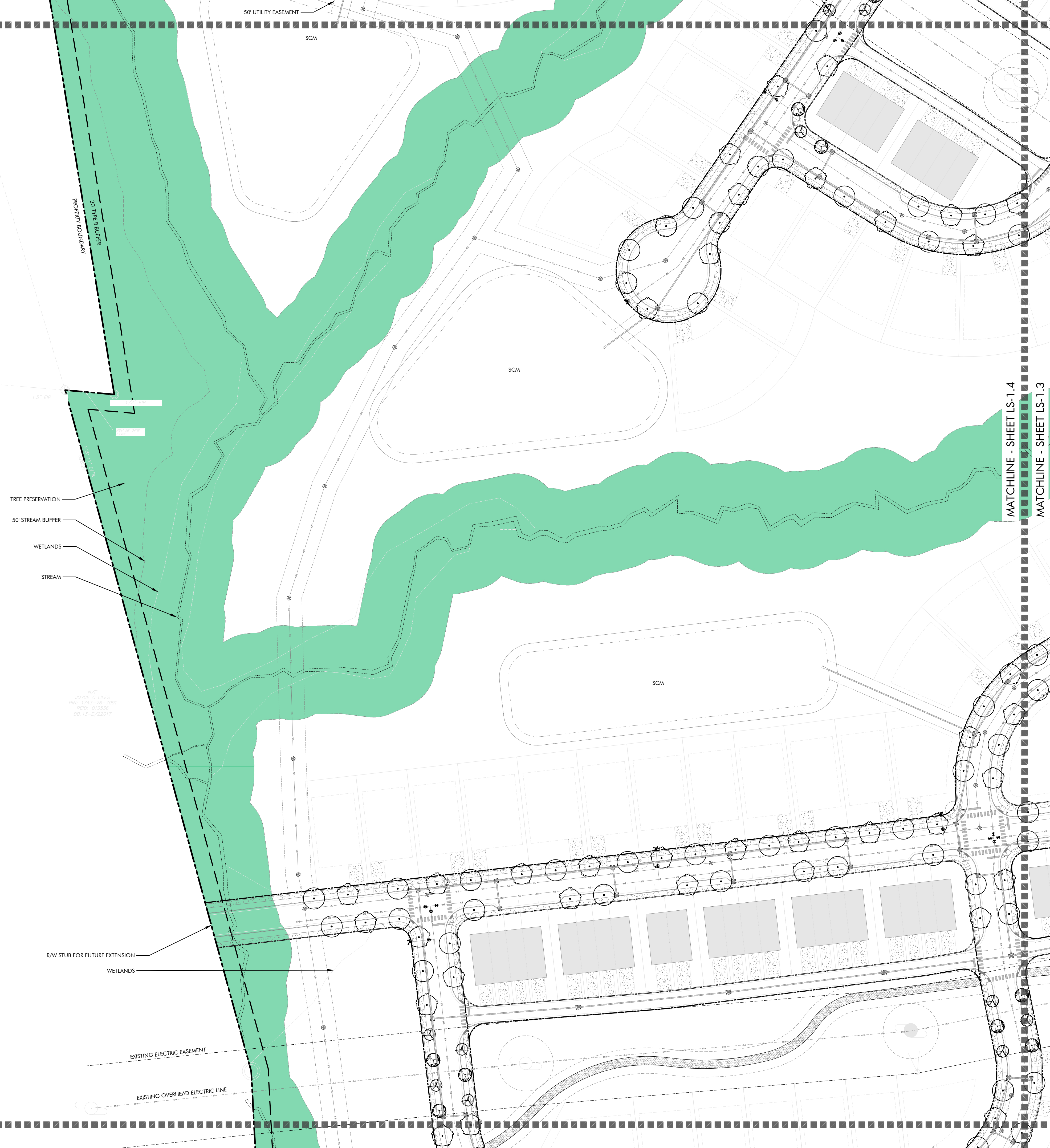
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Sheet No:

LS-1.3

MATCHLINE - SHEET LS-1.1
 MATCHLINE - SHEET LS-1.4

MATCHLINE - SHEET LS-1.4
 MATCHLINE - SHEET LS-1.5



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LYNDON OAKS

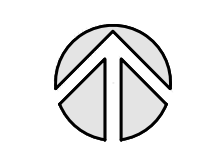
Master Plan

Landscape Plan Enlargement 4

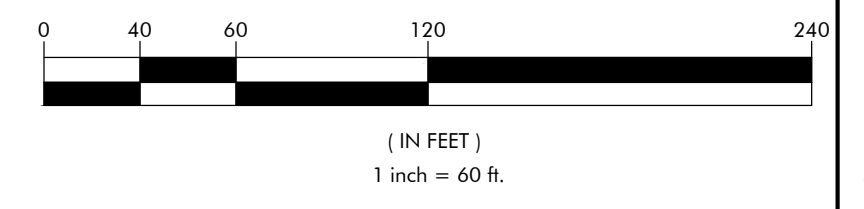
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Sheet No:

LS-1.4

MATCHLINE - SHEET LS-1.4

MATCHLINE - SHEET LS-1.5

EXISTING CREEK

STREAM BUFFER

WETLANDS

GREENWAY TRAIL

N/A
© 2024
P.O. 1248-78-7081
R.D. 01, 3536
08-13-272017

SCM

EXISTING POND

MATCHLINE - SHEET LS-1.5

MATCHLINE - SHEET LS-1.6

WETLANDS

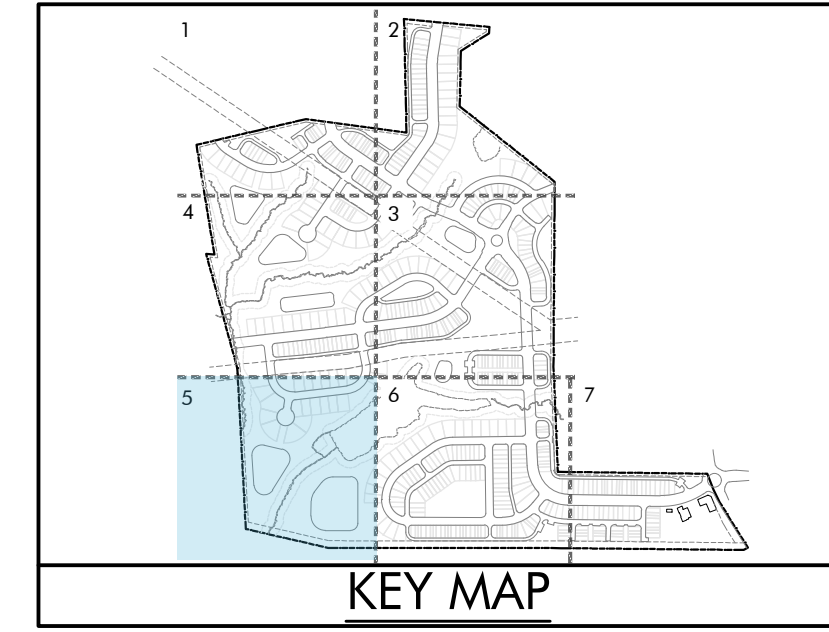
SHADE TREES ALONG GREENWAY TRAIL

TREE PRESERVATION IN STREAM BUFFER

SCM

50' TYPE D BUFFER

PROPERTY BOUNDARY



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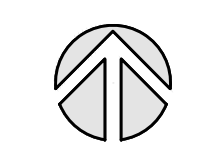
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LYNDON OAKS Master Plan Landscape Plan Enlargement 5

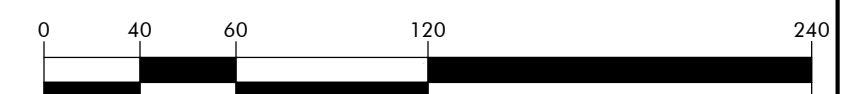
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GRAPHIC SCALE



(IN FEET)
1 inch = 60 ft.

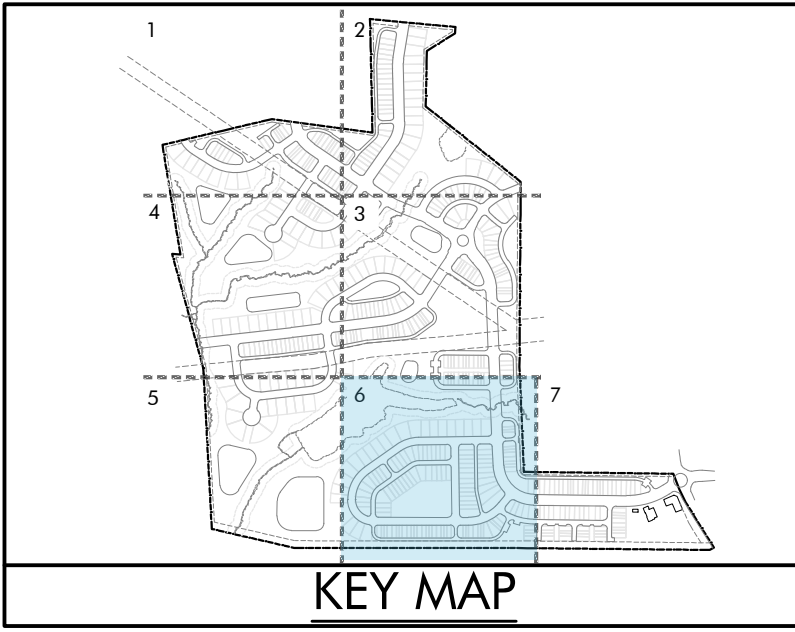
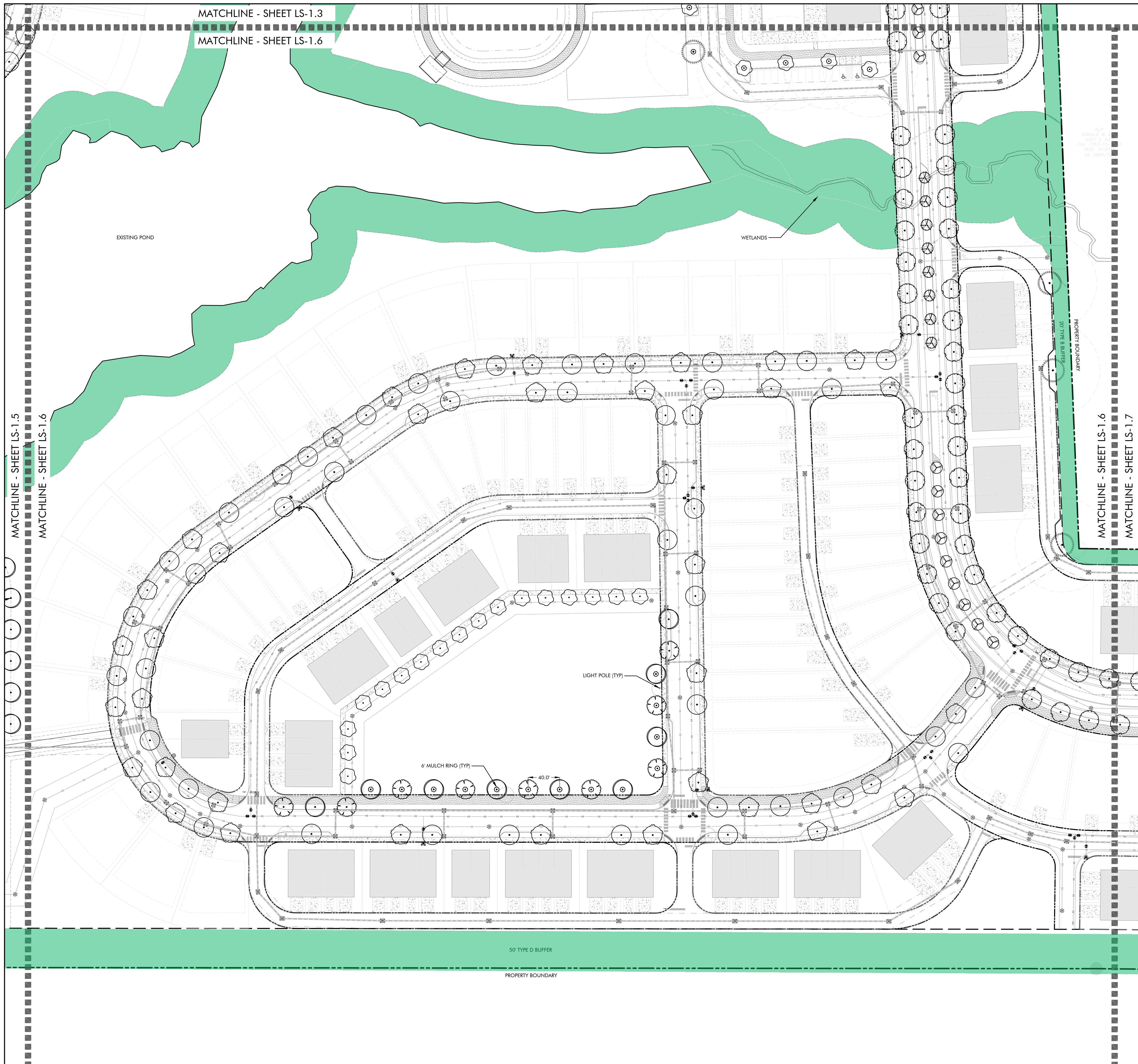
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Project No: 22-RDU-083

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Sheet No:

LS-1.5



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LYNDON OAKS

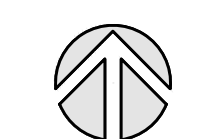
Master Plan

Landscape Plan Enlargement 6

Knightsdale, North Carolina



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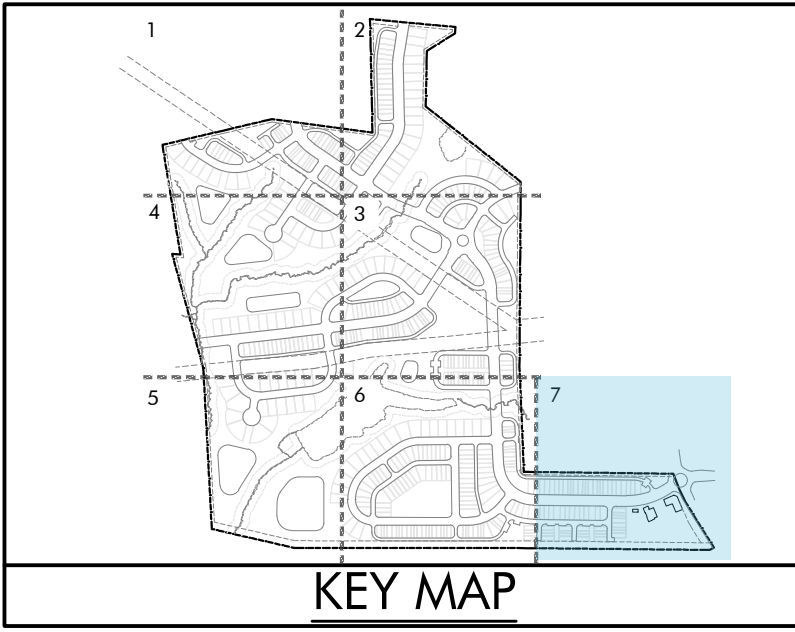
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LS-1.6



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- EXISTING PLANT MATERIAL TO BE PRESERVED IN ALL BUFFER AREAS AS PRIORITY TIER 1 TREE SAVE AREAS. WHERE SUFFICIENT MATERIAL DOES NOT MEET REQUIREMENTS, SUPPLEMENTAL PLANT MATERIAL WILL BE REQUIRED TO MEET BUFFER OPACITY STANDARDS.
 - FINAL LOCATION OF SUPPLEMENTAL BUFFER PLANTINGS TO BE COORDINATED WHEN CONSTRUCTION DOCUMENTS ARE DEVELOPED. SEE SHEET LS-2.0 FOR TYPICAL BUFFER SECTIONS.
 - ANY REQUIRED SCREENING AREAS THAT OVERLAP WITH SIGHT DISTANCE TRIANGLES WILL WAIVE PLANTING REQUIREMENTS AND USE GROUND COVER PLANTINGS.
 - ALL SIDES OF PARKING LOTS FRONTING RIGHT-OF-WAYS WILL BE SCREENED WITH A TYPE B BUFFER YARD PER UDO.
 - IF GRAPHIC REPRESENTATION OF PLANTINGS ON PLANS DO NOT MATCH QUANTITIES IN PLANT SCHEDULE, GRAPHIC REPRESENTATION OF PLANTINGS WILL GOVERN.
 - TREES NOT IN A LANDSCAPE BED SHALL RECEIVE A 6" DIAMETER MULCH RING.
 - LIMITS OF IRRIGATION INCLUDE DENOTED LANDSCAPE BEDS, ALL SOD/SEED AREAS, AND TREES NOT LOCATED IN A LANDSCAPE BED.
 - FULL LANDSCAPE SCHEDULE ON SHEET LS-2.0
 - LANDSCAPE SPECIES, LOCATION, AND QUANTITY IS SUBJECT TO CHANGE AT TIME OF CONSTRUCTION DOCUMENTS.



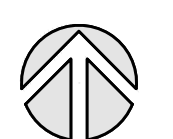
nc firm no: P-2671 sc coa no: C-03044
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 D.R. Horton
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 Raleigh, NC 27615

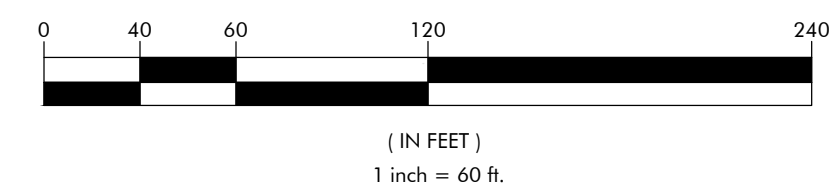
LYNDON OAKS
Master Plan
 Landscape Plan Enlargement 7
 Knightdale, North Carolina



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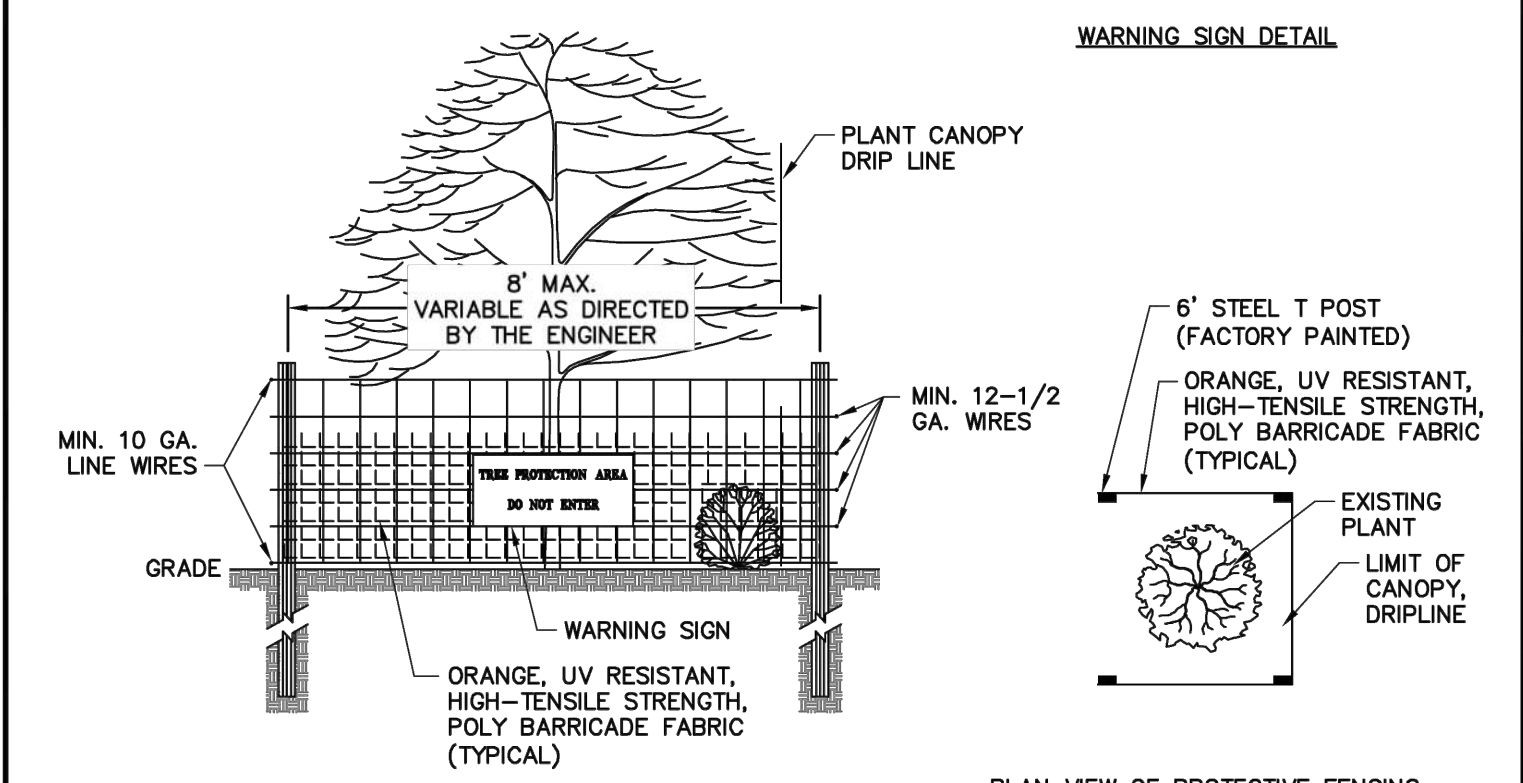
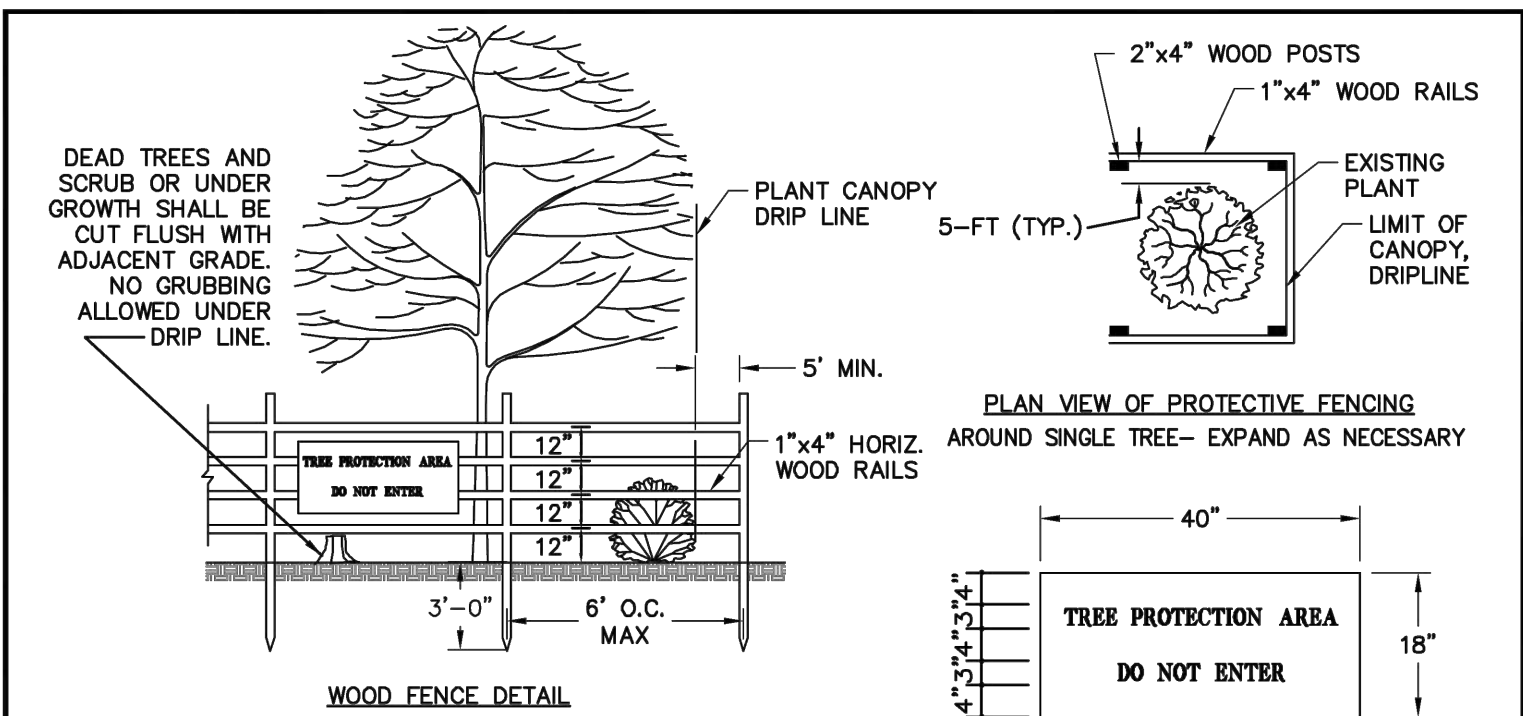
GRAPHIC SCALE



NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

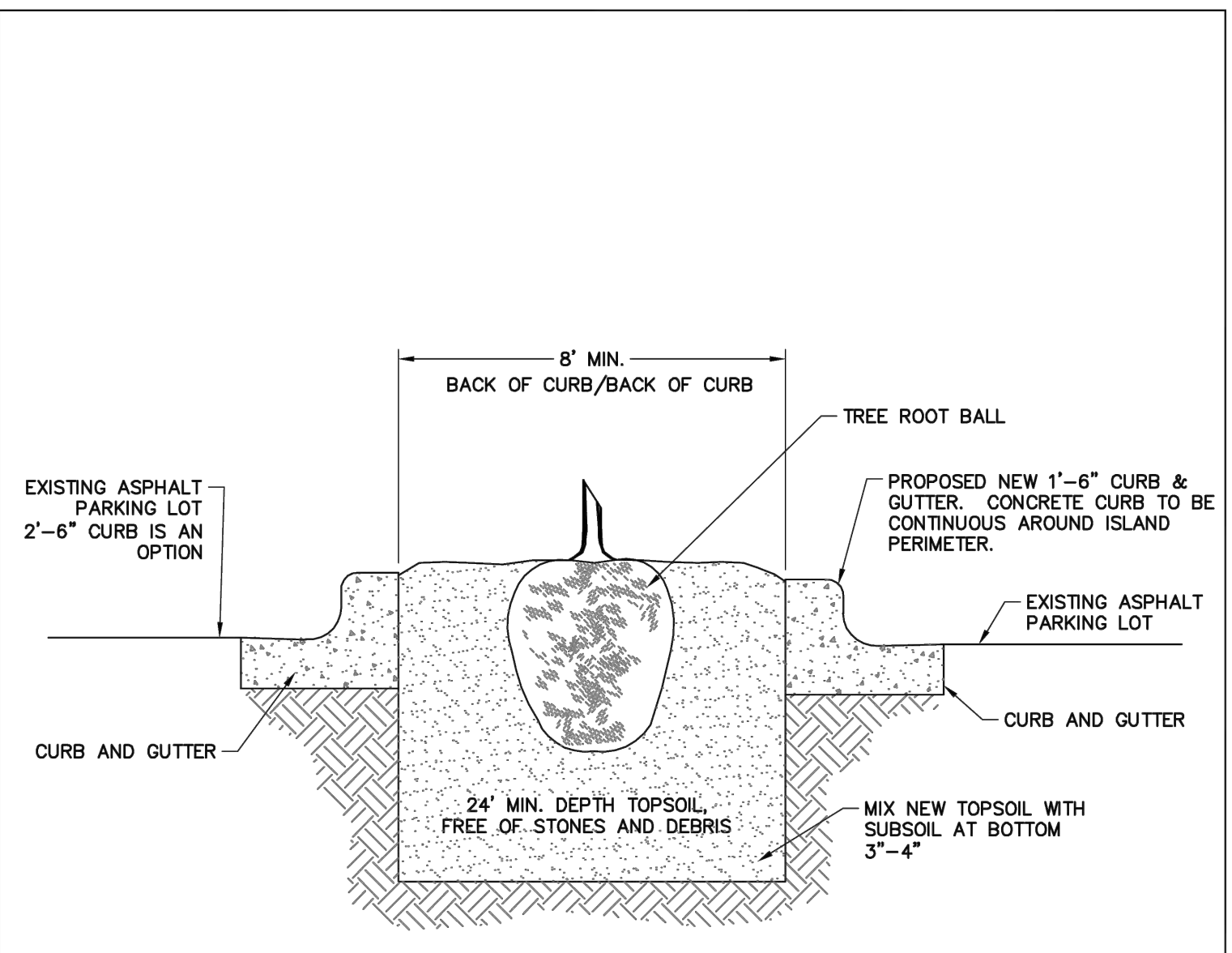
Project No: 22-RDU-083
 Date: 05/03/2024

Sheet No:
LS-1.7



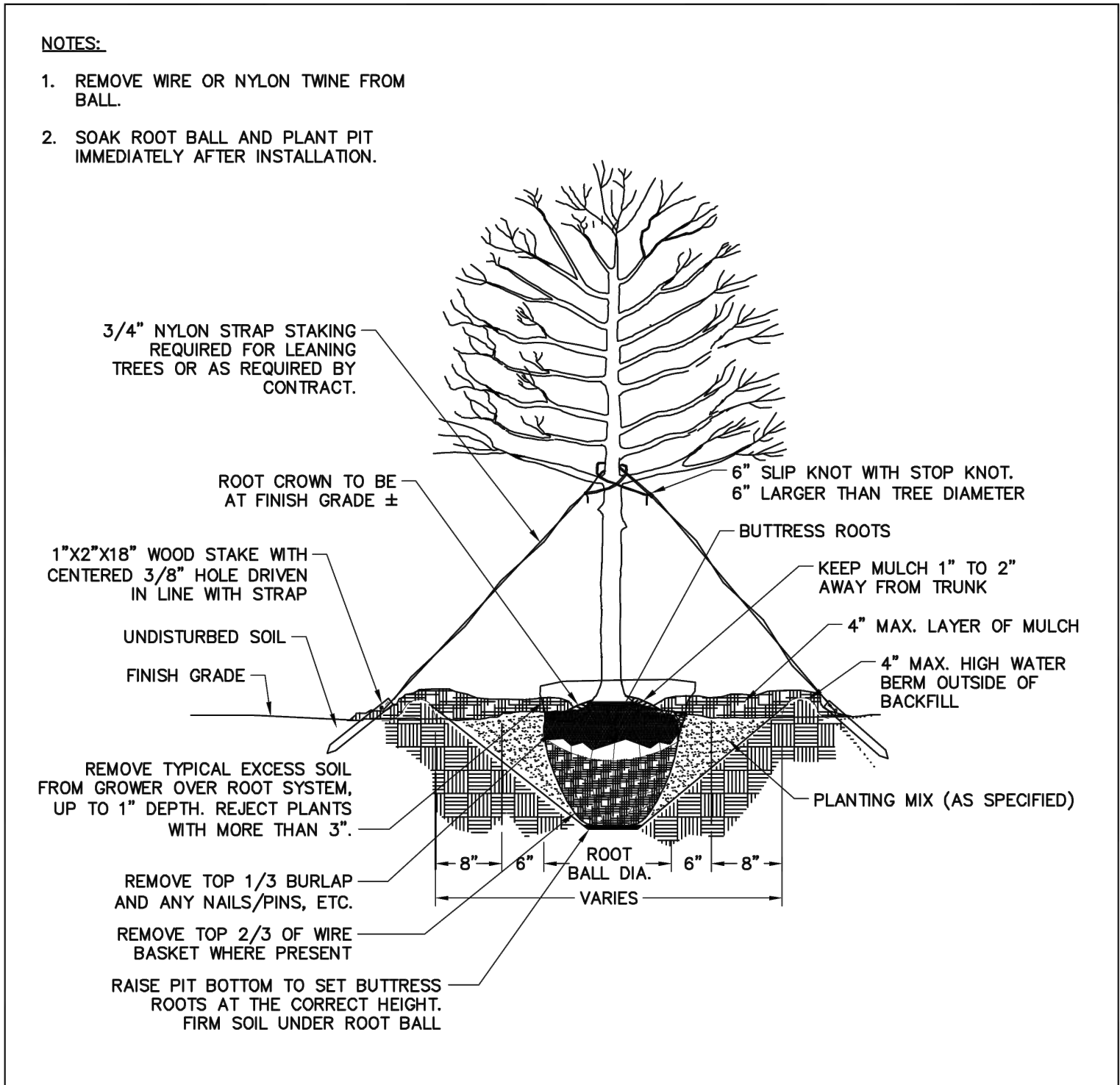
- NOTES:**
1. REMOVE ALL BARRIERS UPON COMPLETION OF PROJECT.
 2. LANDSCAPING PLANS SHALL SHOW LOCATIONS OF ALL TREE PROTECTION FENCES.
 3. ALL PLANTS TO BE SAVED SHALL BE PROTECTED BY FENCING AS SHOWN IN THIS DETAIL.
 4. CONTRACTOR SHALL INSTALL FENCING PRIOR TO BEGINNING ANY CONSTRUCTION OR GRADING ACTIVITY.
 5. CONTRACTOR SHALL CALL FOR INSPECTION AND APPROVAL OF PROTECTIVE FENCING PRIOR TO BEGINNING ANY CONSTRUCTION OR GRADING.
 6. PROTECTIVE FENCING SHALL BE LOCATED 5' OUTSIDE DRIFLINE OF TREES AND 1' MINIMUM OUTSIDE SHRUBS OR OTHER PLANTS.

REVISIONS		STD. NO.
DATE	DESCRIPTION	
TOWN OF KNIGHTDALE STANDARD DETAILS	PLANT PROTECTIVE FENCING	2.10



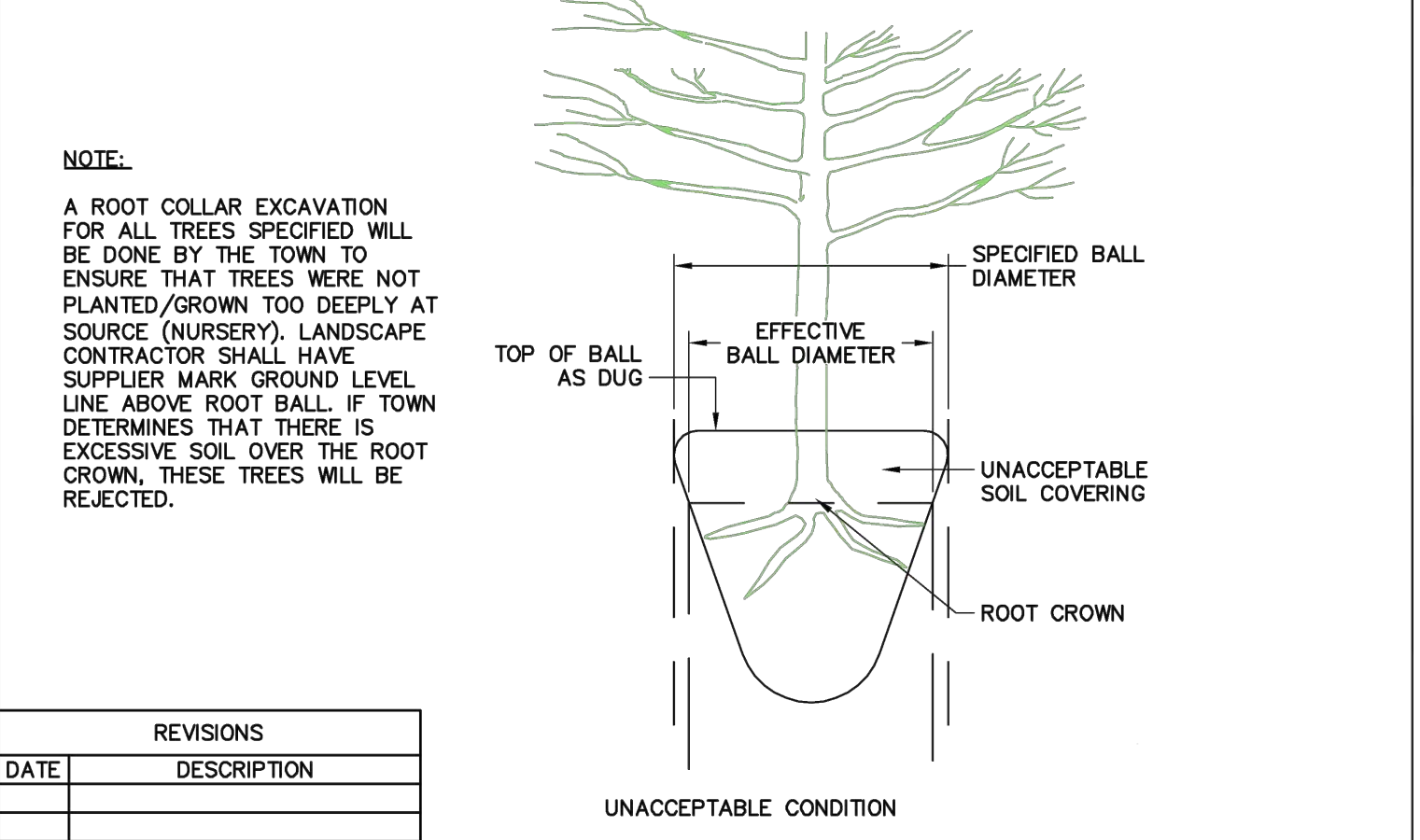
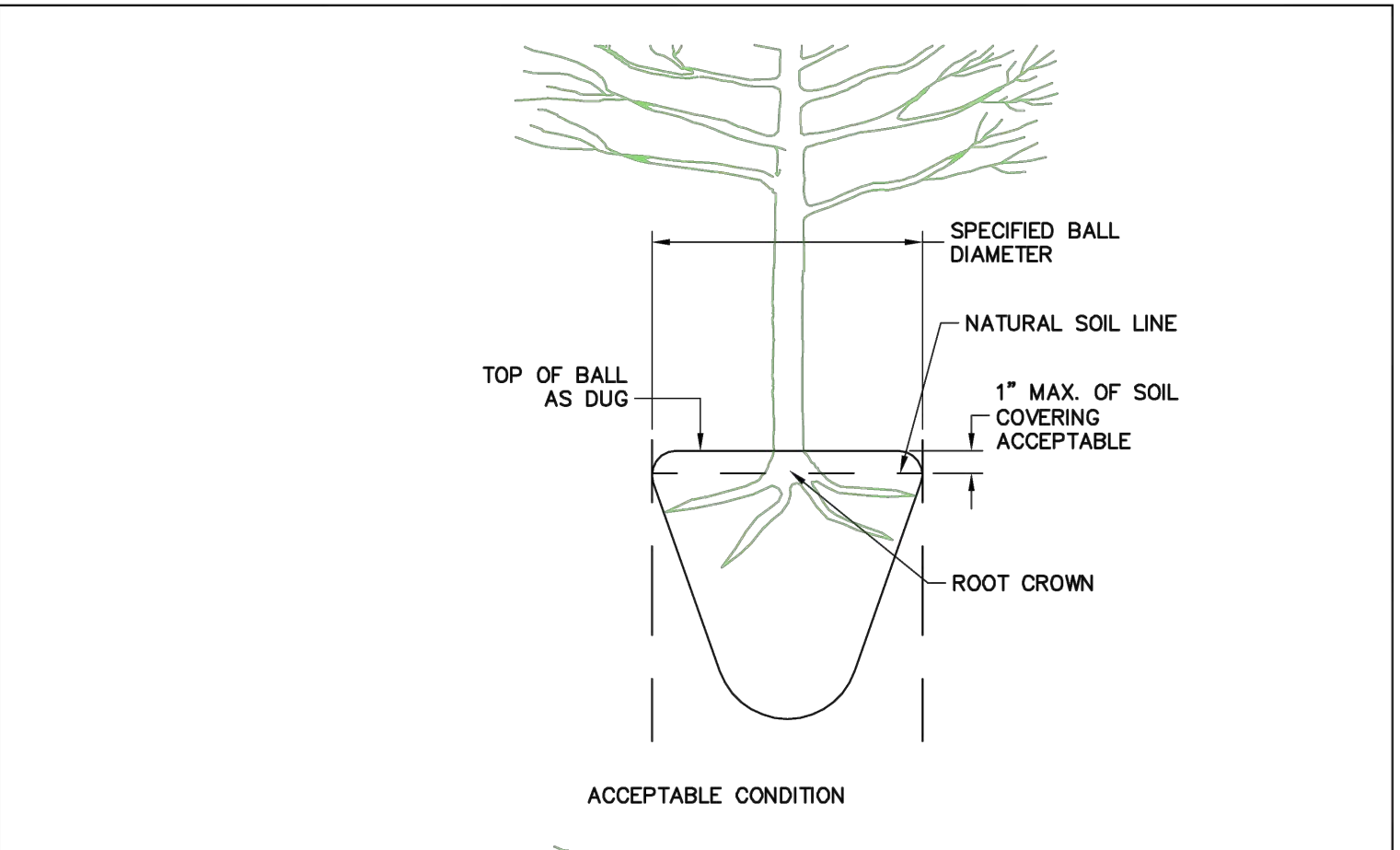
- NOTES:**
1. 300 SF MIN. TOTAL REQUIRED AREA PER TREE.
 2. FOR NEW PLANTING AREAS, REMOVE ALL PAVEMENT, GRAVEL SUB-BASE AND CONSTRUCTION DEBRIS BEFORE PREPARING SOIL AND PLANTING TREES.
 3. REMOVE COMPACTED SOIL AND ADD 24" NEW OR TOPSOIL/PLANTING MIX OR UNCOMPACT AND AMEND TOP 24" OF EXISTING SOIL TO MEET TOPSOIL/PLANTING MIX STANDARDS FOR TREES.

REVISIONS		STD. NO.
DATE	DESCRIPTION	
TOWN OF KNIGHTDALE STANDARD DETAILS	TREE PLANTING ISLAND IN PARKING LOTS	3.23



FOR EXAMPLE:	CALIPER	HEIGHT (RANGE)	MAX. HEIGHT	MIN. ROOT BALL DIA.	MIN. ROOT BALL DEPTH
	2"	12-14'	16'	24"	16"
	3"	14-16'	18'	32"	21"

REVISIONS		STD. NO.
DATE	DESCRIPTION	
TOWN OF KNIGHTDALE STANDARD DETAILS	TREE PLANTING (FOR SINGLE AND MULTI-STEM TREES)	3.25



- NOTE:**
- A ROOT COLLAR EXCAVATION FOR ALL TREES SPECIFIED WILL BE DONE BY THE TOWN TO ENSURE THAT TREES WERE NOT PLANTED/GROWN TOO DEEPLY AT SOURCE (NURSERY). LANDSCAPE CONTRACTOR SHALL HAVE SUPPLIER MARK GROUND LEVEL LINE ABOVE ROOT BALL. IF TOWN DETERMINES THAT THERE IS EXCESSIVE SOIL OVER THE ROOT CROWN, THESE TREES WILL BE REJECTED.

REVISIONS		STD. NO.
DATE	DESCRIPTION	
TOWN OF KNIGHTDALE STANDARD DETAILS	ROOT CROWN DEPTHS	3.21



150 Fayetteville at Ste 1310
Raleigh, NC 27601
+ 919.275.5022
urbanesignpartners.com

nc firm no: P.2671 sc coa no: C.03044
05/03/2024

D.R. Horton
7208 Falls of Neuse Rd. Ste. 201
Raleigh, NC 27615

LYNDON OAKS Master Plan Landscape Details

Knightsdale, North Carolina

NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083
Date: 05/03/2024
Sheet No:

LS-2.1




LIGHTING LEGEND

LIGHT POLE

* LIGHTING PLAN IS CONCEPTUAL IN NATURE & SUBJECT TO CHANGE AT THE TIME OF CONSTRUCTION DOCUMENTS.

Outdoor Lighting
SHOEBOX LED



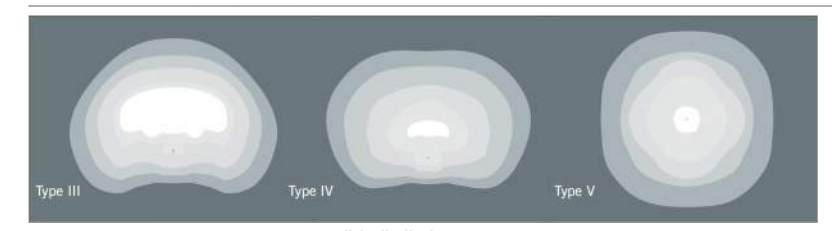
SHOEBOX LED

LED (Light-emitting diode)	150/220/420/530 watts
Mounting height	25' - 35'
Color	Black Bronze Gray (Special conditions)
Pole	Wood Style A

IESNA cutoff classification: Full-cutoff
Color temperature: 3,000K Primary
4,000K Available

FIXTURE	WATTS	LUMENS	PATTERN	BUG RATING
Shoebox 150	150	19,007	III	B2-U0-G3
		18,450	IV	B3-U0-G4
		19,580	V	B5-U0-G3
Shoebox 220	220	23,744	III	B3-U0-G4
		23,061	IV	B3-U0-G4
		24,461	V	B5-U0-G3
Shoebox 420	420	41,379	IV	B3-U0-G5
		45,868	V	B5-U0-G5
Shoebox 530	530	51,429	IV	B3-U0-G5
		57,009	V	B5-U0-G5

LIGHT DISTRIBUTION PATTERNS



POLE AVAILABLE	MOUNTING HEIGHT	FOUNDATION
Style A	25', 30', 35'	Direct Bury, Anchor Base
Wood	25', 30', 35'	Direct Bury

For additional information, contact us at ODLCarolina@duke-energy.com.

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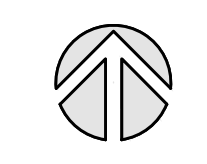
nc firm no: P-2671 sc coo no: C-03044
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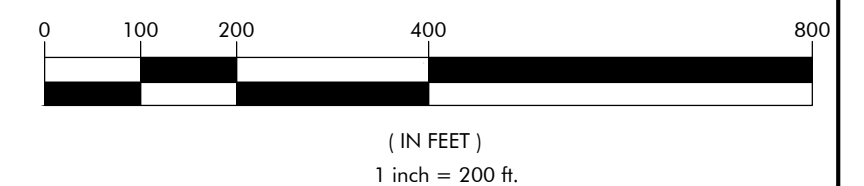
LYNDON OAKS
Master Plan
Lighting Plan
Knightsdale, North Carolina



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GRAPHIC SCALE



NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	09/05/2024		
#3	09/26/2024		

Project No: 22-RDU-083
Date: 05/03/2024

Sheet No:
LS-3.0



SINGLE FAMILY DETACHED

SCALE: NTS

ELEVATION



TOWNHOMES

SCALE: NTS

ELEVATION

NOTES:

ELEVATIONS SHOWN ARE CONCEPTUAL IN NATURE AND SUBJECT TO CHANGE.
SEE ZMA-2-23 FOR LIST OF ARCHITECTURAL STANDARDS.



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nc firm no: P-2671 sc coo no: C-03044

PRELIMINARY DRAWING
FOR REVIEW PURPOSES ONLY

05/03/2024

D.R. Horton

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LYNDON OAKS
Master Plan

Architectural Elevations

Knightsdale, North Carolina

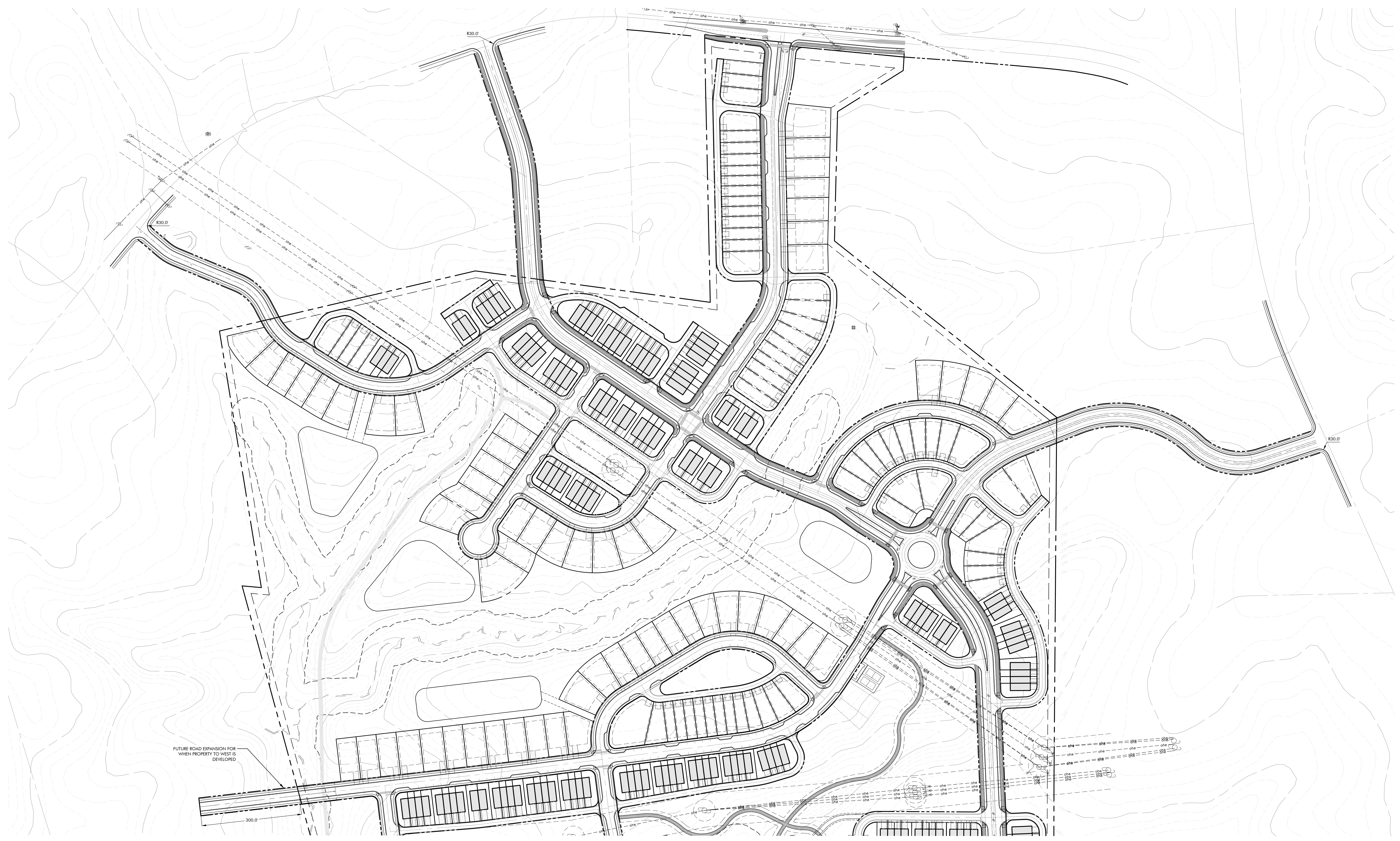
NO.	DATE	BY	REVISIONS:
#1	11/27/2023		
#2	02/05/2024		
#3	03/28/2024		

Project No: 22-RDU-083

Date: 05/03/2024

Sheet No:

A-1.0



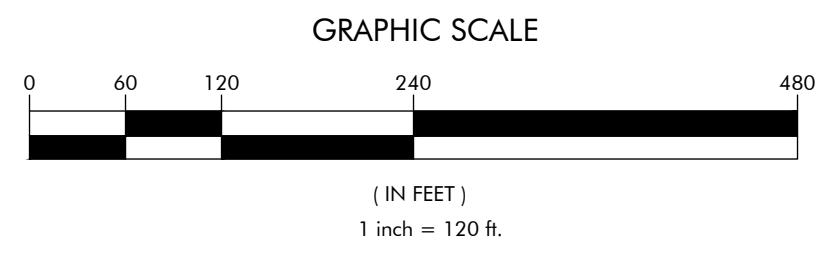
FUTURE ROAD EXPANSION FOR WHEN PROPERTY TO WEST IS DEVELOPED



LYNDON OAKS

KNIGHTDALE, NC

FUTURE ROAD CONNECTION EXHIBIT



1" = 120'
 FEBRUARY 27, 2024
 22-RDU-083

LYNDON OAKS

RMX PLANNED UNIT DEVELOPMENT

The Town of Knightdale
Case Number: ZMA-2-23
May 3, 2024



URBAN
DESIGN
PARTNERS

LANDSCAPE ARCHITECTURE + CIVIL ENGINEERING

LYNDON OAKS

RMX PLANNED UNIT DEVELOPMENT

PREPARED FOR
The Town of Knightdale

DEVELOPER
DR Horton
7208 Falls of Neuse Road
Suite 201
Raleigh, NC 27615

PROJECT TEAM
Urban Design Partners
Planning | Landscape Architecture | Civil Engineering
150 Fayetteville Street
Suite 1310
Raleigh, NC 27601

Parker Poe
Land Use Attorney

Ramey Kemp & Associates
Traffic Engineers

Bateman Civil Survey Company
Surveyors

Sage Ecological Services, Inc.
Environmental Consultant

SUBMITTAL DATE
May 3, 2024

URBAN DESIGN PARTNERS



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01

A grayscale photograph of a man walking a white, fluffy dog on a leash. The man is wearing a dark jacket and pants, and is looking towards the right. The dog is walking on a sidewalk. In the background, there is a two-story house with a porch and several tall, thin evergreen trees. The overall scene is in grayscale, with the text overlaid in white.

**VISION
+ INTENT**

Community Vision

Lyndon Oaks is envisioned as a place of community. The development team believes that through **quality placemaking**, a **mix of uses**, and a **variety of housing options**, will arise a place of creativity and diversity that will reveal a vibrant and active community. Lyndon Oaks will be a community that both embraces and engages nature by providing a network of trails and open space that encourages social interactions and physical activity.



Community Intent

Section 2.4.C. of the Town of Knightdale Unified Development Ordinance states that, “the Planned Unit Development Overlay District process **encourages creativity and innovation** in the design of developments through a master planning process that allows for flexibility from underlying zoning as approved by the Town Council.” These developments bring “**tangible benefits**” to the Town, including “exceptional amenities, outstanding environmental, landscape, architectural, or site design, or conservation of special man-made or natural features of the site.”

Lyndon Oaks meets the intent of the **Planned Unit Development Overlay District** requirements per Town of Knightdale UDO Section 12.2.G.3.g.ii.a. through the following findings:

» Comprehensive Plan

- Lyndon Oaks conforms to the planning policies of the Town as laid out in the KnightdaleNext2035 Comprehensive Plan. The site is designated as a Mixed-Density Neighborhood place type, in a Target Investment Area, and the planned variety of residential densities and neighborhood-serving commercial meet the Town’s future planned growth. In addition, Lyndon Oaks is a neighborhood that exemplifies many of the Guiding Principles for growth and development in Knightdale (Guiding Principles found on page 9).

» Public Welfare

- Lyndon Oaks is maintaining the riparian buffer that bisects the site and large existing pond to allow for maximized preservation of natural features that will be not only preserved, but amenitized through walking trails and greenways to be enjoyed by residents and the surrounding community on public greenways and sidepaths. The supply of light and air to adjacent properties will not be negatively impacted due to the tree preservation, preserved open space, and buffer plantings around the perimeter of the site. There will be no increased danger of fire to current or future residents in the area, and the public health, safety, and welfare will not only be protected but will increase through the new pedestrian and vehicular connectivity, intersection design and improvements that prioritize safety, ample behind-the-curb multi-use paths for cyclists, fountains in the SCMs to keep water aerated, and new public utilities being brought to the area.

» Impact on Other Property

- Lyndon Oaks is primarily a mixed-density residential community. The adjacent land uses consist of existing single family residences and vacant wooded lots. Lyndon Oaks consists of like uses and helps to provide increased connectivity and access to amenities like the greenway trail and a future neighborhood-serving commercial amenity on Bethlehem Road.

» Impact on Public Facilities & Resources

- Lyndon Oaks will provide adequate utilities, road access, drainage, and other necessary facilities to properly serve residents of the site. The project will dedicate a large section of the planned Widewaters Parkway extension to the Town, and plans to provide the greenway connectivity as well.

» Archaeological, Historical, or Cultural Impact

- Lyndon Oaks is maintaining the riparian buffer that bisects the site and large existing pond to allow for preservation of natural features on the property. No archaeological, historical, or cultural resources will be adversely impacted in the development. Lyndon Oaks will preserve and enhance existing natural resources on site for the benefit of the residents and surrounding community that utilizes the greenway.

» Parking & Traffic

- Lyndon Oaks is a pedestrian friendly community. The mix of housing types and rear-loaded units minimizes pedestrian-vehicular conflicts and creates a safer, more appealing streetscape. Proper sight distance triangles are also used at intersections to prevent conflicts when turning a vehicle. All street radii are adequate for emergency vehicle access. There is ample guest parking through the use of on-street parking. Traffic impacts from Lyndon Oaks have been studied and will be mitigated through TIA recommended improvements that will benefit the entire area and alleviate some existing congestion issues. Furthermore, the public roads running through the site will create additional connectivity and routes on local, multimodal streets to existing roads in the area. The public and private trails also provide a safe public pedestrian connection throughout Lyndon Oaks.

» Appropriate Buffering

- A 20’ Type B Buffer is proposed along the perimeter of Lyndon Oaks to provide visual and acoustical privacy between Lyndon Oaks and the surrounding neighbors. A Type B Buffer is also provided between the parking lot at the commercial parcel and Bethlehem Rd. per requirements set forth in the UDO.
- A 50’ Type D Buffer with a 6’ earthen berm and dense plantings is proposed along I-87 to provide visual and acoustical privacy between Lyndon Oaks and the adjacent interstate. The maintained riparian buffers in the center of the site will also create a buffer within the community itself to allow for a greater sense of privacy between houses and a sense of place, such that each section of the neighborhood, each home, is a stone’s throw from the natural environment and open space.

» Performance

- D.R. Horton is America’s largest homebuilder, and the local team that have proposed this project have a proven track record of building successful residential neighborhoods in surrounding communities, as well as in Knightdale, most recently with the Haywood Glen community. The D.R. Horton team is excited to bring a new neighborhood to the Town that reflects the Town’s plans and aligns with the Town’s and D.R. Horton’s shared vision for great neighborhoods.

02



KNIGHTDALENEXT
2035

Comprehensive Plan Consistency

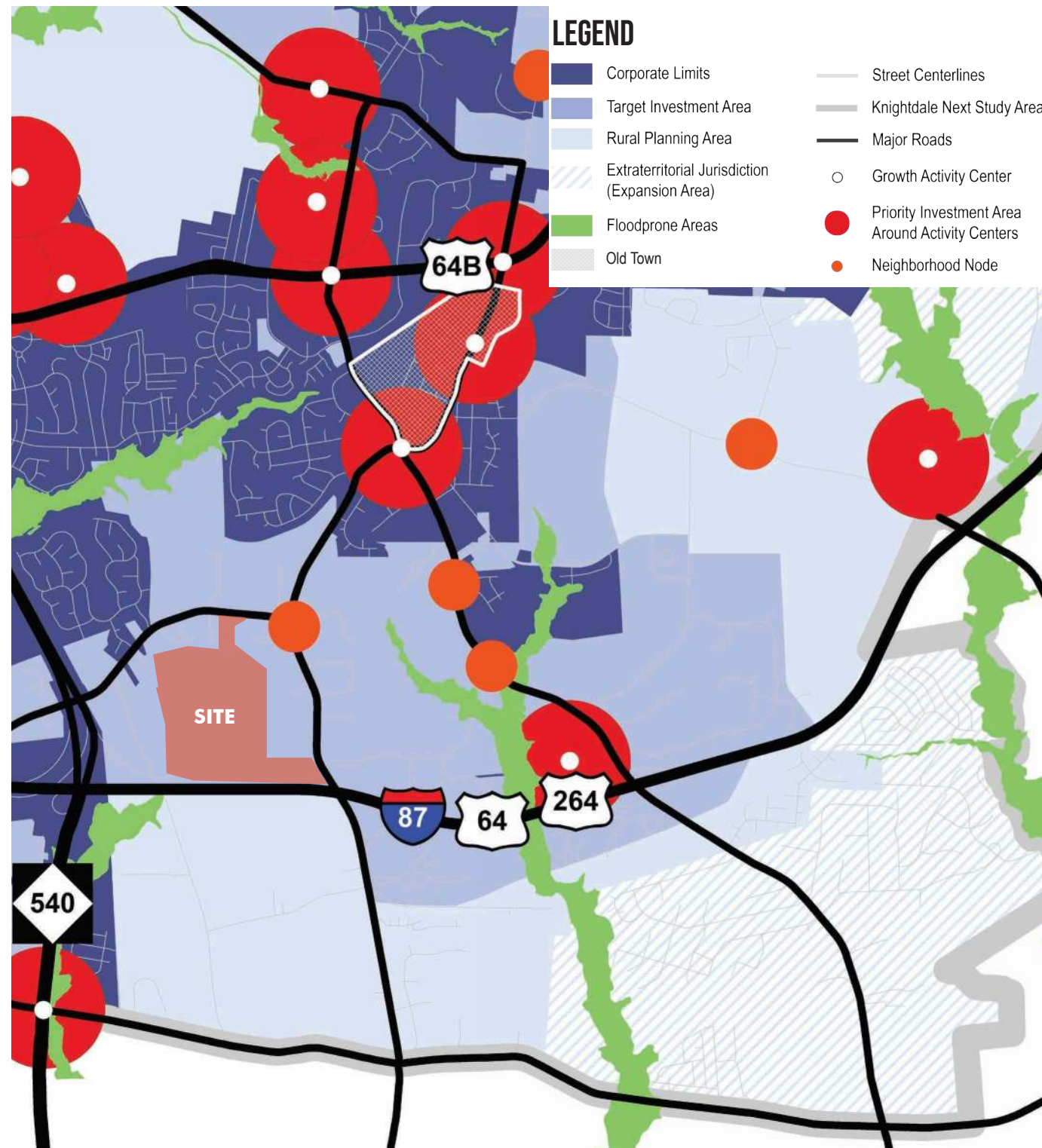


Figure 1: Growth Framework Map from KnightdaleNext2035 Comprehensive Plan

Growth Framework

This PUD request is proposing a rezoning from RT to RMX-PUD to better meet the desired outcome laid out in the KnightdaleNext2035 Comprehensive Plan.

On the Growth Framework Map, Lyndon Oaks is located near a Neighborhood Node at the intersection of Old Faison and Bethlehem Roads, and the site is within a Target Investment Area as identified in the Growth Framework Plan. Target Investment Areas are areas with existing or relatively easy potential access to water and sewer, where growth is supported, and the development will bring additional water and gravity sewer lines to this area to support continued growth and development. The plan says growth in these areas should encourage active living with a complete and comprehensive network of walkable streets in a general grid pattern. It also says that growth in these areas should be guided into more compact and efficient development patterns to justify infrastructure investments. Thus, Lyndon Oaks, which proposes compact residential growth at less than 3 units per acre and neighborhood-oriented commercial uses, in an area that will only continue to feel more growth pressure with the forthcoming I-540 extension, is supported by the Growth Framework Map.

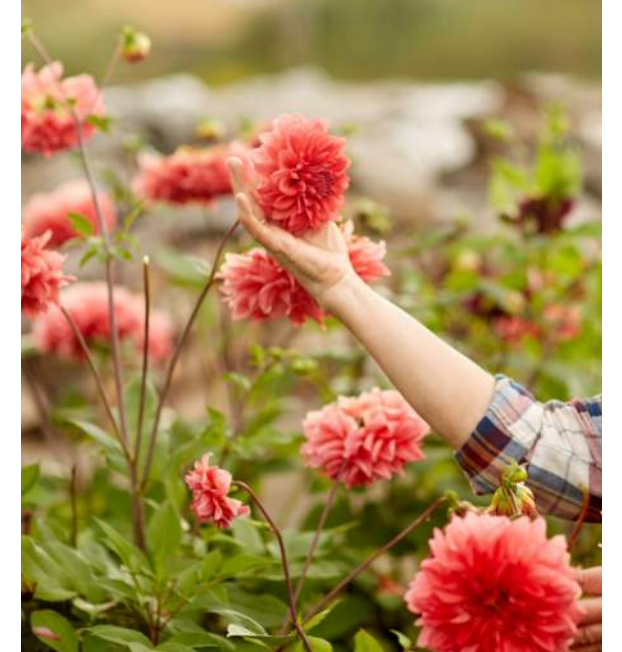
Lyndon Oaks aligns with Knightdale’s vision of neighborhoods throughout the Town feeling a sense of connection. A public greenway trail will be added throughout the site. This aligns with the Sidepaths & Greenways Plan plan found in UDO Appendix B and incorporated into the comprehensive plan. Sidewalks and greenway trails promote pedestrian walkability and connection for the community.

The proposed plan for Lyndon Oaks provides a mix of housing types to promote a diverse and walkable community. The mixed-use community will act as a transition for residential neighborhoods while providing a community-serving commercial outparcel

Comprehensive Plan Consistency

Comprehensive Plan - Guiding Principles

1. **Natural Environment:** Lyndon Oaks promotes and expands opportunities for community residents to enjoy Knightdale's greenways and activates the site's many streams, wetlands, and natural features by preserving them and giving residents access to enjoy those resources.
2. **Parks and Recreation:** Lyndon Oaks will provide the Town's planned pedestrian and cyclist public greenway connectivity from Old Faison Road to Bethlehem Road.
3. **Transportation:** Lyndon Oaks will provide a significant portion of the planned Widewaters Parkway extension, which will eventually provide an important connection from US-64 Business to Bethlehem Road and Crosscut Place. In addition, all of the streets in the neighborhood have been designed with multiple modes of transportation in mind—the project will provide ample bicycle and pedestrian facilities for destination - and recreation- focused trips.
4. **Compact Development Patterns:** The proposed community provides appropriate residential density and neighborhood-serving commercial in a Target Investment Area, near I-87 and I-540, which will become an even more convenient regional access route when the I-540 extension is built.
5. **Community Design:** Lyndon Oaks is a significant private investment that incorporates the distinctive elements of Knightdale. Roundabouts at intersections, strict architectural standards and commitments to beautiful and quality homes, pedestrian prioritization, preservation of open space and unique public and private recreational amenities dominate the community design.
6. **Great Neighborhoods and Expanded Home Choices:** Lyndon Oaks offers a wide variety of housing choices and lot sizes, mixed throughout the neighborhood, and not segregated to certain portions. This will provide opportunities for people and families at all ages and stages, with many different housing price-points, to find a place in this beautiful, heavily-amenitized community.



Comprehensive Plan Consistency

Growth and Conservation Map

On the Growth and Conservation Map, Lyndon Oaks is designated as a **“Mixed-Density Neighborhood.”** These are described as “subdivisions or communities with a mix of housing types and densities.” These Neighborhoods should “incorporate a comprehensive network of open space throughout to accommodate small parks, gathering places and community gardens; preserve tree stands; and help reduce stormwater runoff.”

Lyndon Oaks is consistent with this vision. The community will have a variety of housing options and price points, with six different lot sizes for single-family detached homes, both front and rear loaded, as well as three lot sizes for rear-loaded townhomes. These housing types are not segregated to their own distinct sections of the neighborhood; rather, they have been mixed in throughout the design. Beautiful gathering spaces and amenities will be programmed throughout the community, and the most sensitive natural areas will be preserved and activated.

It is also close to and will help support the planned desired land uses around the subject site. The retail and commercial amenities desired at the **“Neighborhood Node”** located at the intersection of Bethlehem and Old Faison Road will be much more viable with a new residential community so close, and the road improvements that will come with the project will help support desired development patterns in this area as well.

PLACETYPE CATEGORIES

 Preserved Open Space	 Business Office
 Rural Living	 Light Industrial
 Recreation Open Space	 Heavy Industrial
 Single Family Neighborhood	 Civic & Institutional
 Mixed-Density Neighborhood	 Old Town
 Multifamily Community	 Mixed-Use Center
 Neighborhood Node	 Mixed-Use Center (Suburban Retrofit)
 Retail	 Transit-Oriented Development (BRT)
	 Regional Mixed-Use Center

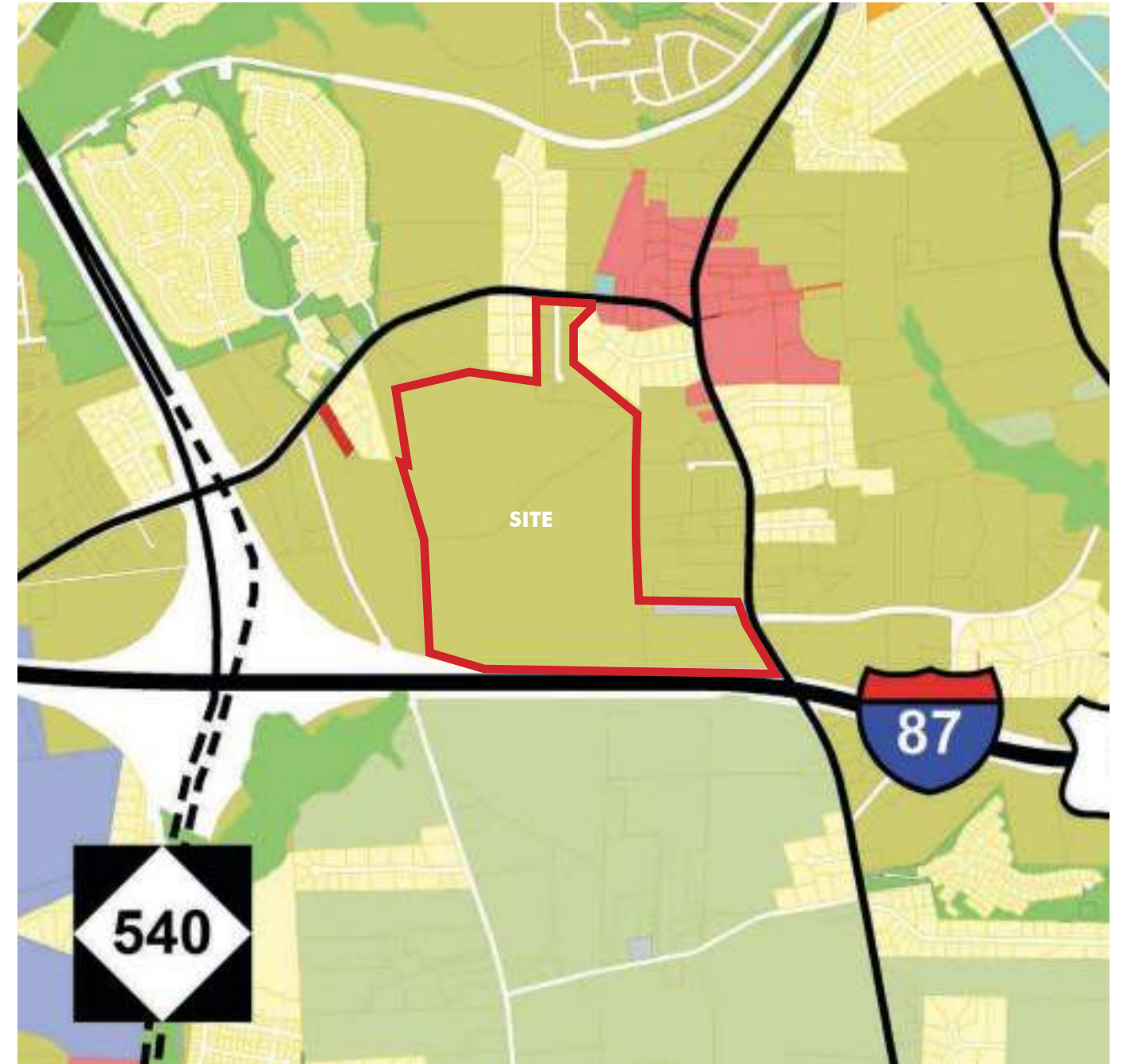


Figure 2: Growth and Conservation Map from KnightdaleNext2035 Comprehensive Plan

Comprehensive Plan Consistency

Transportation

Lyndon Oaks furthers the goals of **Knightdale's Comprehensive Transportation Plan (CTP)** in several ways.

First, with this project, an additional road connection will be made between Old Faison Road (at Tart Farm Road) and Bethlehem Road (at Crosscut Place), creating instant additional connectivity in the area. Additional road connections create more routes for drivers and ease congestion at nearby intersections, and create local streets that are safer for walking and biking, making these more viable forms of transportation. These are just a few of the reasons the CTP encourages additional local connector streets like those planned in Lyndon Oaks.

Second, Lyndon Oaks will construct ± 0.8 miles of the planned Widewaters Parkway extension through the project area, which is a key part of a larger planned connection of Widewaters Parkway to Cross Cut Place on the Roadway Network Plan. When completed across Old Faison Road to the north, this road will make it much easier for residents traveling to and from US 64. The Master Plan has placed the Widewaters Parkway stub in a location that will make it easy to angle in a way that will sync with the alignment of the extension to the north.

Lyndon Oaks also supports the CTP goal of making new streets multimodal and accessible for pedestrians and cyclists. Sidewalks will line both sides of every planned street to enhance pedestrian safety and connectivity in the neighborhood, many of them wider 6' and 10' sidewalks. Buffered bike lanes will line both sides of Widewaters Parkway and additional pedestrian safety infrastructure will be provided at pedestrian crossings across Widewaters Parkway. Lyndon Oaks will also make pedestrian crossings on Widewaters Parkway safer by providing Rectangular Rapid Flash Beacons (RRFBs) at the private trail crossing and the entrance road (Tart Farm Road) intersection.

Finally, Lyndon Oaks has studied the projected transportation impact of the development on numerous roads and intersections, and will implement recommended improvements to mitigate those impacts and improve area roads, consistent with the CTP policy of "account[ing] for broader impacts of development on the surrounding mobility networks." See TIA summary in the Infrastructure section of this document.

Legend

	Comprehensive Transportation Plan Planning Area
Street Type & Status:	
	Freeway, Existing
	Freeway, Proposed
	Boulevard, Existing
	Boulevard, Proposed
	Urban Avenue, Existing
	Urban Avenue, Proposed
	Avenue, Existing
	Avenue, Proposed
	Urban Main Street, Existing
	Urban Main Street, Proposed
	Main Street, Existing
	Main Street, Proposed

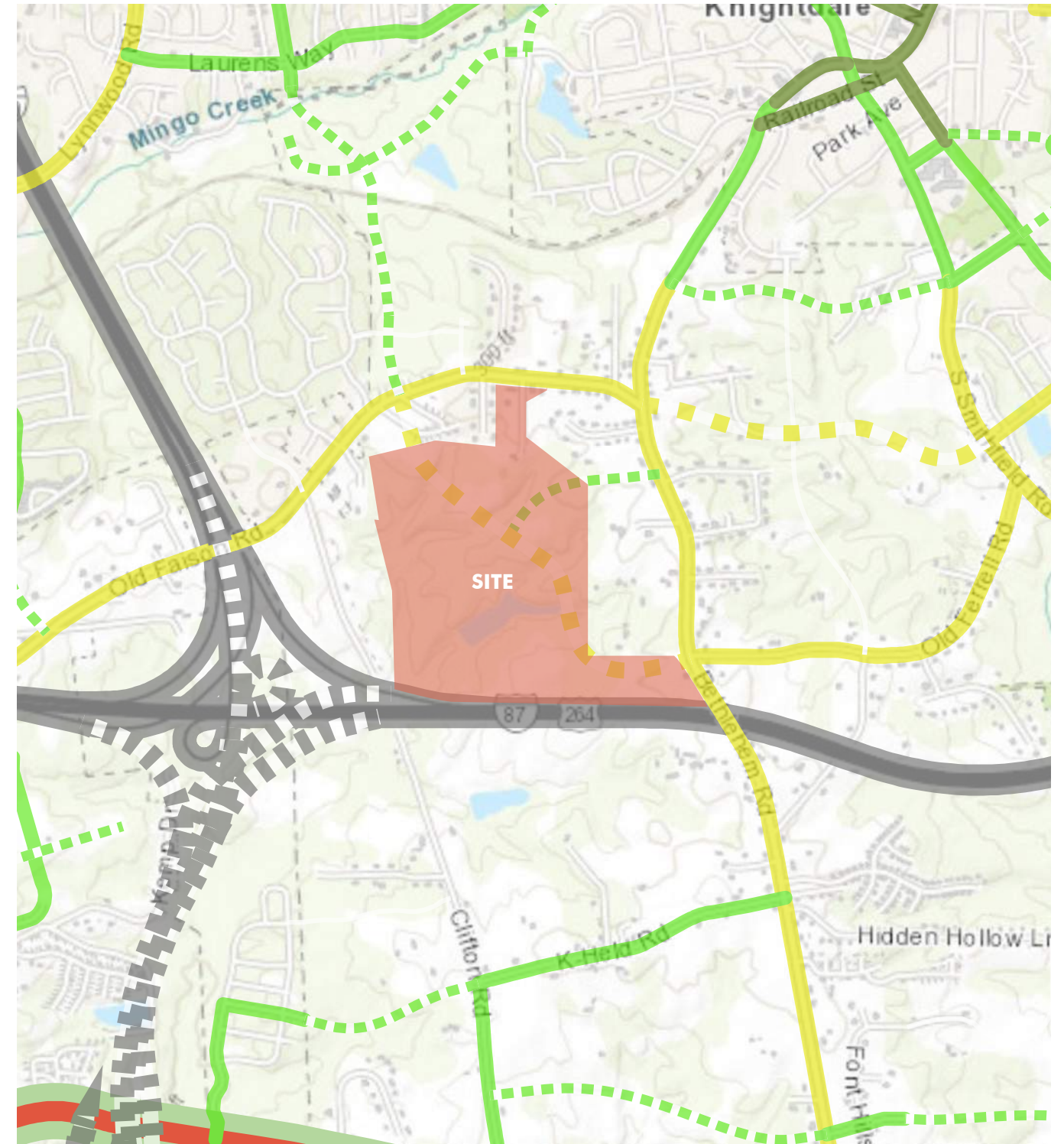


Figure 3: Roadway Network Plan from the Knightdale UDO

Comprehensive Plan Consistency

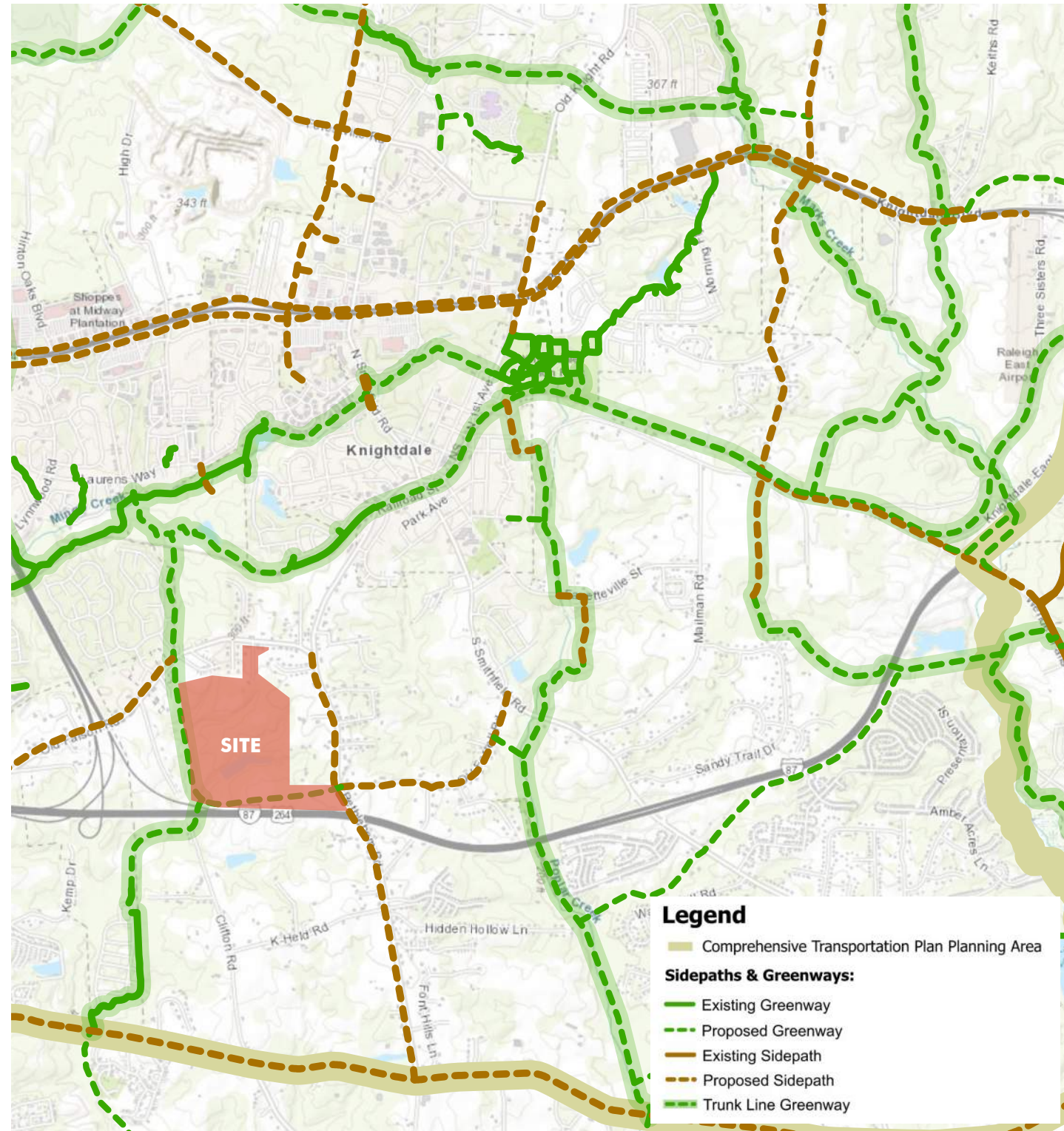


Figure 4: Sidepaths & Greenways Plan from the Knightdale UDO

Trails & Greenways

Lyndon Oaks will provide a network of public greenways and private trails in its open space, as well as recreational amenities while maintaining existing natural features. The plan also provides safe and convenient pedestrian and bicycle access for nearby residents through the use of sidewalks, buffered bike lanes on both sides of Widewaters Parkway, and 10' sidewalks on Tart Farm Road as well, and public and private trails in the natural areas to connect the neighborhood's residential sections.

Pet waste stations and benches will be provided along public greenways and private greenways with public access easements every 2,000 linear feet. Signage shall be provided where the public greenway intersects with other greenways. Signage, bench design, and pet waste station design shall adhere to applicable Town Standard Specifications.

Environmental Impacts and Permitting: The Town of Knightdale has planned substantial street and public greenway improvements on a site with many environmentally-sensitive streams and wetlands that will require extensive permitting through state and federal agencies at the next stage of development, and developer will provide the Town's desired public improvements to enhance vehicular and pedestrian connectivity in this area. Developer will provide and will facilitate environmental permitting for the stream and wetlands impacts of the planned Widewaters Parkway extension. Developer will also pursue environmental permitting for a greenway alignment that is substantially similar to the "Option A" alignment of the greenway shown on the Master Plan. If the US Army Corps of Engineers (USACE) defines the "project" in such a way that the project will cause a cumulative stream and wetland impact of 0.5 acres or more, the developer may adjust plans to reduce stream and wetlands impacts by the following:

- » Providing the greenway alignment shown in Option B
- » Providing boardwalks instead of paved bridges over streams and wetlands
- » Removing the turn lane and narrowing the Widewaters Parkway section in the areas where it crosses streams and wetlands
- » Providing the planned public greenway connection from Old Faison Road to Bethlehem Road as a side path on Widewaters Parkway, with private natural trails in the natural areas that connect residential areas of the neighborhood
- » Any other minor plan adjustments approved by Town staff that do not include impacts to planned residential lots.



Comprehensive Plan Consistency

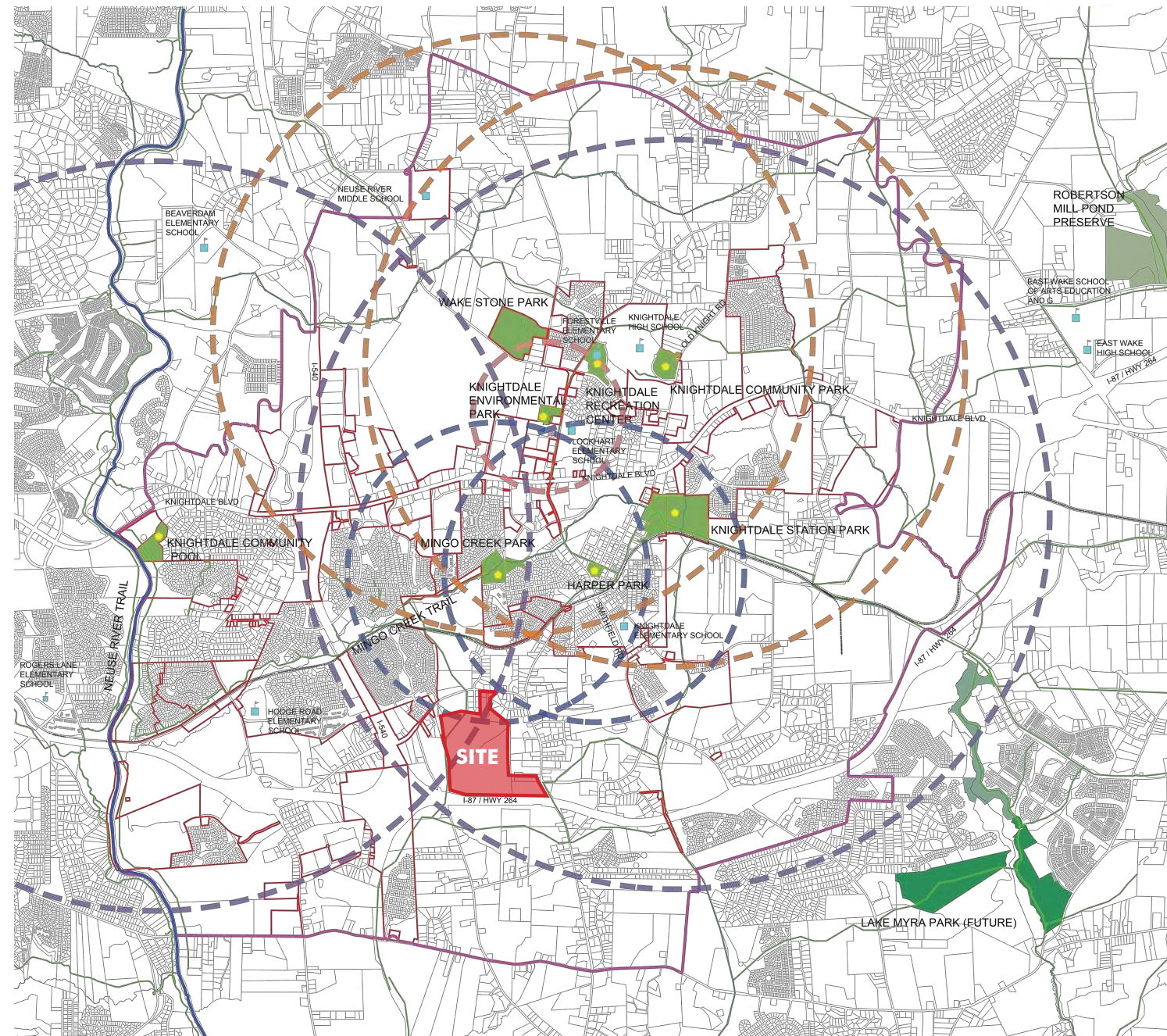


Figure 5: 2021 Town of Knightdale Comprehensive Parks and Rec. Master Plan

Parks and Recreation

Consistent with the **Comprehensive Parks and Recreation Master Plan**, Lyndon Oaks is within the two-and-a-half mile service area of Knightdale Station Park, and part of the neighborhood will be in the one-mile service area of Mingo Creek Park. When the Widewaters Parkway extension is completed to the north, Lyndon Oaks residents will be able to easily walk or bike to this park. The ample amenities, open space, and approximately ± 6.5 miles of trails and sidewalks within the planned community will also provide recreational opportunities for residents.

03

**EXISTING
CONDITIONS**

Vicinity Map

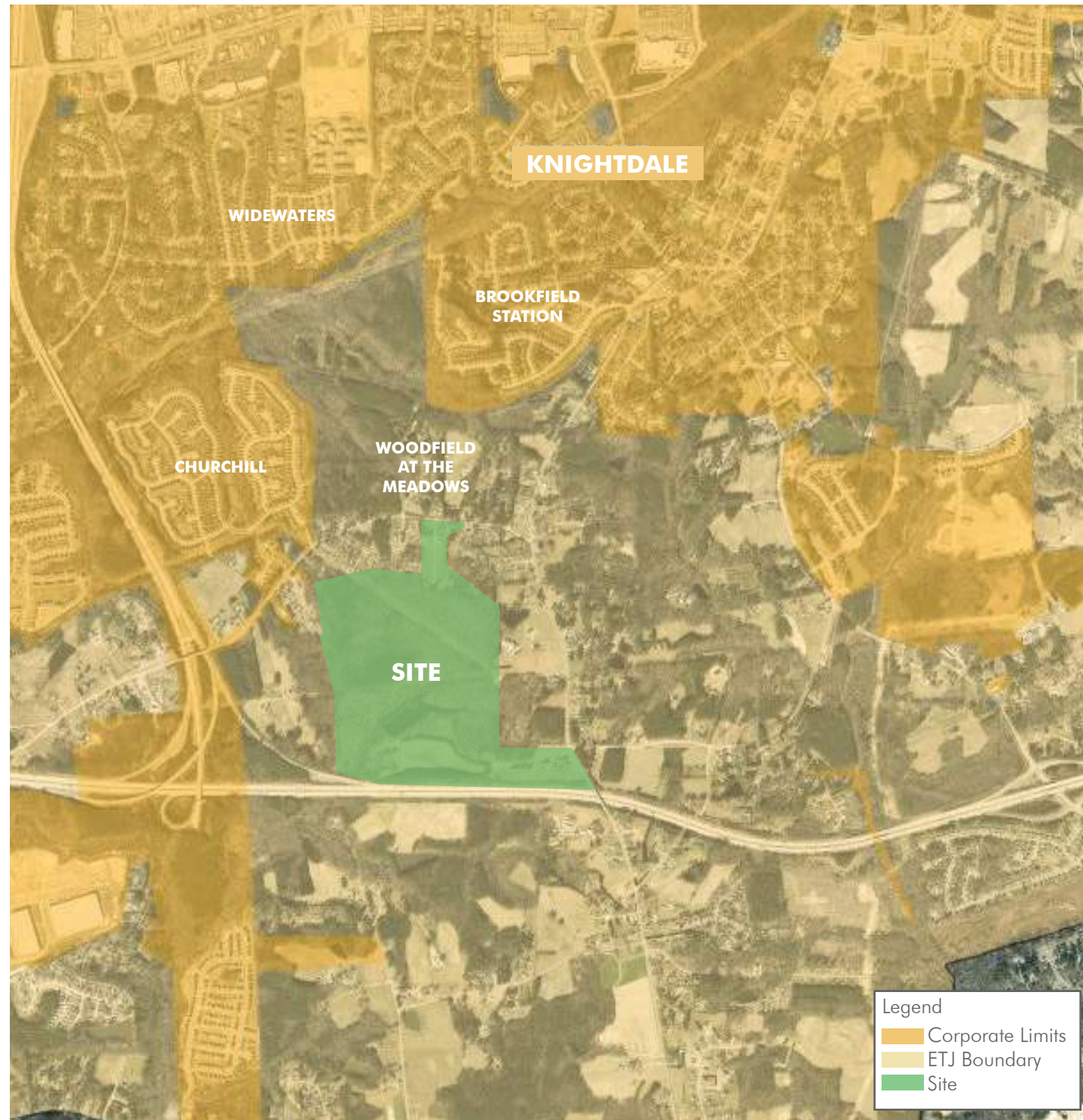


Figure 6: Vicinity Map (Not to Scale)

Vicinity Map

The site is within **Knightdale's ETJ**, and approximately 150 feet from the primary corporate limits at the northwest corner. Most of the site is undeveloped, except for a few larger homes on the portions of the site that front Old Faison Road and Bethlehem Road. The largest portion of the property is bisected by multiple streams and wetlands, and features a large natural pond. Two overhead utility easements also run from east to west through the property.

Uses in the vicinity are almost exclusively residential, as is the current Knightdale ETJ zoning. Residential uses vary in size and housing type. Immediately to the north on Bethlehem Road is a mobile home park. Very large residential lots lie to the east, and the parcel immediately to the west on Clifton Road is vacant. Churchill, an established Knightdale neighborhood, is located to the northwest of the site across Old Faison Road, and it is zoned GR8.

Current Zoning Map

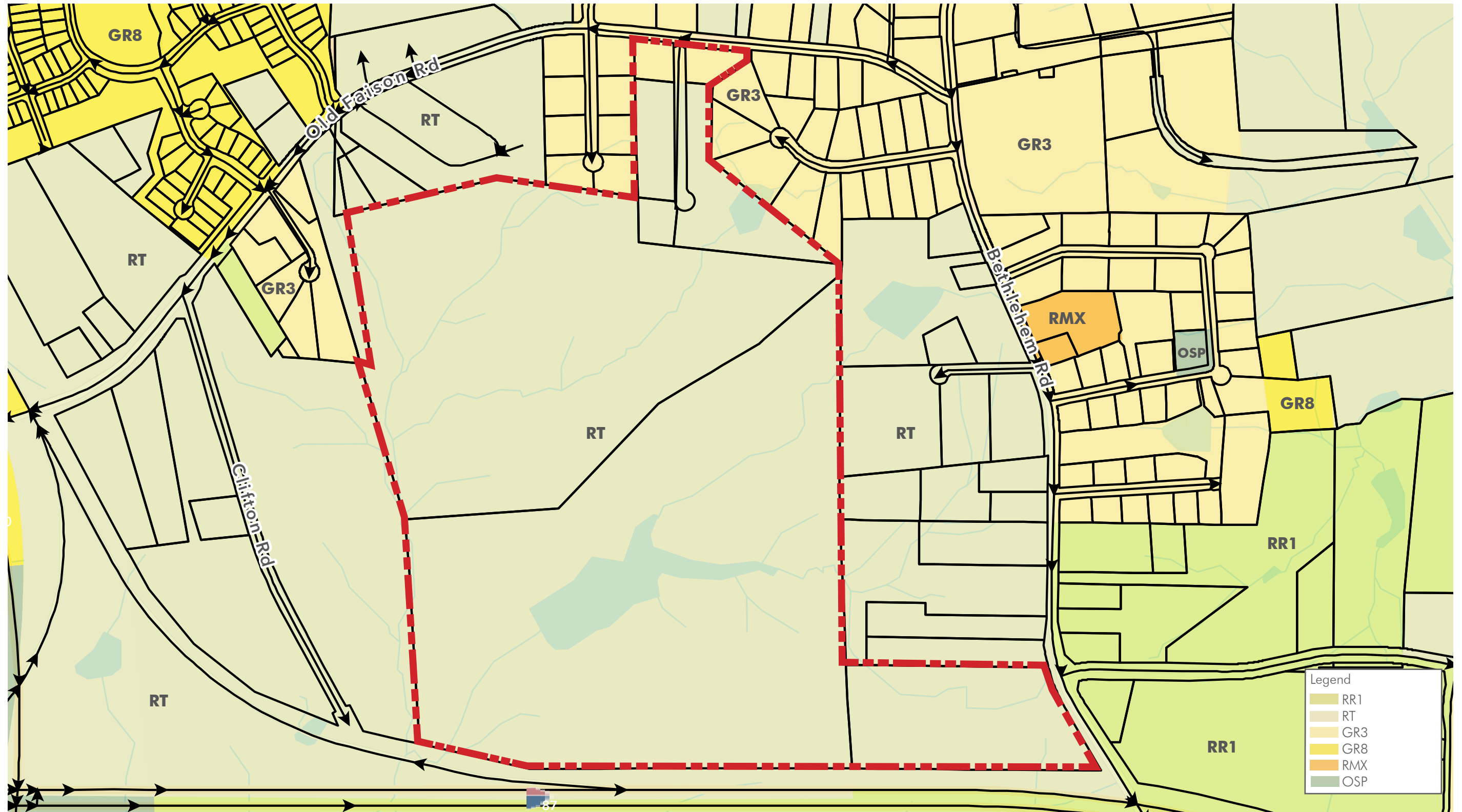


Figure 7: Current Zoning Map

EXISTING CONDITIONS

Current Uses

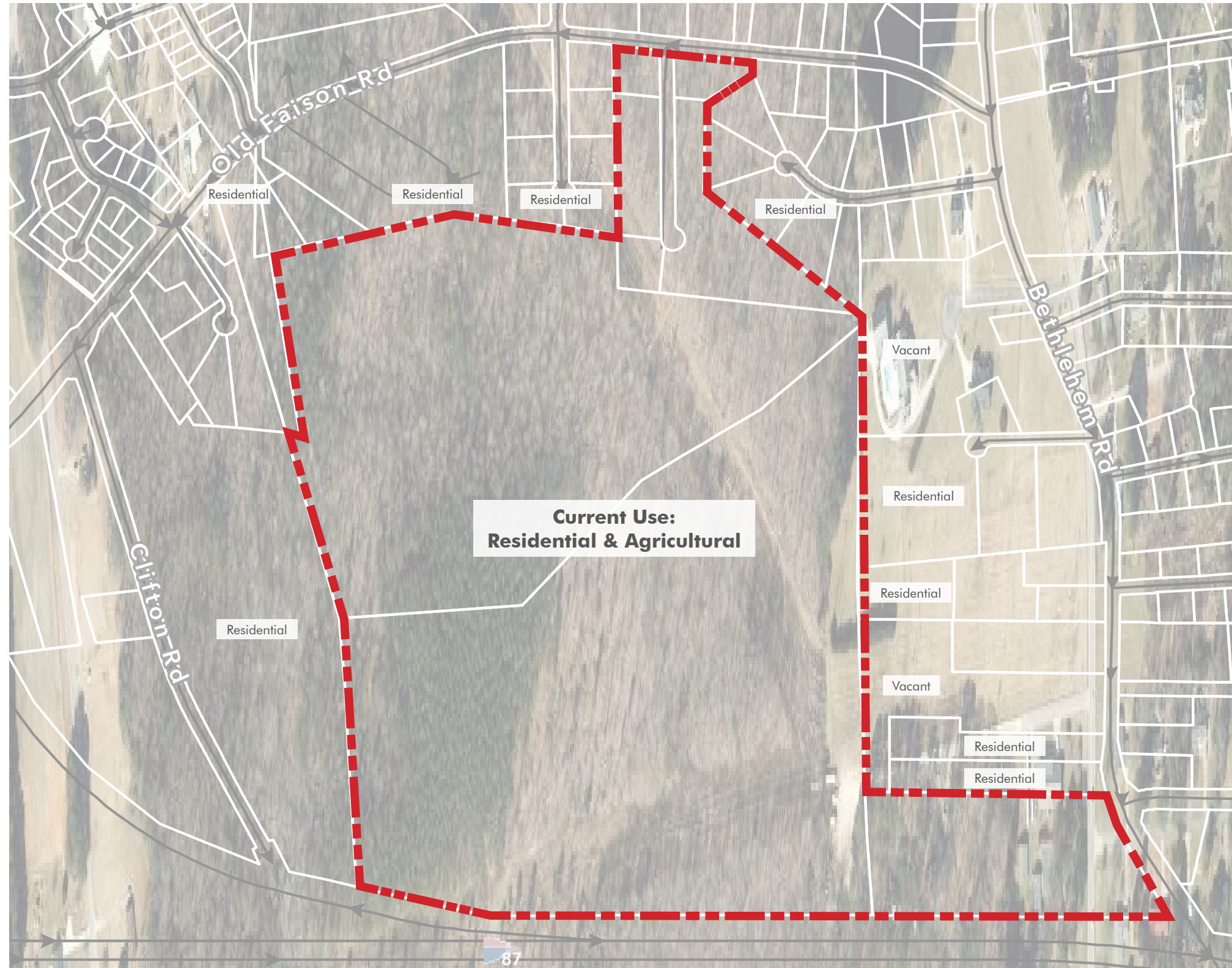


Figure 8: Current Uses Map

EXISTING CONDITIONS



Topography & Boundary Map

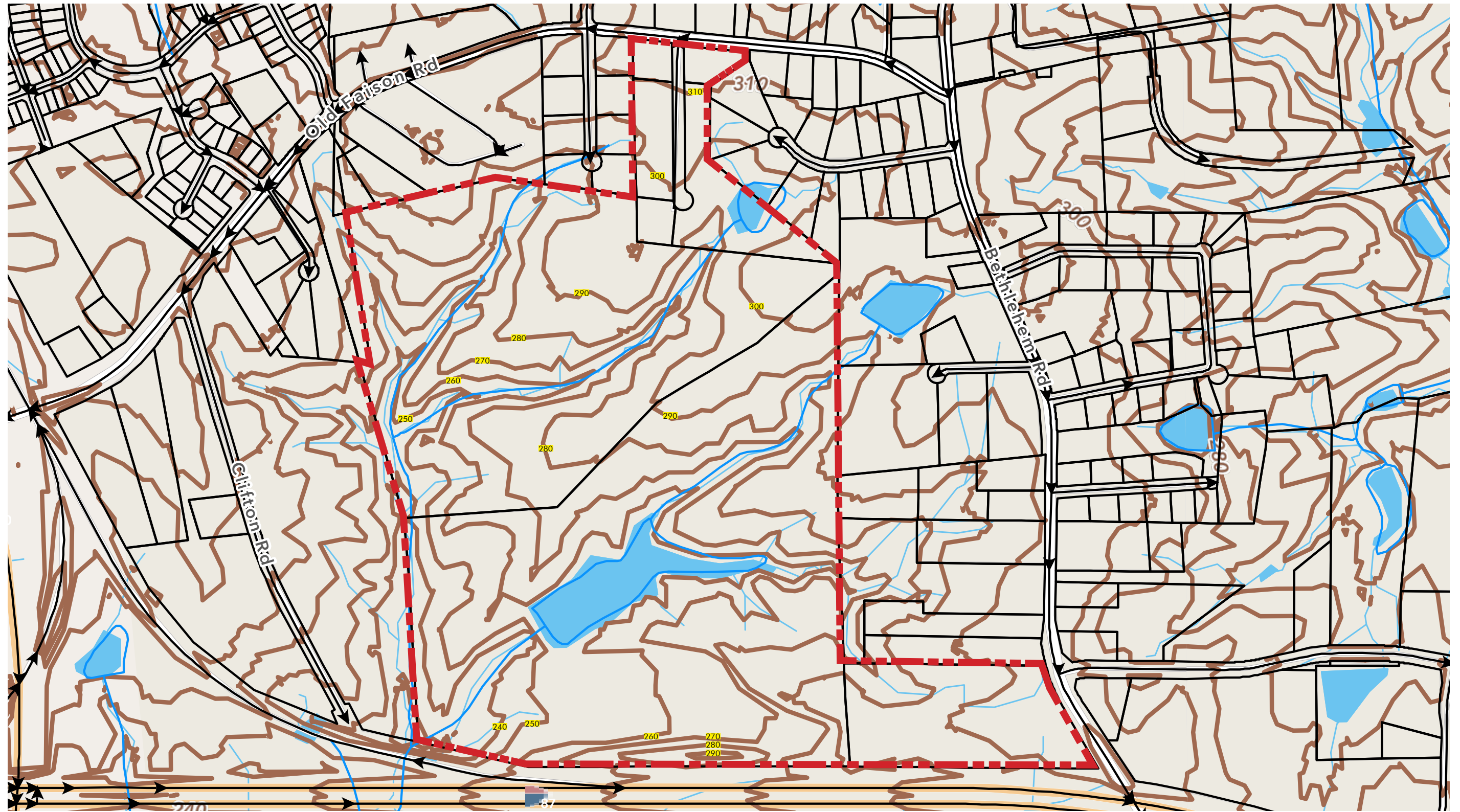


Figure 9: Topography Map



Soils Map

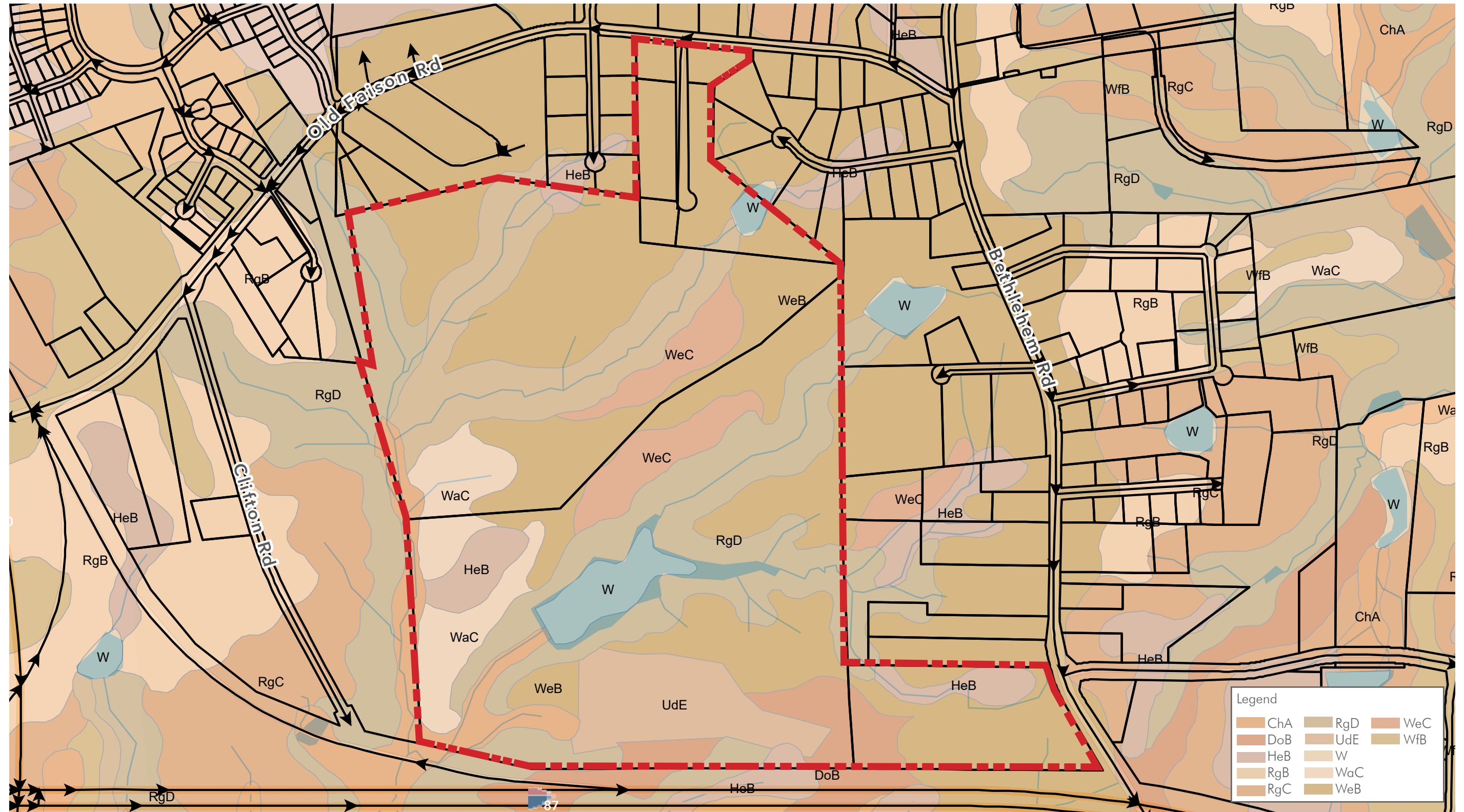


Figure 10: Soils Map



Vegetation Map



The site currently consists of mainly vacant land, two existing ponds, four perennial streams, two large electric easements, and several single family homes.

Figure 11: Vegetation Map

EXISTING CONDITIONS



Preliminary Wetland & Stream Map

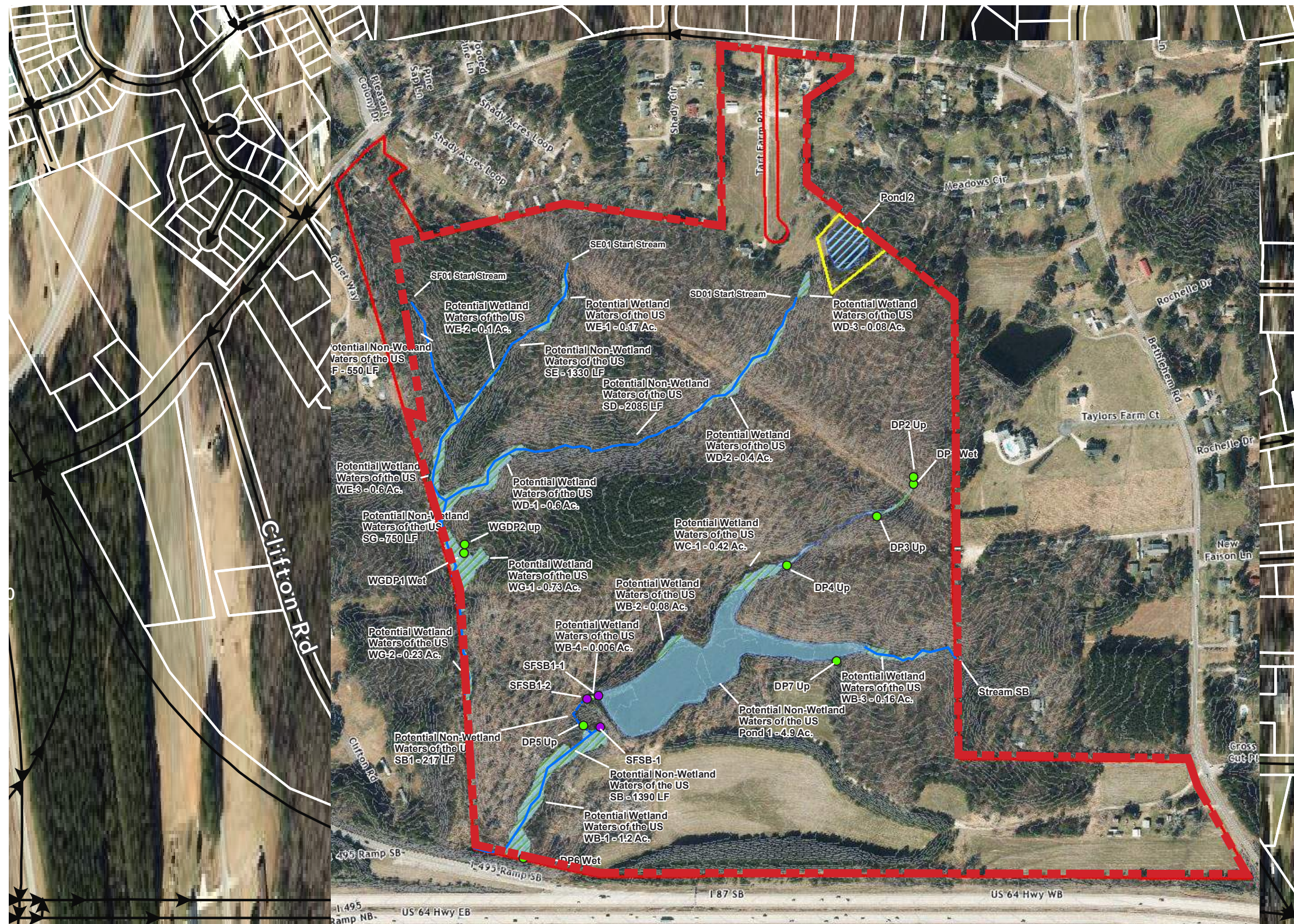


Figure 12: Wetland Sketch Map from Sage Ecological Services

Legend


- Project Study Area
- Too Thick Too Evaluate
- Z Contour
- Data Point
- 50-Foot Neuse River Riparian Stream Buffer
- Non-Buffered Intermittent Stream - Jurisdictional Waters of the US
- Perennial Stream - Jurisdictional Waters of the US; Subject to 50-Foot Neuse River Riparian Buffers
- Wetlands - Jurisdictional Waters of the US
- Non-Jurisdictional Pond - Pond Built In High Ground
- Pond - Jurisdictional Waters of the US; Subject to 50-Foot Neuse River Riparian Buffer

Detailed Delineation of Waters of the US
Suitable for Preliminary Planning Only

S&EC reserves the right to modify this map based on more fieldwork, and any other additional information. Approximations were mapped using topographic maps, air photos and ground truthing. If the site is going to be disturbed, S&EC's detailed delineation should be approved and permitted by the U.S. Army Corps of Engineers as required. If the user of this work desires an accurate map of the regulated features flagged by S&EC, they should retain a NC Registered Professional Land Surveyor to locate S&EC's flagging.

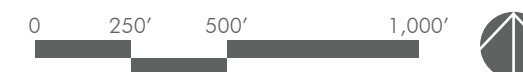
Figure 3

Drawn By:
David Gainey



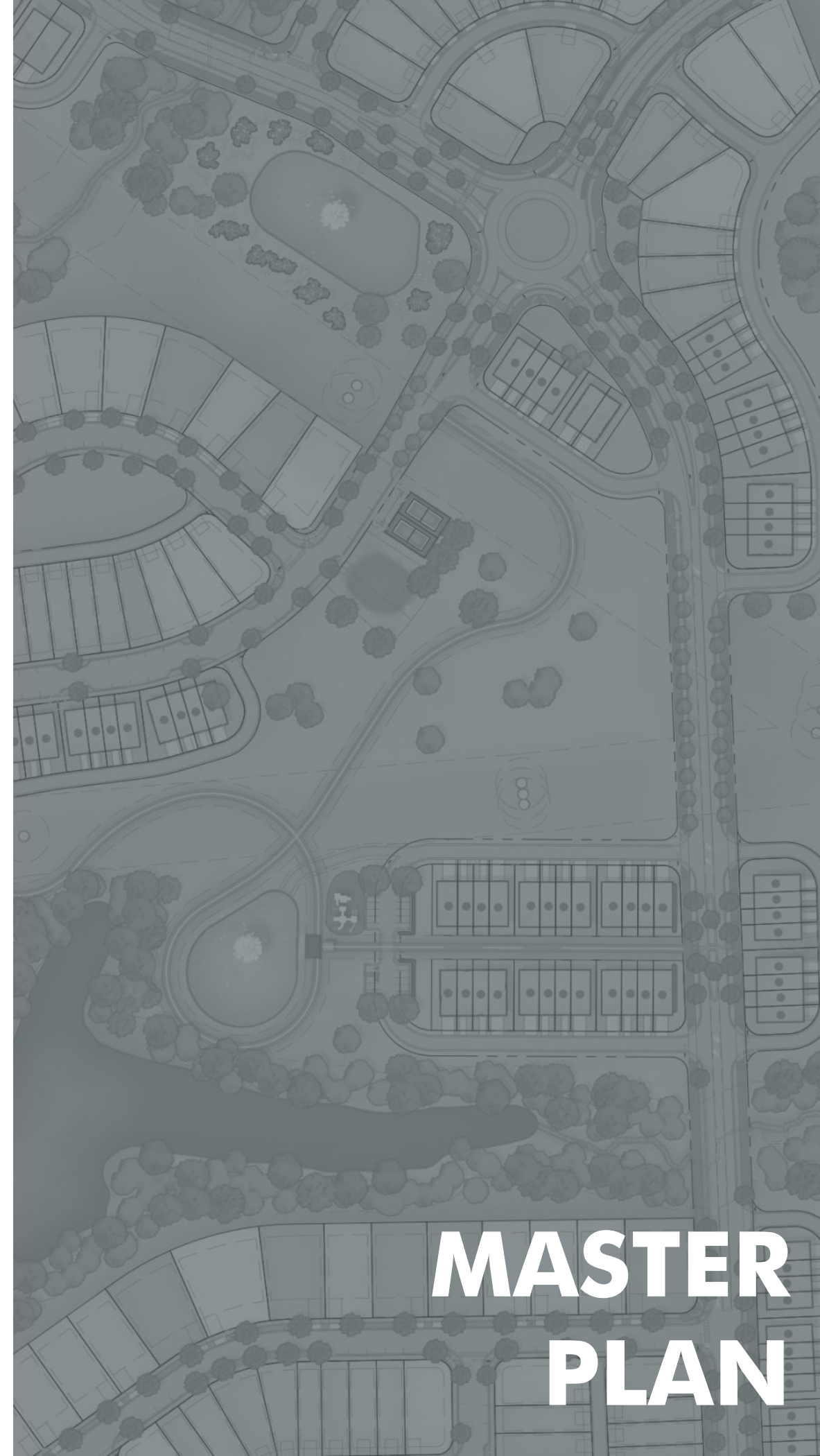
Sage Ecological Services, Inc.
Office: 919-335-6757
Cell: 919-559-1537

NOTE: Location, shape and size of depicted features on the evaluated site are approximate and should be surveyed by a licensed NC surveyor for final site planning.



04

**MASTER
PLAN**



Lyndon Oaks - Master Plan



SITE DATA	
» ACRES:	± 171.88 AC TOTAL
» PINS:	1753-15-2116; 1743-95-3683; 1743-97-1085; 1743-98-6356; 1743-98-9384; 1753-07-1583; 1743-97-6575
» EXISTING ZONING:	RT
» PROPOSED ZONING:	RMX-PUD

DEVELOPMENT SUMMARY	
COMMERCIAL:	± 15,000 SF
RESIDENTIAL:	
FRONT LOADED (SINGLE FAM. DETACHED)	
60' LOTS	66 DU
70' LOTS	24 DU
80' LOTS	10 DU
REAR LOADED (SINGLE FAM. DETACHED)	
30' LOTS	46 DU
35' LOTS	39 DU
40' LOTS	24 DU
TOWNHOMES (REAR LOADED)	
20' UNITS	117 DU
22' UNITS	130 DU
DUPLEXES (REAR LOADED)	
22' UNITS	26 DU
TOTAL:	482 DU (UP TO 500 DU)
REQ. PARKING (MINIMUM):	
SINGLE FAMILY:	627 SPACES
TOWNHOMES:	819 SPACES
COMMERCIAL:	27 SPACES
• EV	2 SPACES
• BICYCLE	3 SPACES

- NOTES:
1. THIS PLAN IS CONCEPTUAL IN NATURE AND IS SUBJECT TO CHANGE.
 2. THIS EXHIBIT WAS PREPARED USING AVAILABLE RECORD INFORMATION, ALTA SURVEY, GIS MAPS, RECORD PLANS, AERIAL IMAGERY, AND LAND RECORDS.
 3. THIS PLAN WILL BE SUBJECT TO REVIEW AND APPROVAL BY LOCAL AND STATE PLANNING AND ENGINEERING REVIEW AGENCIES.
 4. THE WORK OF THIS PRODUCT IS THE PROPERTY OF URBAN DESIGN PARTNERS, PLLC. NO USE OR REPRODUCTION OF THIS PLAN IS PERMITTED WITHOUT WRITTEN AUTHORIZATION FROM URBAN DESIGN PARTNERS, PLLC.



Figure 13: Lyndon Oaks Master Plan

Lyndon Oaks - Distribution of Uses

USE AREAS				
USE TYPE	TYP. LOT SIZE (SF)	# OF LOTS	TOTAL AREA (SF)	ACRES
SINGLE FAMILY				
30' LOT	3,600	46	165,600	
35' LOT	4,200	39	163,800	
40' LOT	4,800	24	115,200	
60' LOT	7,200	66	475,200	
70' LOT	8,400	24	201,600	
80' LOT	9,600	10	96,000	
			1,217,400	27.9
DUPLEX / TOWNHOUSE				
20' TH	1,800	117	210,600	
22' TH	1,980	130	257,400	
22' DUPLEX	2,160	26	56,160	
			524,160	12.0
COMM/RETAIL*			25,689	0.6
DISTRIBUTION OF USES				
				ACRES
GROSS SITE AREA (PER SURVEY)				171.8
DEDICATED RECREATIONAL OPEN SPACE				21.53
PUBLIC R/W				28.3
NET AREA**				121.97
USE TYPE	PROP. (AC)	MIN.-MAX. DISTRIBUTION(%)	PROV. DIST. (%)	
SINGLE FAMILY	27.9	15%-60%	22.9%	
DUPLEX / TOWNHOUSE	12.0	10%-40%	9.9%	
COMMERCIAL / RETAIL	0.6	5%-20%	0.5%	

Lyndon Oaks is proposing a development density of ± 3 dwelling units per acre. The maximum density for RMX zoning is 18 dwelling units per acre. Density is calculated by dividing the amount of dwelling units by the total site area. Knightdale requires use distribution minimum and maximum percentages (based on use areas) for subdivisions with both residential and non residential uses (Section 11.1.B of the UDO). The distribution of uses is calculated by dividing the use areas by the net site area. In this PUD, we are proposing an allowance on the distribution of uses for both duplex/townhomes, and retail. See site allowances page for more information.

*Note: Retail is not required in Lyndon Oaks because there are no multi-family units being proposed.

**Note: Net area is calculated by removing the area of open space (required amount) and rights-of-way from the total site area. The Town of Knightdale UDO Sec. 11.1.B states that the required distribution of uses shall be calculated as the net development area which excludes street rights-of-way and dedicated open space.

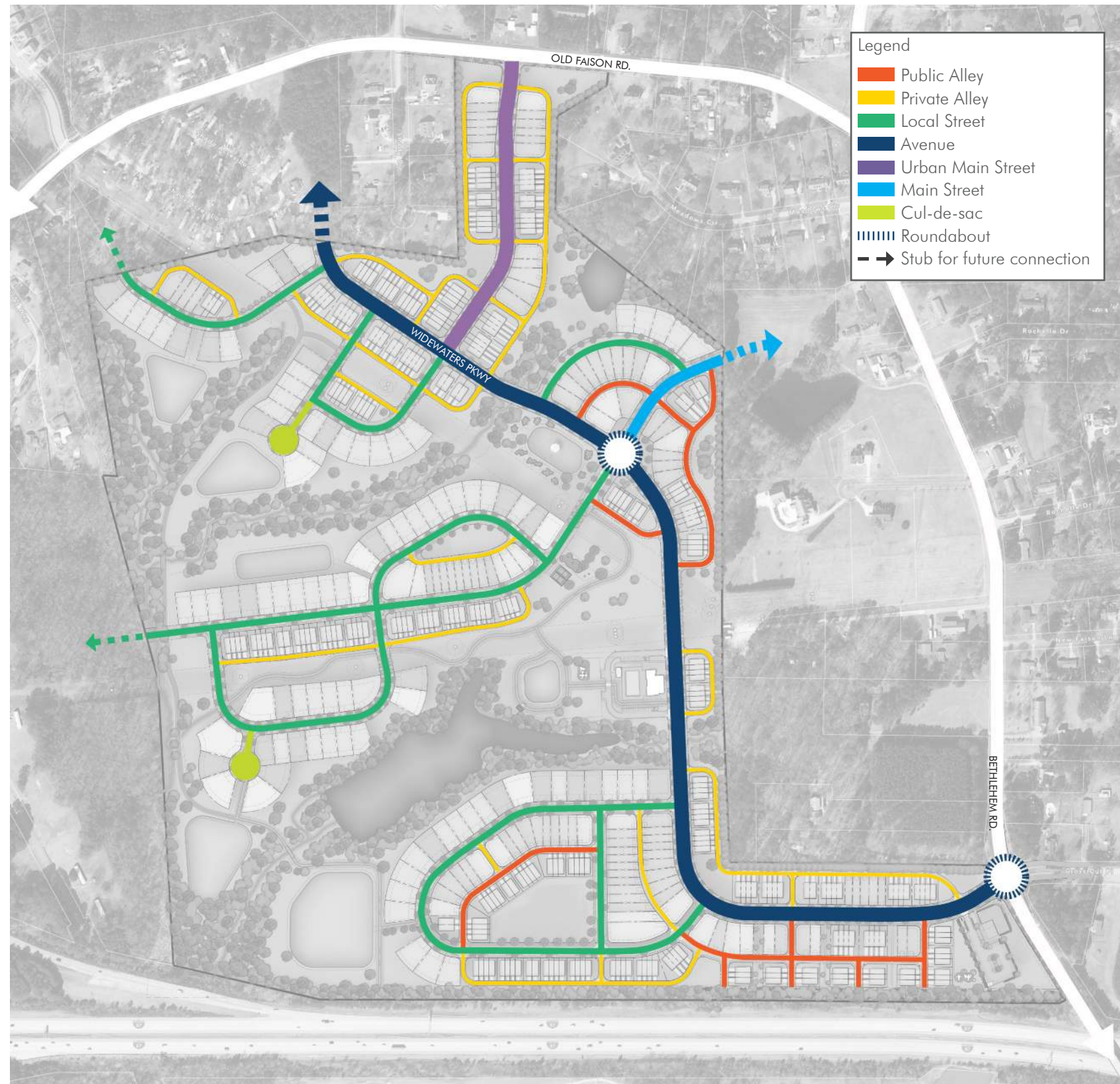
*** Note: See page 43 for more information on Commercial Outparcel

05



INFRASTRUCTURE

Street Plan



Legend

- Public Alley
- Private Alley
- Local Street
- Avenue
- Urban Main Street
- Main Street
- Cul-de-sac
- ⊙ Roundabout
- - - - -> Stub for future connection

TOWN OF KNIGHTDALE STREET TYPES

PRIVATE ALLEY	
» RIGHT OF WAY:	20'
» STREET WIDTH:	20'
» DESIGN SPEED:	10 MPH
PUBLIC ALLEY	
» RIGHT OF WAY:	32'
» STREET WIDTH:	18' (FOC-FOC)
» DESIGN SPEED:	10 MPH
LOCAL STREET	
» RIGHT OF WAY:	54'-60'
» STREET WIDTH:	28'- 41' (FOC-FOC)
» DESIGN SPEED:	20 - 25 MPH
URBAN MAIN STREET	
» RIGHT OF WAY:	72'
» STREET WIDTH:	48' (BOC-BOC)
» DESIGN SPEED:	20 - 25 MPH
CUL-DE-SAC	
» RIGHT OF WAY:	61.5'
» STREET WIDTH:	48' RADIUS
» DESIGN SPEED:	20 - 25 MPH
MAIN STREET	
» RIGHT OF WAY:	70'
» STREET WIDTH:	47'
» DESIGN SPEED:	20 - 25 MPH
WIDEWATERS PARKWAY AVENUE (3 LANE - SIDEPATHS)	
» RIGHT OF WAY:	75'-80'
» STREET WIDTH:	VARIABLE
» DESIGN SPEED:	30 - 35 MPH



Figure 14: Vehicular Circulation

Connectivity Index



TOWN OF KNIGHTDALE CONNECTIVITY INDEX*

REQUIRED SCORE: 1.40 MIN.
 PROVIDED SCORE: 1.47

— LINKS: 28 LINKS

● NODES: 19 NODES

$$\frac{28 \text{ LINKS}}{19 \text{ NODES}} = 1.47 \text{ CONNECTIVITY INDEX SCORE}$$

*Connectivity index is calculated according to Town of Knightdale UDO Sec. 11.3.E.7
 ** Block lengths shown are the lengths of the links. Block lengths are all under 800 LF when alleys are included. See Master Plan ZMA-2-23.



Figure 15: Connectivity Index for Lyndon Oaks

Pedestrian Circulation Plan



Figure 16: Pedestrian Network for Lyndon Oaks

PEDESTRIAN CIRCULATION

Lyndon Oaks is proposing a connected network of amenity spaces and dwelling units through the use of sidewalks, sidepaths, and greenway trails to create a walkable, pedestrian-friendly community as seen in Figure 16.

» The plan shown is preliminary and subject to change at the time of site plan.

SIDEWALK NETWORK

» WIDTH: 5'-6' wide

SIDE PATH

» WIDTH: 10' wide

GREENWAY TRAIL

» WIDTH: 10' pavement width (30' easement)

PLAZA

» PROVIDED SIZE: ± 2,000 SF

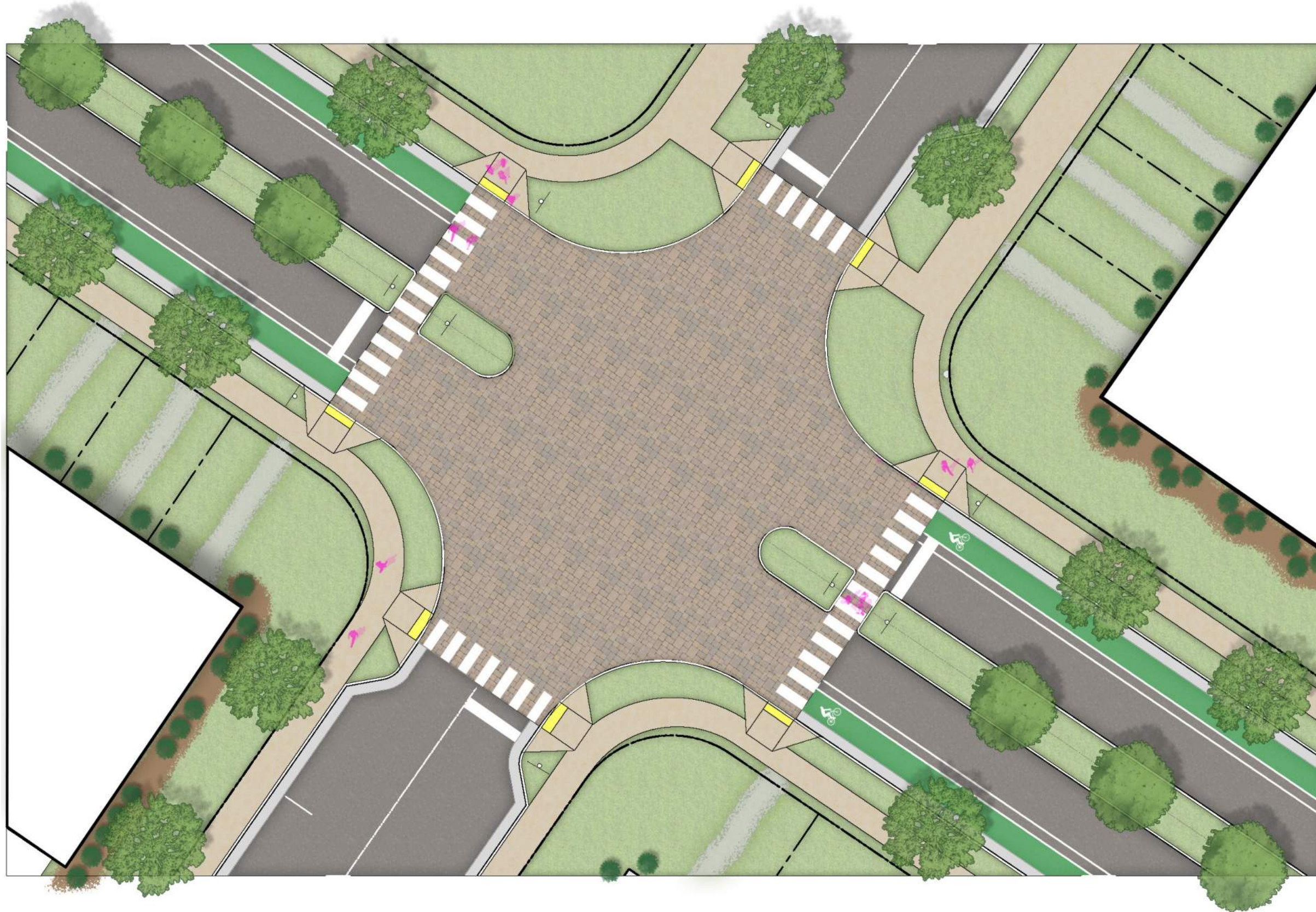
AMENITY SPACES

» See Open Space Plan

Easement for future pedestrian bridge over I-87: Prior to approval of construction drawings for the first phase of development, the applicant will offer a minimum of 100 square feet of site area as a non-exclusive, permanent easement area to be dedicated to the Town for construction, maintenance, and repair of footings for a future pedestrian bridge over I-87, together with such temporary construction easements as are necessary for completion of such work. If accepted by the Town Transportation Director, such easement area shall be shown on the construction drawings and subdivision plat and granted to the Town pursuant to a recorded Deed of Easement.



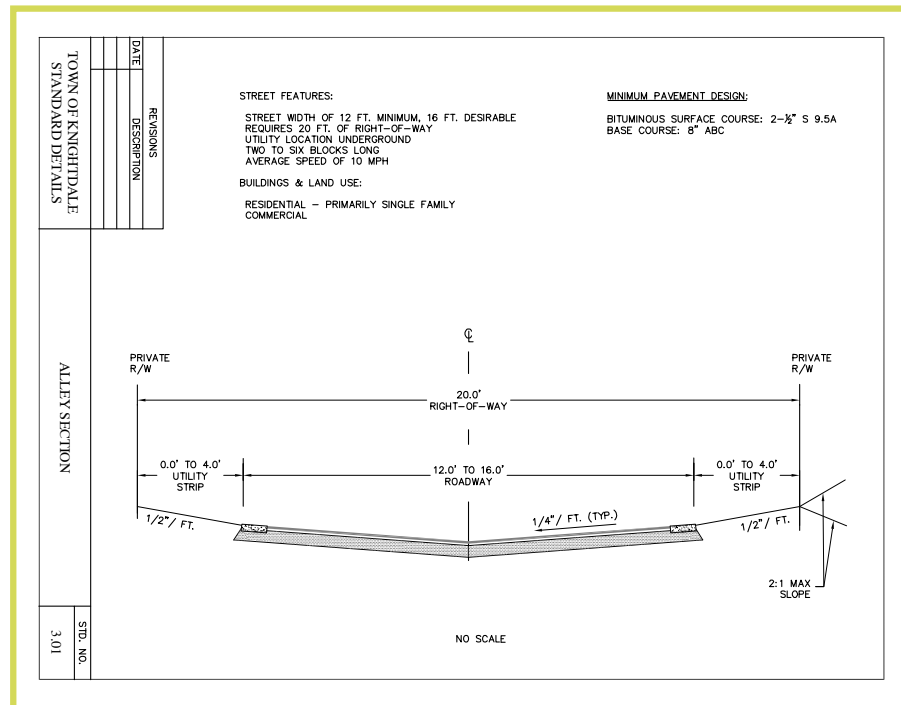
Urban Main St (Tart Farm Rd) & Widewaters Parkway Intersection Design



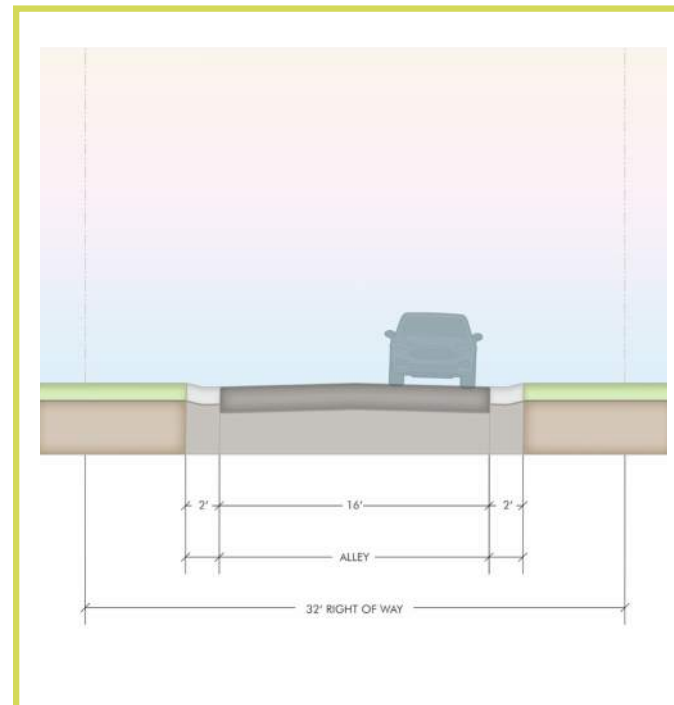
PEDESTRIAN CIRCULATION

Lyndon Oaks proposes a four-way stop with traffic calming measures, including high-visibility crosswalks, stamped concrete, Rectangular Rapid Flash Beacons (RRFBs), and pedestrian refuges in the Widewaters Parkway median at the intersection of the Urban Main Street and Widewaters Parkway.

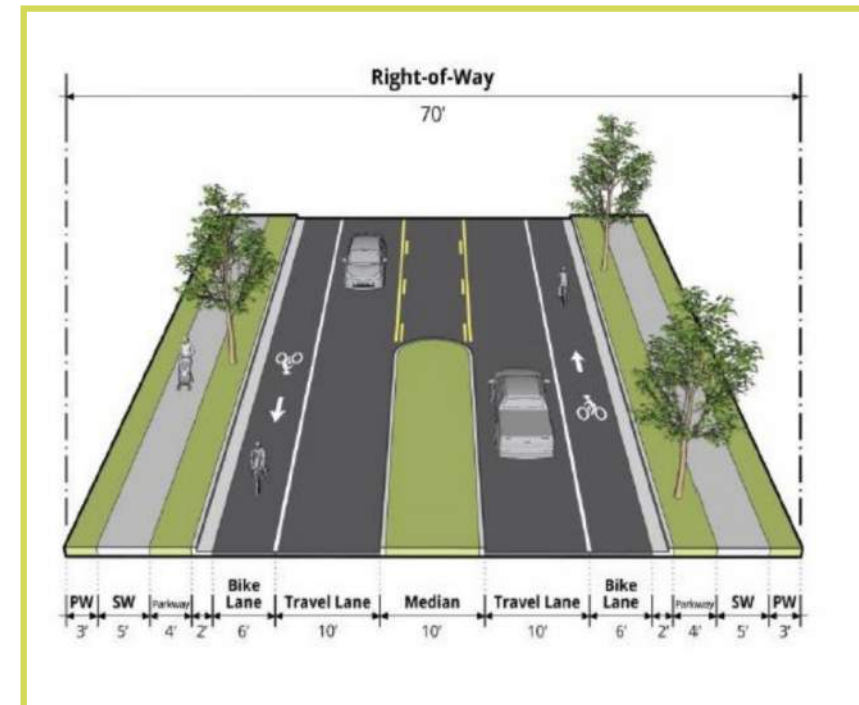
Street Sections



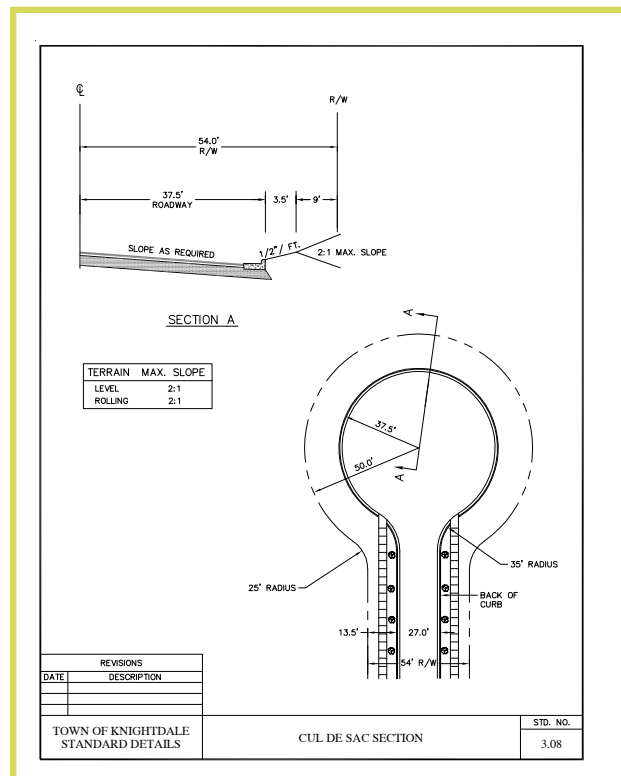
Private Alley



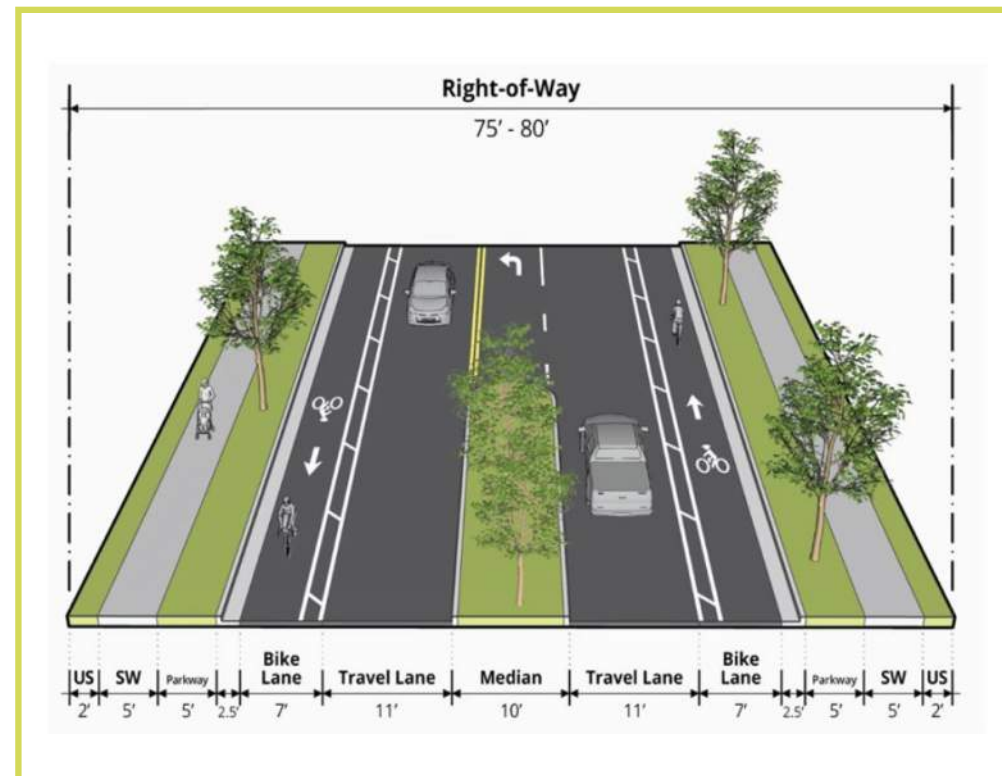
Public Alley



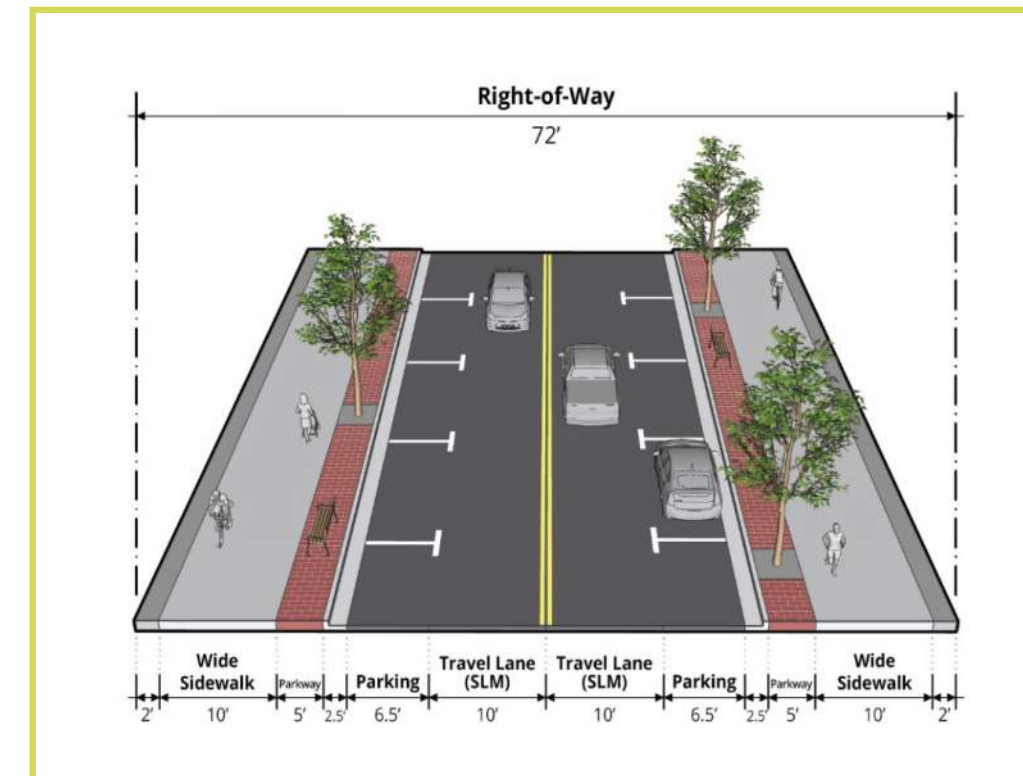
Main Street



Cul-de-sac



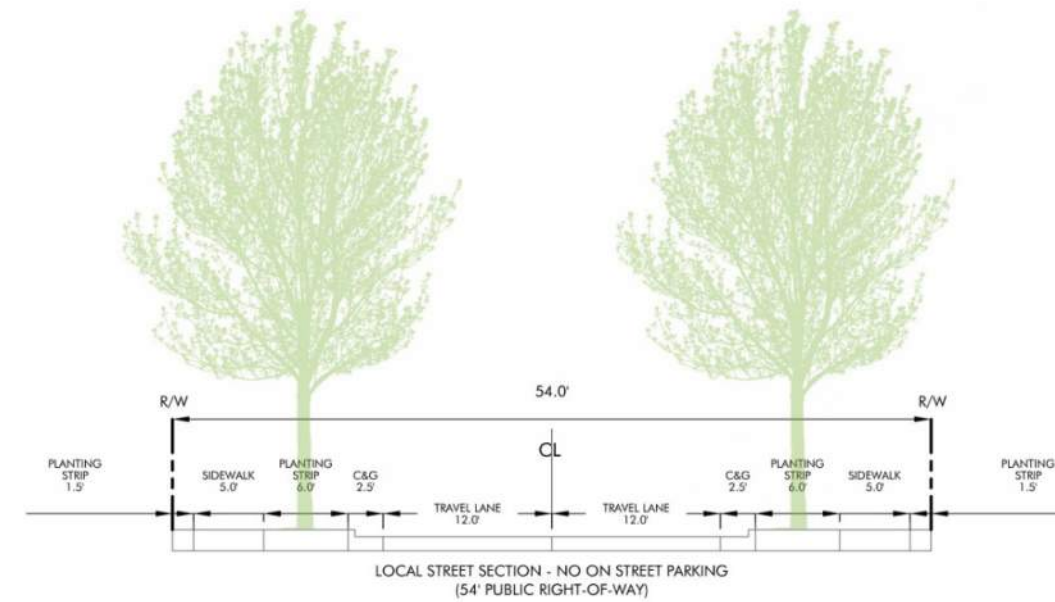
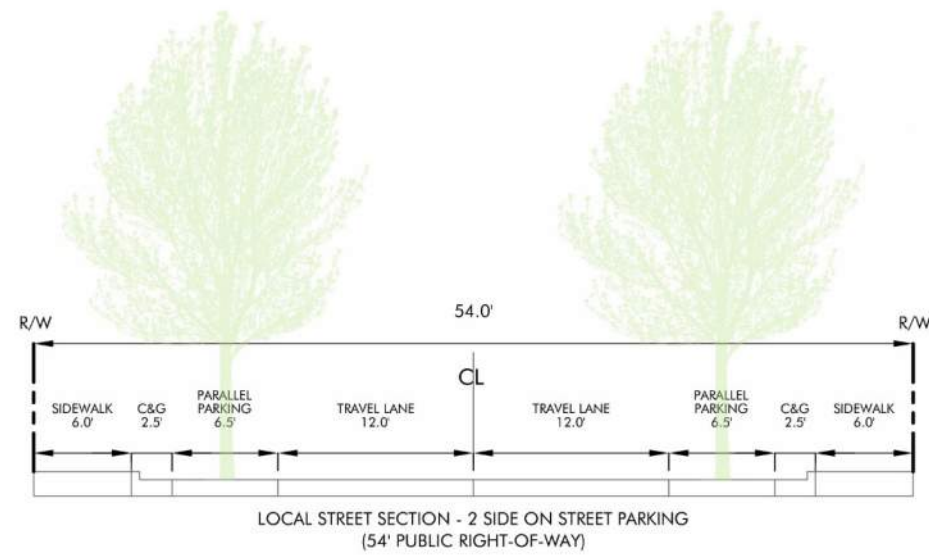
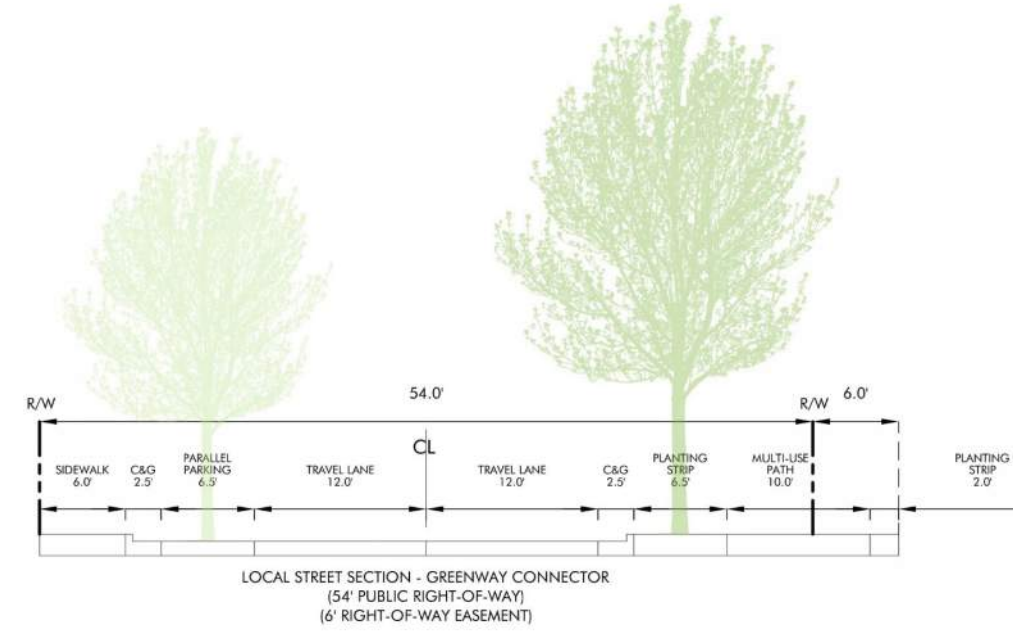
Avenue 3 Lane Divided



Urban Main Street

*Note: Staff is developing updated sections and details. Sections to be updated once updates received from Town of Knightdale

Additional Sections & Details



Preliminary Stormwater Plan



Figure 17a: Stormwater Plan for Lyndon Oaks

STORMWATER

The property is located within the Neuse River Basin. The site consists of two existing ponds, and 4 perennial streams which create four unique drainage areas. Stormwater runoff within these areas will be controlled by multiple SCMs as seen in Figure 17a. The outfall is located at the southwest corner of the property. All stormwater shall be subject to the stormwater management requirements set forth in Chapter 9 of The Town of Knightdale Unified Development Ordinance. The stormwater treatment shall adhere to guidelines established in the NCDEQ Stormwater Design Manual.

The SCM in the center of the site and near the central roundabout will have a fountain installed to create a water amenity. See Figure 17b for an example of a pond fountain.

The developer will work with Town staff during the construction drawing phase on SCM design if any "additional safety structures such as fences, vegetative buffers, landscaping elements, or any combination of the three" are needed, pursuant to UDO Sec. 7.3.J.

- » The plan shown is preliminary and subject to change per direction from Knightdale staff.

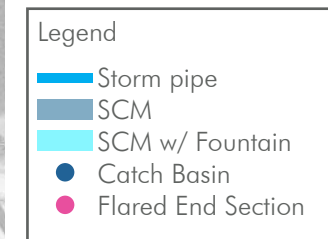


Figure 17b: SCM fountain example



Preliminary Utility Plan



Figure 18: Utility Plan for Lyndon Oaks

WATER & SEWER

Lyndon Oaks will design all water and sewer to meet the standards of the City of Raleigh. Water allocation points will be met for the Major Subdivision through the use of many site elements that listed in the tables below. Precedent images of these elements can be found in the Site Programming section of this PUD.

» The plan shown is preliminary and subject to change per direction from City of Raleigh staff.

WATER ALLOCATION POLICY

Required amount: 50 pts.

MAJOR SUBDIVISION	
Major Subdivision (Base points)	15 pts.
Conservation of Natural Habitat	7 pts.
Construct a fountain within the SCM	4 pts.
On Street Parking	4 pts.
Residential Architectural Standards	15 pts.
3,000' or more of 6-foot wide path	3 pts.
Resort Style Pool	2 pts.
Deck/Patio (2,000 SF minimum)	2 pts.
Clubhouse (No Meeting Space, bathrooms and changing rooms only)	3 pts.
Outdoor display of public art	4 pts.
IPEMA Certified Playground Equipment	4 pts.
TOTAL PROVIDED	63 pts.
TOTAL MIN. REQUIRED	50 pts.
SINGLE-USE RETAIL	
Single Use Retail Base Points	41 pts.
Construction of Gateway Landscaping	5 pts.
Deck/Patio (2,000 SF minimum)	2 pts.
Outdoor Display of Public Art	4 pts.
TOTAL PROVIDED	52 pts.
TOTAL MIN. REQUIRED	50 pts.

Note: Any combination of features listed above can be used to meet the 50 point minimum requirement. Single-Use Retail Water Allocation Policy points are preliminary and subject to change when the developer of the commercial outparcel submits a site plan.



Transportation Impact Analysis Summary

A Traffic Impact Analysis (TIA) was conducted by Ramey Kemp Associates in accordance with the Knightdale Unified Development Ordinance (UDO) and the North Carolina Department of Transportation (NCDOT) capacity analysis guidelines. A full copy of the TIA was submitted for review and approval with the PUD submittal. The listed recommended improvements are subject to further review and final approval by NCDOT, and shall be phased with the development.

Study Area

The study area for the TIA was determined through coordination with the Town and NCDOT and consists of the following existing intersections:

- » Old Faison Road & Bethlehem Road
- » Old Faison Road & Tart Farm Road
- » Old Faison Road & Pleasant Colony Drive
- » Old Faison Road & Quiet Way
- » Bethlehem Road & Crosscut Place
- » US 64 EB Ramps & Hodge Road
- » US 64 WB Ramps & Old Faison Road
- » Smithfield Road & Old Ferrell Road
- » Bethlehem Road & Poole Road

Recommended Improvements

Based on the analysis of the TIA (including improvements to be installed by the Lyndon Oaks development), the following improvements have been recommended to be constructed to mitigate traffic impacts by the proposed development.

Westbound I-87 Ramps & Hodge Road (Improvements by STIP W-5705AK)

- Construct a westbound right turn lane with 100 feet of storage and appropriate decel and taper.
- Construct a westbound left turn lane with 100 feet of storage and appropriate decel and taper.
- Modify signal timings and signal phasing.

Old Faison Road & Bethlehem Road (Improvements by Developer)

- Construct a signal
- Construct a northbound left turn lane with 250 feet of storage
- Construct a westbound right turn lane with 250 feet of storage
- Construct a westbound left turn lane with 175 feet of storage
- Roundabout improvement may be provided in lieu of the signal and turn lanes recommended by the TIA if later requested by the Town, sufficient right-of-way is available, and this improvement is approved by NCDOT.

Old Faison Road & Tart Farm Road / Site Access A (Improvements by Developer)

- Construct an eastbound right turn lane with 75 feet of storage and appropriate decel and taper.
- Construct exclusive northbound left turn lane with full storage.
- Provide stop control for northbound approach.

Bethlehem Road & Crosscut Place / Site Access B (Improvements by Developer)

- Construct a single-lane roundabout.
- Construct eastbound approach with one ingress and one egress lane.
- Provide yield control for eastbound approach.

Westbound I-87 Ramps & Hodge Road (Improvements by Developer)

- Extend westbound left turn lane to contain 175 feet of storage and appropriate decel and taper.
- Extend southbound left turn lane to contain 275 feet of storage and appropriate decel and taper.
- Extend eastbound left-through lane to contain 275 feet of storage and appropriate decel and taper.

Bethlehem Road & Poole Road (Improvements by Developer)

- Construct left turn lanes with 100 feet of storage.

06



AMENITIES

Site Programming



Lyndon Oaks will provide a variety of amenities to promote a healthy lifestyle, opportunities to enjoy the outdoors, and foster a sense of community. The site is proposing a network of ±20 acres of open space and recreational amenities. The proposed amenities may include but are not limited to the following:

- 1 **Greenway Trail**
 - » Public and private greenway trails will be located throughout Lyndon Oaks and will be 10' wide asphalt paths. In addition to the 10' multi-use paths along Widewaters Parkway, there are many pedestrian routes through the community.
- 2 **Village Green**
 - » The Village Green will be a large open green space that can be used for a variety of activities by the community.
- 3 **Playground**
 - » The IPEMA certified playground will be located in two open spaces to allow for greater access for families in Lyndon Oaks
- 4 **Dog Park**
 - » The playground will be located in one of the active open space areas and will provide a place for residents to let their dogs run.
- 5 **SCM Fountains**
 - » The pond fountains will amenitize the stormwater ponds located in both the center of the site, and near the central roundabout active open space.
- 6 **Neighborhood Serving Commercial**
 - » The commercial outparcel will be low-intensity, and will meet the needs of nearby residents.
- 7 **Pickleball Court**
 - » Two Pickleball courts will allow for additional active recreational opportunities for the neighborhood

Site Programming



1



2



3

1 Pocket Park

» Pocket parks will be located in several places throughout Lyndon Oaks and may include benches, open play lawn, landscaping, exercise equipment, and other recreational opportunities.

2 Bench Swing Park

» A bench Swing Park will be located in one of the open spaces for the community and will provide a fun seating alternative for people of all ages.

3 Hammock Park

» A Hammock Park will be located in Lyndon Oaks to add an additional type of open space that will allow residents of all ages to relax and unwind.



4



5



6

4 Pollinator Garden

» A pollinator garden will be provided around the SCM nearest to the central roundabout to create a habitat for pollinators in the community.

5 Overlook with Gazebo

» The central pond will have an overlook plaza and gazebo to allow for a community gathering focal point.

6 Benches

» Benches will be provided in open spaces around Lyndon Oaks to provide spaces for the community to relax & rest.



7



6



7

7 Public Display of Art

» An art installation and roadside landscaping may be provided near the central roundabout for enjoyment from both vehicular and pedestrian users of the site.

Clubhouse Programming



3



2



3

- 1 Clubhouse**
Lyndon Oaks will have a Clubhouse for the residents that will include community amenities and a resort-style pool.
- 2 Pool**
The Clubhouse will include an outdoor resort-style pool for the neighborhood residents.
- 3 Clubhouse Amenities**
The Clubhouse may include a kitchen, outdoor area, and community room.



1



2

3



1



1

3

AMENITIES

Open Space Plan



Figure 19: Open Space Plan for Lyndon Oaks

Lyndon Oaks provides both active and passive recreation areas throughout the development. All recreation spaces are located in places that are accessible to all residents of the community. The network of public Greenway Trails and private trails create additional connection opportunities between all of the provided open spaces.

Active open space areas may include but are not limited to the following uses: A Clubhouse, Resort-style Pool, (2) Playgrounds, (2) Pickleball courts, a Bench Swing Park, a Hammock Park, Greenway Trails, and additional pocket parks with exercise equipment.

Passive open space areas may include but are not limited to the following uses: Village Green, Pollinator Gardens, Public Display of Art, Paths & Benches, Private Greenway Trails, Open Lawns and entry landscaping and hardscaping.

Enhanced landscaping, hardscaping, and unique signage will be provided at both neighborhood entrances.

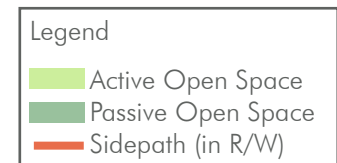
Please see Site programming page for example imagery.

RECREATIONAL OPEN SPACE STANDARDS*

PROXIMITY ZONE:	All outside 1/2 mile distance
REQ. OPEN SPACE:	± 751,920 sf (17.26 ac)
» REQ. ACTIVE SPACE (50%):	± 8.63 ac (50% of req.)
» REQ. PASSIVE SPACE (50%):	± 8.63 ac (50% of req.)
PROP. OPEN SPACE:	± 21.53 ac total
» PROP. ACTIVE SPACE:	± 8.77 ac (0.14 ac over req.)
» PROP. PASSIVE SPACE:	± 12.76 ac (4.13 ac over req.)

* Open space is calculated according to Town of Knightdale UDO Sec. 11.2.c.

**According to Town of Knightdale UDO Sec. 11.2.A, Recreational Open Space can not include areas covered by buildings, structures, parking lots, dry ponds, required setbacks, or public R/W.



07



LANDSCAPE

Preliminary Landscape Plan



Figure 20: Landscape Plan for Lyndon Oaks

Lyndon Oaks will design all landscape areas according to the Knightdale Unified Development Ordinance. The site is surrounded by a 20' Type B Buffer yard as required in Section 7.4.I.1. of the UDO. The preliminary plan only shows typical buffer plantings and street tree plantings as seen in Figure 20. The riparian buffer will consist of tree preservation to maintain the natural qualities of the site.

- » The plan shown is preliminary and subject to change per direction from Town of Knightdale staff.
- » More detailed landscape plans with planting details and species list will be provided at the time of site plan and are subject to review from Town of Knightdale Staff.
- » Location and amounts of trees are conceptual in nature and subject to change.

Landscape Buffers

The perimeter Lyndon Oaks contains a Type B Buffer Yard per Town of Knightdale Unified Development Ordinance Chapter 7. Figure 21 shows a typical 100' section of the Type B buffer for Lyndon Oaks.

TYPE B BUFFER YARD STANDARDS (UDO Sec. 7.4.1.3)

Minimum width: 20'

Minimum landscape height/opacity:

- » Ground to 6' Semi-opaque screen
- » 6' - 30' Intermittent visual obstruction

Maximum landscape horizontal openings:

- » 5' Semi-opaque screen
- » 20' Intermittent visual obstruction

Required plantings:

- » Canopy Trees 3 per 100 linear feet
- » Understory Trees 5 per 100 linear feet
- » Shrubs 20 per 100 linear feet
- » Evergreen vs. deciduous
 - Canopy trees: A minimum of 40% / maximum 60% must be evergreen
 - Understory trees: A minimum of 40% / maximum 60% must be evergreen
 - Shrubs: At least 80% must be evergreen

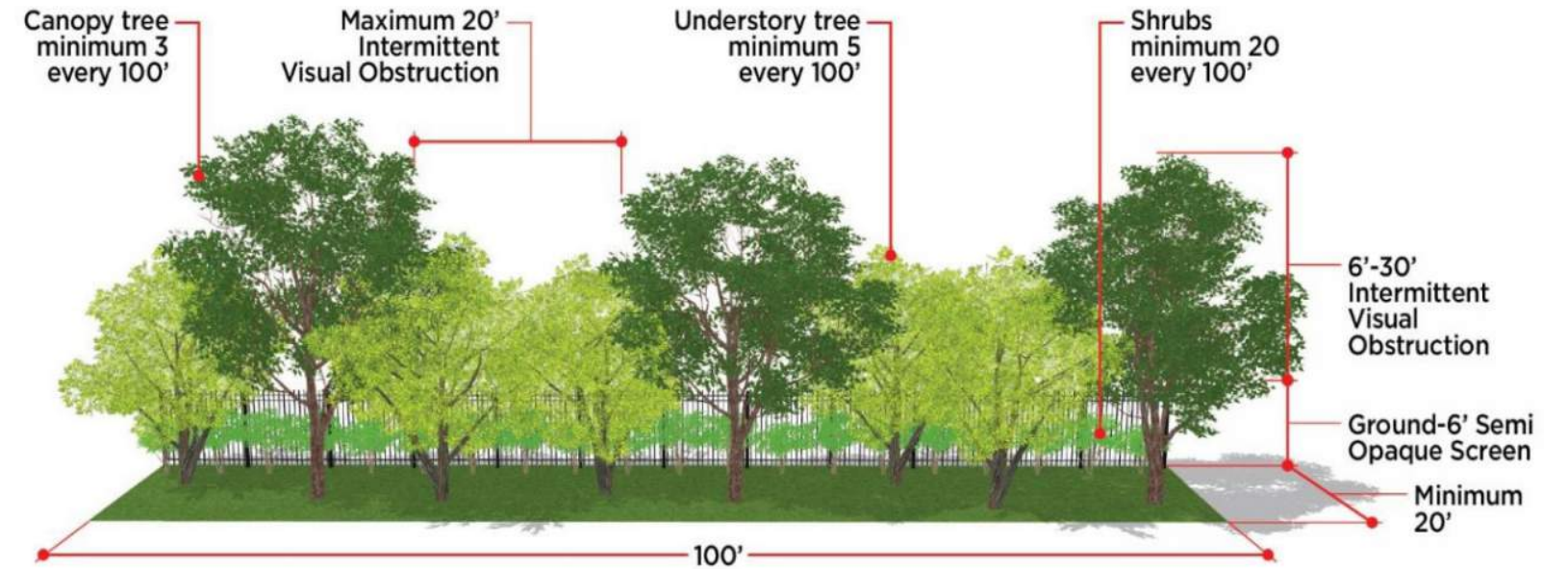


Figure 21: Type B Buffer

The southern property line of Lyndon Oaks contains a Type D Buffer Yard per Town of Knightdale Unified Development Ordinance Chapter 7. Figure 22 shows a typical 100' section of the Type D buffer for Lyndon Oaks.

TYPE D BUFFER YARD STANDARDS (UDO Sec. 7.4.1.5)

Minimum width: 50'

Minimum landscape height/opacity:

- » Ground to 30' Opaque Screen
- » Ground to 6' Earthen Berm

Maximum landscape horizontal openings:

- » None permitted

Required plantings:

- » Canopy Trees 5 per 100 linear feet
- » Understory Trees 8 per 100 linear feet
- » Shrubs 35 per 100 linear feet
- » Evergreen vs. deciduous
 - Canopy trees: A minimum of 40% / maximum 60% must be evergreen
 - Understory trees: A minimum of 40% / maximum 60% must be evergreen
 - Shrubs: At least 80% must be evergreen

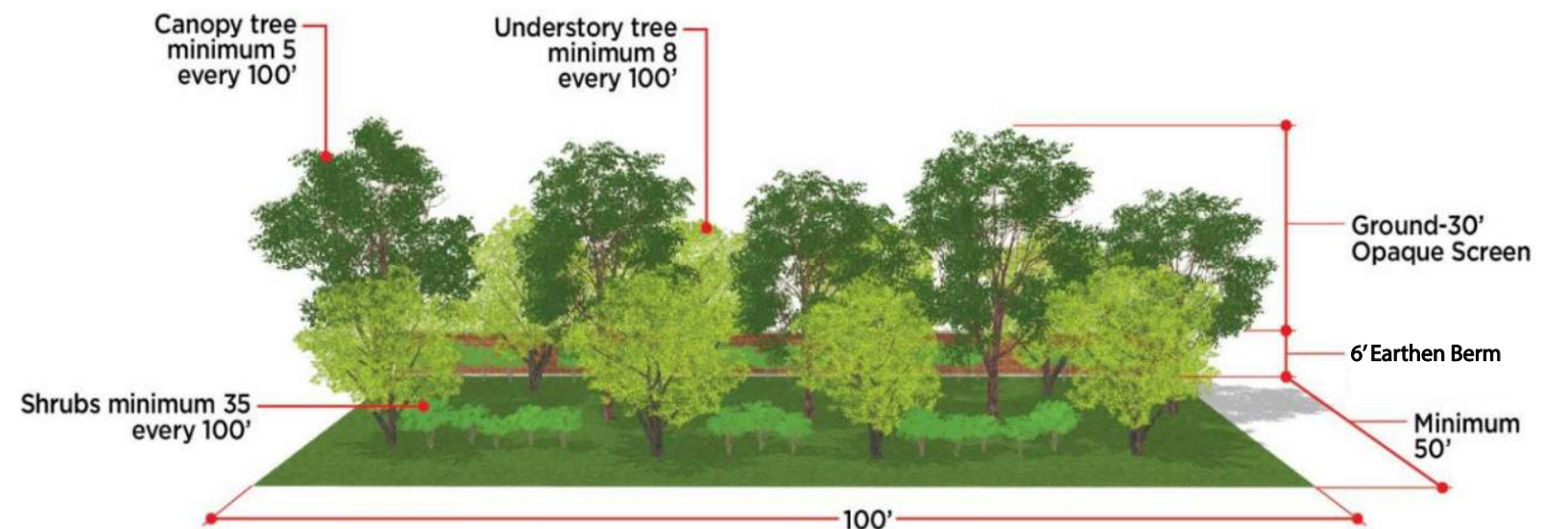


Figure 22: Type D Buffer

08



**ARCHITECTURAL
DESIGN STANDARDS**

Architectural Elevations - Single Family Detached



Note: Elevations shown are conceptual in nature and subject to change. See page 45 for list of architectural standards.



LYNDON OAKS - PLANNED UNIT DEVELOPMENT (PUD)

Architectural Elevations - Single Family Attached (Townhomes)



Elevations shown are conceptual in nature and subject to change. Individual townhome units will be combined into duplexes, triplexes, and quadplexes. (See images in upper middle and upper right.) See page 45 for list of architectural standards.

Architectural Design Standards

STANDARDS FOR ALL HOMES

1. All homes will have a combination of two or more of the following materials on the front facade (not including foundation) unless the home is only stone or brick:
 - » *stone*
 - » *brick*
 - » *lap siding*
 - » *Shakes*
 - » *board and batten*
 - » *window pediments*
 - » *recessed windows*
 - » *side and/or front window box bays*
 - » *roof gables*
 - » *roof dormers*
 - » *roofline cornices*
 - » *metal roofing as accent*
 - » *column*
 - » *shutters*
 - » *fiber cement siding*
2. The exterior siding material on the side and rear facades will be fiber cement. When two materials are used, the materials shall be different but complimentary colors.
3. Vinyl is prohibited except for window trim, soffits, fascia, and/or corner boards.
4. Main roof pitches (excluding porches) will be at least 6:12
5. For every 30 feet (or fraction) of continuous side elevation (calculated on a per floor basis), there shall be one window or door added to the side elevations. Any siding break on the side of the home such as a fireplace, side porch, wall offsets could be used as an alternative to windows.
6. Anti-monotony provisions: in order to promote variation in home appearances,

no home located adjacent, across the street, or diagonal shall have the same elevation and color combination.

7. All front doors shall have glass inserts, transoms, or glazing.
8. There shall be a minimum 12" overhang on every gable end.
9. Each front entrance shall contain a covered stoop or porch.
10. Foundation shall be raised to a minimum height of 18 inches above finished yard grade in the front and shall contain a minimum of 2 stair risers up to the front porch. Foundations will be wrapped in brick or stone on all sides. Areas under front porches may be enclosed with lattice or other decorative form of screening to match architectural style.

STANDARDS FOR SINGLE-FAMILY DETACHED FRONT-LOADED HOMES

1. Single-family 2 story homes will have a minimum heated area of 1,900 square feet.
2. Single-family 1 or 1.5 story homes will have a minimum heated area of 1,600 square feet.
3. All homes will have a front porch with a minimum depth of 6 feet, which may encroach up to 6 feet in the front setback. Front porch posts will be at least 6"x6".
4. Garages will not protrude more than 6 feet from the front porch stoop, unless an alternative is approved by staff.
5. All garage doors shall contain window inserts and decorative hardware.
6. Garages will not exceed 50% of the front facade width or will be split into two bays.
7. All homes shall have a minimum 100 square foot patio, deck, terrace, or courtyard

Architectural Design Standards (cont.)

STANDARDS FOR SINGLE-FAMILY DETACHED REAR-LOADED HOMES

1. Homes will have a minimum heated area of 1,500 square feet, except up to 50% of rear-loaded single-family detached homes may have a minimum heated area of 800 square feet.
2. All homes will have a front porch with a minimum depth of 5 feet, which may encroach up to 6 feet in the front setback. Front porch posts will be at least 6"x6".

STANDARDS FOR TOWNHOMES AND DUPLEXES

1. Townhomes shall be at least 2 stories with rear loaded garages and a with a minimum heated area of 1,100 square feet.
2. All townhouse units shall provide detailed design elements using at least one (1) of the features from each of the four categories below:
 - a. Entrance
 - i. *Recessed entry with 6" minimum width door trim*
 - ii. *Covered porch with 6" minimum width pillars/posts/columns*
 - b. Building Offset
 - i. *Facade offset*
 - ii. *Roofline offset*

c. Roof

- i. *Dormer*
- ii. *Gable*
- iii. *Cupola/Tower/Chimney*
- iv. *Decorative cornice of roof line (flat roof only)*

d. At least two (2) of the following facade elements:

- v. *Bay window*
- vi. *Balcony*
- vii. *Porch*
- viii. *Shutters*
- ix. *Window trim with 4" minimum width*
- x. *Patterned finish (scales, shakes, wainscoting, brick, or stone)*

3. Townhomes shall have a front porch with a minimum depth of 4 feet.

Development Conditions - Commercial Outparcel



1. Use Standards: The proposed use standards will restrict certain uses otherwise permitted in the RMX zoning district. This will encourage more neighborhood oriented and small businesses to better serve residents.
 - » Businesses with operating hours of 24 hours per day are not permitted.
 - » Vape, Tobacco, and CBD Stores, or businesses that sell these products are not permitted.
 - » The following principal uses shall be the only allowable uses, and shall be permitted by-right:
 - Child/Adult Day Care Center (6 or more people)
 - Personal Services
 - Professional Services
 - Medical Services
 - Neighborhood Retail/Restaurant (2,000 sf or less)
 - Allowed RMX district accessory uses
 - Bar/Tavern/Microbrewery
 - Studio (Arts, dance, martial arts, music)
2. Developer shall stub potable water, sanitary sewer, and storm drainage to commercial outparcel prior to plat recordation.
3. Developer shall construct curb, gutter, and sidewalk improvements along commercial outparcel prior to plat recordation.
4. The commercial outparcel shall not be owned by the Homeowners Association.
5. Developer is responsible for maintaining the commercial outparcel in a manner consistent with Lyndon Oaks Homeowners Association properties, until the commercial property is developed and/or sold by developer.
6. A 10' landscape buffer shall be planted adjacent to residential lots prior to plat recordation.
7. Parking lots shall contain vegetative screening in accordance with Knightdale's UDO.
8. Buildings shall be permitted up to two (2) stories.
9. Buildings shall meet the design standards for mixed-use buildings in UDO Sec 6.8.
10. All infrastructure on the residential portion of the project shall be sized appropriately to accommodate a two story commercial building.

09



**SITE DEVELOPMENT
ALLOWANCES**

Lyndon Oaks - Site Development Allowances

The proposed zoning for Lyndon Oaks is RMX-PUD. Lyndon Oaks meets all standards set forth in the Town of Knightdale's Unified Development Ordinance with the exception of a few conditions. Due to primarily environmental and other site constraints, and in order to provide more landscape conservation and beneficial common open space for an overall better-integrated design, the applicant is requesting the following site development allowances:

Bulk and Dimensional Standards (UDO Sec. 3.4)

- Lot width (street loaded)
 - » In the RMX base district, the minimum required lot width for a street loaded single-family lot is 80', and the minimum lot width for an alley-loaded single-family lot is 30'.
 - » In order to create a variety of housing types and price-points, and a more walkable community, we are proposing single family detached front-loaded homes with lot widths of less than 80', and single family attached alley-loaded homes with lot widths of less than 30'.
- Driveway length
 - » In the RMX base district, the minimum required driveway length is 35' for a residential lot.
 - » For front-loaded residential types, we are proposing a 25' minimum driveway length, and a 20' minimum driveway length for rear-loaded. This is mitigated by ample on-street guest parking, and the proposed driveway lengths allow for homeowners or guests to park a car in the driveways without blocking pedestrian facilities.
- Setbacks
 - » For the House building type, the minimum required rear setback is 25' and the side setbacks can be a minimum of 20% of the lot width (to a minimum of 5' for lots over 60' wide and 3' for lots less than 60' wide).

Standard Street Sections (UDO Sec. 10.4.A.1.b & Town Standard Details)

- Local Street Section - Two Way
 - » The standard detail calls for a 54.0' right-of-way, and allows for informal parking.
 - » Lyndon Oaks is proposing on-street parking along sections of road where there are no residential driveways. On street parking will allow for guest parking near units. We are not providing on-street parking on streets with front-loaded units to avoid any potential sight triangle conflicts as residents exit their driveways.

Proposed Distribution (UDO Sec. 11.1.B)

- Lyndon Oaks is providing residential uses and nonresidential space, and thus has a required distribution of uses listed in Knightdale's UDO Sec. 11.1.B. Lyndon Oaks will meet the required distribution ranges for single-family homes, but will not be providing a multifamily component, and the commercial component, which is not at a planned Neighborhood Node and will likely be small-scale neighborhood-serving commercial or office use(s), is smaller than the minimum 5%. The proposed distribution range for townhomes is also smaller than the minimum of 10%. The proposed

use distribution is as follows:

» Dwelling - Single Family	22.9%
» Dwelling - Townhouse	9.9%
» Retail/Restaurant/Entertainment/Office/Service	0.5%

Residential Clearing & Grading (UDO Sec. 9.3.B)

- Lyndon Oaks will require a site development allowance pertaining to Residential Clearing and Grading as specified in Section 9.3.B of the UDO in order to provide more efficient utility infrastructure and effective stormwater design.
 - » Currently, mass grading is prohibited on lots 60' in width or greater.
 - » Lyndon Oaks requests to mass grade 60' and 70' single-family lots (an additional 96 lots, 19.2% of the lots being developed). Any lot 80' in width or greater shall not be mass graded.

Street Section for Neighborhood Streets (UDO Sec. 11.3.B.3.)

- The project places residential development in pods to avoid additional impacts to the site's many streams and wetlands, and the pod at the center of the site is greater than 20 acres and can only be accessed by the street to the west of the Widewaters Parkway roundabout without creating additional undue environmental impacts. Because the street provides access from Widewaters Parkway to a residential area of over 20 acres, UDO 11.3.B.3 would require a Main Street section; however, Lyndon Oaks proposes a modified Local Street Section - Two Way, which is 16' feet narrower than the Main Street section, and will allow for more green space on the site. The proposed modified Local Street Section - Two Way would not require a median, which would cause the residential sections to the north and south of the road to be disconnected and makes pedestrian crossings more difficult. The project does not wish to build a larger street section than what is needed to access this pod of residential development, and that will not be needed to connect to what is planned to be Mixed Density Residential development on the approximately 20 vacant acres to the east on Clifton Road, which is a dead-end road and is not planned to connect over I-87 on the Roadway Network Plan. Thus, this future street connection will likely only serve to connect two residential neighborhoods and will not collect regional trips, so a more neighborhood-friendly, slower street section is more appropriate.

Lyndon Oaks - Site Development Allowances Continued

Rear-Loaded Homes that Front a Public Green with Pedestrian Facilities (UDO Sec. 11.2.C.11)

- In order to provide flexibility for the proposed neighborhood design that includes small groups of rear-loaded townhomes that front passive open space areas with pedestrian facilities to encourage community gathering and use of alternative forms of transportation, Lyndon Oaks requests a site development allowance for open space areas that have a minimum of 34' of frontage on either a public street or alley, rather than 50' of frontage.

Public Alleys for Utilities (UDO Sec. 10.4.A.1.b)

- Lyndon Oaks requests 32' public rights-of-way (alleys), as shown in the Master Plan, in order to provide utilities to rear-loaded homes that either front public open space rather than a public right-of-way, or do not directly front a public right-of-way, and alley access provides a more-direct route to connect homes to public utilities (particularly around the Widewaters Parkway roundabout).

Modified Avenue (currently called Tart Farm Rd) and Widewaters Parkway Intersection Design (UDO 11.3.B.4)

- Lyndon Oaks proposes a four-way stop with traffic calming measures, including high-visibility crosswalks, stamped concrete, Rectangular Rapid Flash Beacons (RRFBs), and pedestrian refuges in the Widewaters Parkway median at the intersection of the Urban Main Street, and Widewaters Parkway. These measures will slow down traffic and ensure pedestrian safety at this intersection, which is very close to the intersection of Widewaters Parkway and the Roadway Network Plan's planned Main Street connection to a future Neighborhood Node at the intersection of Bethlehem and Old Faison Roads, where the development will be providing a roundabout to calm traffic as well.

10

**NEIGHBORHOOD
MEETING**

Meeting Information

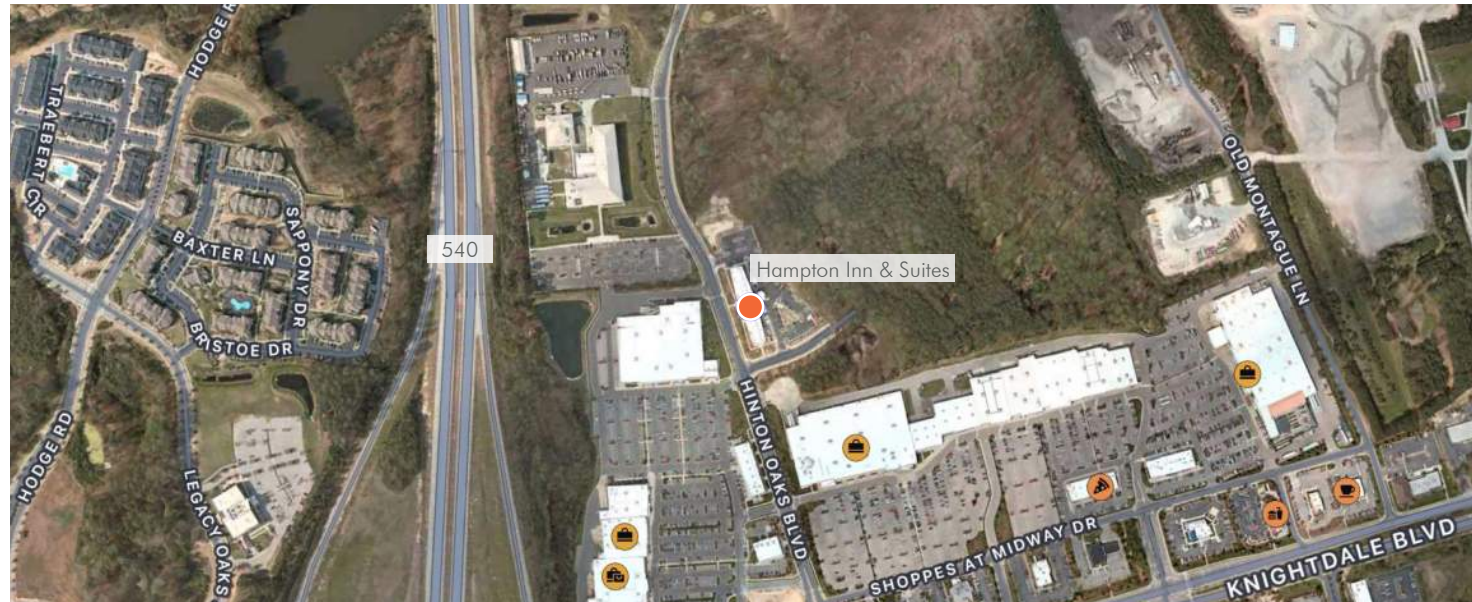


Figure 23: Hampton Inn & Suites Knightdale Vicinity Map (Not to scale)

A neighborhood meeting was held on November 14th, 2023 at 6:00 pm at the Hampton Inn & Suites Knightdale. See Figure 23 for a map of the meeting location. There were approximately 40 neighbors in attendance, along with a member of the development team, four members of the design team, and one Senior Planner from the Town of Knightdale.

Date of meeting:	Tuesday Nov. 14, 2023
Time of meeting:	6:00 pm EST
Meeting address:	Hampton Inn & Suites Knightdale 405 Hinton Oaks Blvd. Knightdale, NC 27545
Developer:	
» DR Horton:	Reese Bridges, PE, Entitlements Manager Cameron Jones
Design consultants:	
» Urban Design Partners:	Brian Richards, PLA
» Ramey Kemp Associates:	Caroline Cheeves, PE
» Parker Poe:	Ashley Honeycutt Terrazas, Land Use Attorney
Town of Knightdale:	Gideon Smith, AICP, Senior Planner

Neighborhood Meeting Questions

At the neighborhood meeting for Lyndon Oaks, the neighbors had questions about stormwater, density, renter vs. owner housing, traffic, number of new children in schools, forced annexation, construction timeline, target price for homes, quality of residents moving, wildlife habitat, and property values. Their questions are as follows:

» **Question/Concern #1: Stormwater, Runoff, and Grading**

- Applicant clarified that grading is not complete but the goal is for the site to be balanced (no import, no export dirt) but ultimately the site will tie into the grade along the perimeter of the site. Stormwater runoff is state-regulated and cannot be increased on adjacent properties. SCMs are monitored yearly.

» **Question/Concern #2: How Is Density Calculated?**

- Applicant clarified that density is calculated based on total gross acreage, it does not exclude amenity areas, thus residential density on the site is less than 3 units/acre.

» **Question/Concern #3: Renter vs. Owner-Occupied Housing**

- Applicant clarified that North Carolina law prohibits regulating occupancy in a zoning ordinance.

» **Question/Concern #4: Traffic on Bethlehem Road & Roundabouts Being Dangerous for Large Emergency Vehicles**

- Applicant clarified that roundabouts are designed such that emergency vehicles can drive over the center island. Roundabouts are safer because they eliminated left turns which are the most dangerous at an intersection. Knightdale UDO has been prioritizing roundabouts to improve community safety.

» **Question/Concern #5: Traffic in General, TIA Process, Intersections Studied**

- Applicant's transportation engineer clarified the Traffic Impact Analysis process and highlighted that focus is on weekday peak hours, 7AM-9AM and 4PM-6PM. She clarified that all of the traffic counts and turning movements are taken into account. She listed the intersections that would be studied. She clarified that the goal of TIA is to determine worst-case scenario (maximum trip generation at buildout). Town requires study of roadway network considering compounded growth until 2042. TIA would be submitted with the application.

» **Question/Concern #6: Number of New Children in Schools & Strain on Emergency Services**

- Applicant clarified information will be submitted to Wake County Public Schools and emergency services.

» **Question/Concern #7: Taking Neighbors Land, Forced Annexation**

- Applicant clarifies that it is giving land to Knightdale to make roadway improvements. Applicant clarified that roadway improvements will be within the already existing public right of way, but if property needed to be acquired to make an improvement, the Town and NCDOT cannot use eminent domain to take property for a private development. NC law now requires voluntary annexation.

» **Question/Concern #8: Timeline of Construction of Development**

- Applicant clarified estimated timing: Zoning process through Spring and Summer of 2024; Design drawings (water, sewer, grading, traffic) 12 months later (end of 2024); At least a year to a year and a half to develop the first phase (end of 2025, beginning 2026); begin wrapping up houses at end of 2026; Early 2027 moving first customers into first phase

Neighborhood Meeting Questions (Continued)

- » **Question/Concern #9: Target Price for the Homes**
 - Applicant clarified that sizes ranged from 1,500 square feet for townhouses and smaller houses to 3,000+ square feet for single family homes; in today's terms, pricing would range from \$200,000 to \$600,000+, but could not commit to pricing for future market rates.

- » **Question/Concern #10: Quality of Residents Moving**
 - Town is strict about architectural standards, and the plan is to provide a high quality product.

- » **Question/Concern #11: Wildlife and Native Habitat**
 - Applicant clarified that natural vegetation will be retained around pond, lake, and streams and there will be a walking trail as a natural amenity onsite and they are doing their homework to develop consistently with the history of the land. Approximately 17 acres of preserved area.

- » **Question/Concern #12: Decrease in Property Values**
 - Applicant clarified they have never seen property values decrease due to development with exception of some industrial projects.

Attendance Sheet

Neighborhood Meeting for Lyndon Oaks Planned Unit Development

Hampton Inn & Suites, 405 Hinton Oaks Blvd, Knightdale, NC 27545

Date: November 14, 2023

Name	Address	Name	Address
Karen Palke	121 Meadows Cr Knightdale	Kenny Holley	307 Rochelle Dr. Knightdale
Leamon Dixon	107 Shady Circle Knightdale	Donald Kay	932 Bethlehem Rd Knightdale
Eddie Matthews	106 Shady Circle Knightdale	Barbara & Dick Sossomon	1124 Poplar Circle Knightdale
Joyce Liles (Gayle)	4040 Clifton Rd	Brad Pope	1008 Bethlehem Rd. Knightdale
Tim Ferrell	3033 Churchill Road	Carrie Gray	1940 Bethlehem Rd. Raleigh
Kristin Trent	5309 Crosscut Pl	Michael & Karen Palko	121 Meadows Circle Knightdale
Don Curtis Jr.	5309 Crosscut Pl	Laurie Pope	116 Bethlehem Rd Knightdale
Carole & Kenny Faison	802 Bethlehem Rd Knightdale	Sam Maise	203 Robertson St. Knightdale
Chris Posh	4308 Old Faison Rd Knightdale	Patricia Loftin	1044 Stone Eagle Lane Raleigh 27610
Trey Tart	106 Tart Farm Rd Knightdale	Sara Hauser	4228 K Held Rd Knightdale
EC Tart	4325 Old Faison Rd		
Sonya Debnam	4521 Old Faison Rd		
Wesley Knapp	919-740-9955 Churchill HOA		
Nanay & Gene Zack	133 Meadows Circle Knightdale		
Caster Kennemer			
Gwen Gray	4275 Old Faison Rd Knightdale		
Don & Doris Curtis	5309 Cross Cut Place Knightdale		
Annette Miltz	128 Meadows Circle Knightdale		
Trudy Guffey	5101 Woodfield Ln Knightdale		
Rhonda Neely	129 Meadows Circle Knightdale		
Brian & Kim King	125 Meadows Circle Knightdale		
Tim & Denise Jones	132 Meadows Circle Knightdale		
Jack & Angela Worden	4407 Omaha Dr. Knightdale		
Gerald & Ann Akland	112 Old Ferrell Rd		

NEIGHBORHOOD MEETING

URBAN
DESIGN
PARTNERS

LANDSCAPE ARCHITECTURE + CIVIL ENGINEERING



November 3, 2023

Dear Knightdale Area Property Owner:

You are invited to a neighborhood meeting to learn more about a proposed project adjacent to or near your property. A Neighborhood Meeting is required by the Town of Knightdale Unified Development Ordinance and all property owners within 200 feet of the proposed development must receive notification of the meeting. The meeting will be an opportunity for residents and property owners to learn more about the project and provide feedback. This is the very beginning of the rezoning process prior to submitting an application to the Town. Following this meeting, the next steps include possible revisions to the plan based on comments received, application submittal, a Town Council Joint Public Hearing, a Land Use Review Board meeting, and finally the Town Council decision.

Meeting Date: November 14, 2023 Meeting Time: 6:00 – 7:00 pm

Meeting Location: The Hampton Inn & Suites Knightdale
405 Hinton Oaks Blvd., Knightdale, NC 27545

Application Type: Planned Unit Development rezoning

Approving Authority: Town Council Legislative Approval

Address/PIN: 0 Bethlehem Road (PIN 1743953683)
940 Bethlehem Road (PIN 1753152116)
0 Old Faison Road (PIN 1743971085)
0 Old Faison Road (PIN 1753071583)
4313 Old Faison Road (PIN 1743989384)
4325 Old Faison Road (PIN 1743986356)
106 Old Tart Farm Road (PIN 1743976575)

Description of Proposal: The applicant is requesting to rezone 7 parcels of land in order to build a primarily residential community with a small amount of neighborhood-serving commercial space. The land is currently zoned Rural Transition (RT) and is proposed to be rezoned to General Residential Low Density (GR3) or Medium Density (GR8) and Residential Mixed Use (RMX) or Neighborhood Mixed Use (NMU), with a Planned Unit Development overlay (PUD). The proposed uses are front and rear-loaded single-family detached homes, duplexes, townhomes, and commercial uses.

Estimated Submittal Date: November 27, 2023

Enclosed is a map showing the location of the property being considered for this proposal. Additional materials and information may be available at the meeting. Please see the Development Services 2023 Meeting and Submittal Schedule for future public meetings dates

https://www.knightdalenc.gov/sites/default/files/uploads/DevelopmentServices/submittal_and_meeting_schedule_2023.pdf).

If you have any questions, comments, or concerns about the proposal prior to the meeting you may contact me at (919) 835-4043 or via email at ashleyterrazas@parkerpoe.com. These will be recorded as part of the meeting summary, which is submitted to Town staff and elected officials. You may also contact the Town of Knightdale Development Services Department at (919) 217-2243.

Thank you,

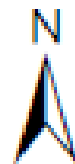
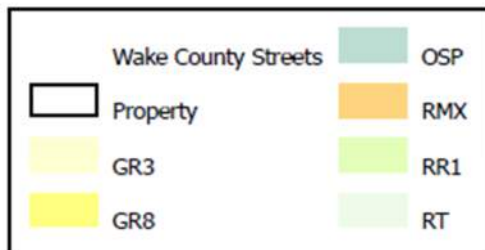
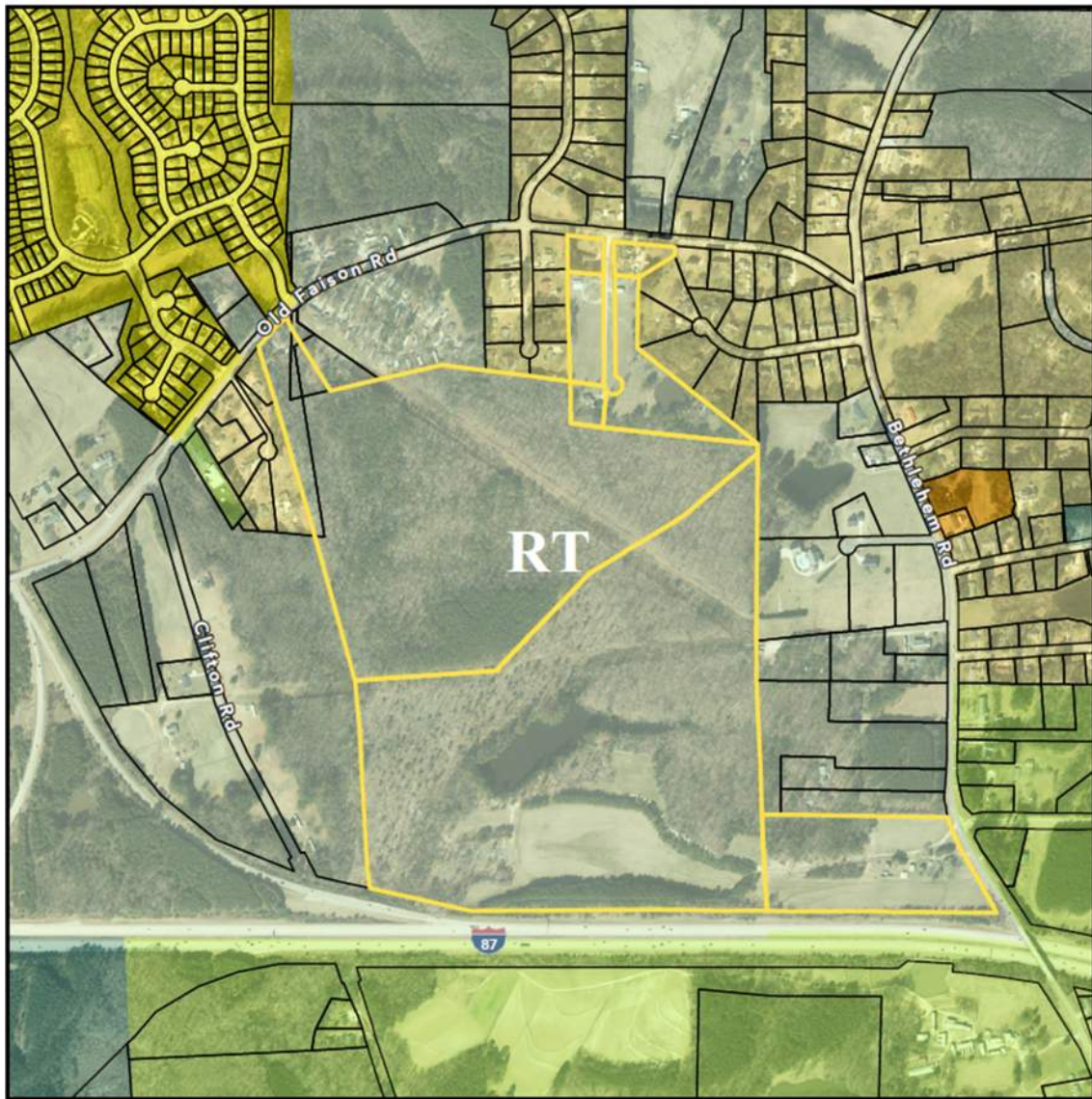
Ashley Honeycutt Terrazas

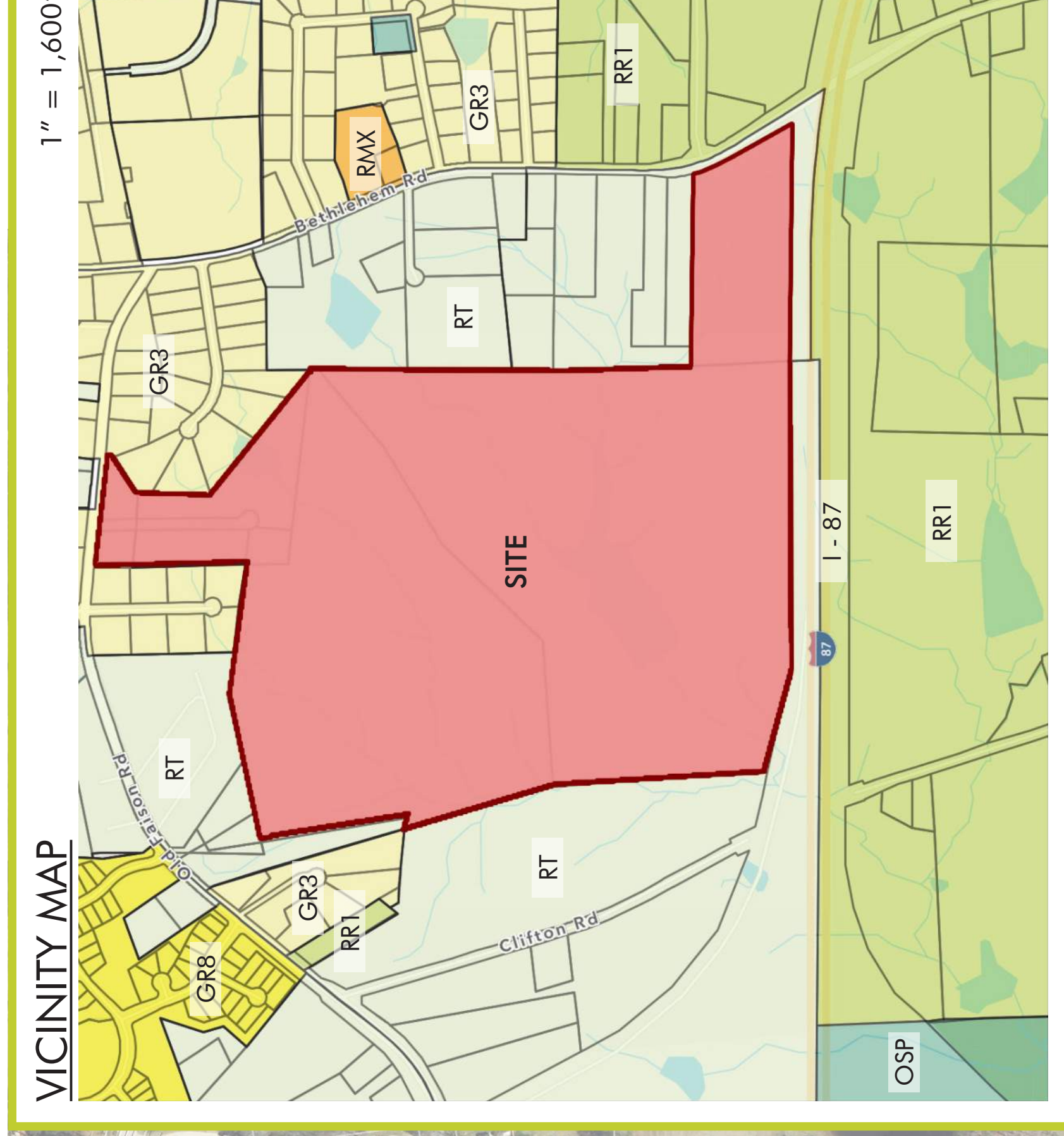
Cc: Town of Knightdale Development Services Department

Meeting Agenda

1. Applicant presentation: applicant will describe the nature of this rezoning request.
2. Public questions: applicant will field any questions from the public.

Vicinity Map w/ Current Knightdale Zoning





NOTES

1. THIS PLAN IS CONCEPTUAL IN NATURE AND IS SUBJECT TO CHANGE.
2. THIS EXHIBIT WAS PREPARED USING AVAILABLE RECORD INFORMATION, ALTA SURVEY, GIS MAPS, RECORD PLANS, AERIAL IMAGERY, AND LAND RECORDS.
3. THIS PLAN WILL BE SUBJECT TO REVIEW AND APPROVAL BY LOCAL AND STATE PLANNING AND ENGINEERING REVIEW AGENCIES.
4. THE WORK OF THIS PRODUCT IS THE PROPERTY OF URBAN DESIGN PARTNERS, PLLC. NO USE OR REPRODUCTION OF THIS PLAN IS PERMITTED WITHOUT WRITTEN AUTHORIZATION FROM URBAN DESIGN PARTNERS, PLLC.

SITE DATA

ACRES: ± 171.88 AC TOTAL

PIN: 1753-15-2116, 1743-95-3683,
 1743-97-1085, 1743-98-6356,
 1743-98-9384, 1753-07-1583,
 1743-97-6575

EXISTING ZONING: RT

PROPOSED ZONING: PUD

BASE DISTRICT: G

DEVELOPMENT SUMMARY

COMMERCIAL: ± 15,000 SF

RESIDENTIAL:

FRONT-LOADED (SINGLE FAM. DETACHED)

- 60' LOTS: 75 DU (15.0%)
- 70' LOTS: 33 DU (6.6%)
- 80' LOTS: 26 DU (5.2%)

REAR-LOADED (SINGLE FAM. DETACHED)

- 30' LOTS: 77 DU (15.4%)
- 35' LOTS: 47 DU (9.4%)
- 40' LOTS: 50 DU (10.0%)

TOWNHOMES (REAR-LOADED)

- 20' UNITS: 84 DU (16.8%)
- 22' UNITS: 88 DU (17.6%)
- 24' UNITS: 20 DU (4.0%)

TOTAL: 500 DU

REQ. PARKING (MINIMUM):

- SINGLE FAMILY: 616 SPACES
- TOWNHOMES: 384 SPACES
- COMMERCIAL: 53 SPACES
- » EV: 2 SPACES
- » BICYCLE: 2 SPACES

REQ. OPEN SPACE: ± 20.9 AC

500 DU x 3.5 BEDS = 1,750 BEDS

1,750 BEDS x 520sf = 910,000sf (20.9 AC)

Project Contact Information

Project Name: Lyndon Oaks **Proposed Zoning:** PUD

Location: See attached addendum.

Property PIN(s): See attached addendum. **Acreage/Square Feet:** See attached addendum.

Property Owner: See attached addendum.

Address: See attached addendum

City: _____ **State:** _____ **Zip:** _____

Phone: _____ **Email:** _____

Developer: D.R. Horton

Address: 7208 Falls of Neuse Road, Suite 201

City: Raleigh **State:** NC **Zip:** 27615

Phone: (984) 247-9614 **Email:** trbridges@drhorton.com

Engineer: Urban Design Partners

Address: 150 Fayetteville Street, Suite 1310

City: Raleigh **State:** NC **Zip:** 27601

Phone: (919) 275-5002 **Email:** _____

Builder (if known): Same as Developer

Address: _____

City: _____ **State:** _____ **Zip:** _____

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Lyndon Oaks
Ownership Addendum

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Pin: 1743989384
Deed Reference (book/page): 15474/755
Acreage: 1.08 ac
Owner: Lee T. Alford
Owner Address: 4313 Old Faison Road, Knightdale, NC 27545-9179

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Pin: 1743986356
Deed Reference (book/page): 2253/285
Acreage: 1.05 ac
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Owner Address: 4325 Old Faison Road, Knightdale, NC 27545-9179

Parcel 7

Site Address: 106 Tart Farm Road
Pin: 1743976575
Deed Reference (book/page): 9018/621
Acreage: 1.0 ac
Owner: Milton Edward Tart, III and Janet Stanley Tart
Owner Address: 106 Tart Farm Road, Knightdale, NC 27545-8134

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Neighborhood Meetings are a required step within the [rezoning process](#). The standards for Neighborhood Meetings can be found in [UDO Section 12.2.C.3](#). Neighborhood Meetings may be held in-person or virtually.

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March 22, 2024

Dear Knightdale Area Property Owner:

You are invited to a second neighborhood meeting to learn more about a pending project adjacent to or near your property. This is a voluntary meeting and will be an opportunity for the applicant to provide nearby residents and property owners updates and information about the case, and give them a chance to provide additional feedback. Following this meeting, the next steps include possible revisions to the plan based on comments received, a Town Council Joint Public Hearing, a Land Use Review Board meeting, and finally the Town Council decision.

Meeting Date: April 4, 2024

Meeting Time: 7:00 – 8:00 pm

Meeting Location: The Knightdale Recreation Center
101 Lawson Ridge Road, Knightdale, NC 27545

Application Type: Planned Unit Development rezoning

Approving Authority: Town Council Legislative Approval

Address/PIN: 0 Bethlehem Road (PIN 1743953683)
940 Bethlehem Road (PIN 1753152116)
0 Old Faison Road (PIN 1743971085)
0 Old Faison Road (PIN 1753071583)
4313 Old Faison Road (PIN 1743989384)
4325 Old Faison Road (PIN 1743986356)
106 Old Tart Farm Road (PIN 1743976575)

Description of Proposal: The applicant is requesting to rezone 7 parcels of land in order to build a primarily residential community with a small amount of neighborhood-serving commercial space. The land is currently zoned Rural Transition (RT) and is proposed to be rezoned to Residential Mixed Use (RMX) with a Planned Unit Development overlay (PUD). The proposed uses are front and rear-loaded single-family detached homes, duplexes, townhomes, and +/- 15,000 square feet commercial uses.

Submittal Date: Application was submitted on November 27, 2023

Enclosed is a map showing the location of the property being considered for this proposal. Additional materials and information may be available at the meeting. Please see the Development Services 2023 Meeting and Submittal Schedule for future public meetings dates (https://www.knightdalenc.gov/sites/default/files/uploads/DevelopmentServices/submittal_and_meeting_schedule_2023.pdf).

If you have any questions, comments, or concerns about the proposal prior to the meeting you may contact me at (919) 835-4043 or via email at ashleyterrazas@parkerpoe.com. These will be recorded as part of the meeting summary, which is submitted to Town staff and elected officials. You may also contact the Town of Knightdale Development Services Department at (919) 217-2243.

Thank you,

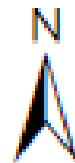
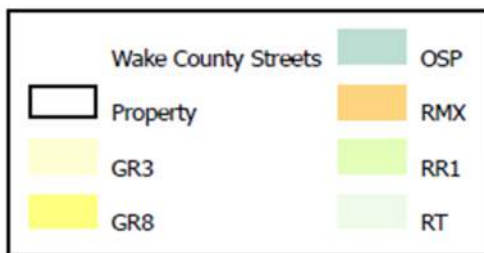
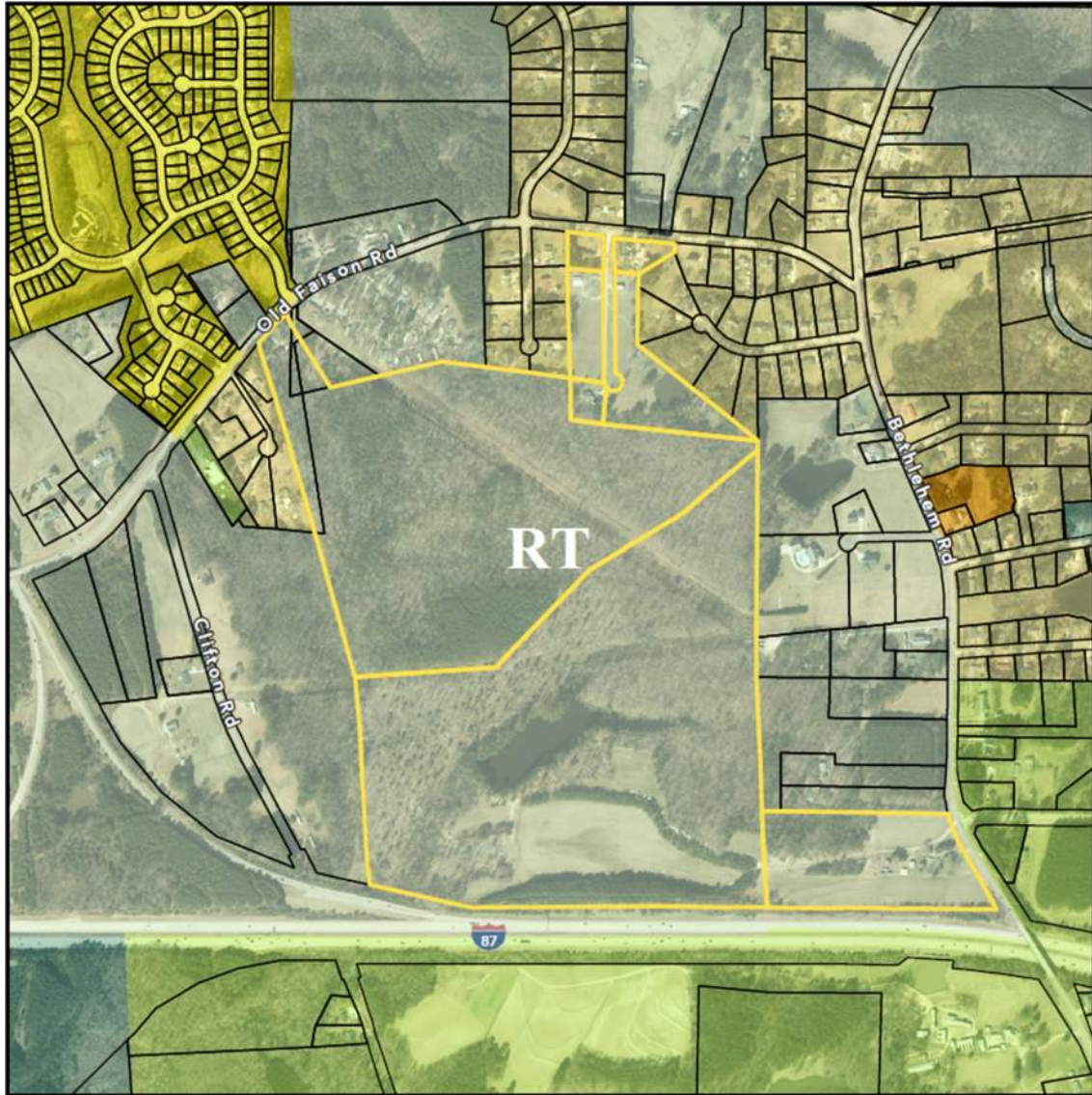
Ashley Honeycutt Terrazas

Cc: Town of Knightdale Development Services Department

Meeting Agenda

1. Applicant presentation: applicant will describe the nature of this rezoning request.
2. Public questions: applicant will field any questions from the public.

Vicinity Map w/ Current Knightdale Zoning



LYNDON OAKS

MASTER PLAN

KNIGHTDALE, NC

SITE DATA

- » ± 171.88 AC TOTAL
- » ACRES: 1753-15-2116; 1743-95-3683; 1743-97-1085; 1743-98-6356; 1743-98-9384; 1753-07-1583; 1743-97-6575
- » PINS:

- » EXISTING ZONING: RT
- » PROPOSED ZONING: RMX-PUD

DEVELOPMENT SUMMARY

COMMERCIAL: ± 15,000 SF

RESIDENTIAL:

FRONT LOADED (SINGLE FAM. DETACHED)

- 60' LOTS 71 DU
- 70' LOTS 25 DU
- 80' LOTS 11 DU

REAR LOADED (SINGLE FAM. DETACHED)

- 30' LOTS 71 DU
- 35' LOTS 53 DU
- 40' LOTS 49 DU

TOWNHOMES (REAR LOADED)

- 20' UNITS 96 DU
- 22' UNITS 106 DU
- 24' DUPLEX UNITS 18 DU

TOTAL: 500 DU

REQ. PARKING (MINIMUM):

- SINGLE FAMILY: 840 SPACES
- TOWNHOMES: 660 SPACES
- COMMERCIAL: 27 SPACES
- EV 2 SPACES
- BICYCLE 3 SPACES

- NOTES:
 1. THIS PLAN IS CONCEPTUAL IN NATURE AND IS SUBJECT TO CHANGE.
 2. THIS EXHIBIT WAS PREPARED USING AVAILABLE RECORD INFORMATION, ALTA SURVEY, GIS MAPS, RECORD PLANS, AERIAL IMAGERY, AND LAND RECORDS.
 3. THIS PLAN WILL BE SUBJECT TO REVIEW AND APPROVAL BY LOCAL AND STATE PLANNING AND ENGINEERING REVIEW AGENCIES.
 4. THE WORK OF THIS PRODUCT IS THE PROPERTY OF URBAN DESIGN PARTNERS, PLLC. NO USE OR REPRODUCTION OF THIS PLAN IS PERMITTED WITHOUT WRITTEN AUTHORIZATION FROM URBAN DESIGN PARTNERS, PLLC.





Project Contact Information

Project Name: Lyndon Oaks Proposed Zoning: PUD

Location: See attached addendum.

Property PIN(s): See attached addendum. Acreage/Square Feet: See attached addendum.

Property Owner: See attached addendum.

Address: See attached addendum

City: _____ State: _____ Zip: _____

Phone: _____ Email: _____

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Address: 7208 Falls of Neuse Road, Suite 201

City: Raleigh State: NC Zip: 27615

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NEIGHBORHOOD MEETING MINUTES

Lyndon Oaks – Amendment to PUD / Master Plan

Meeting Date/Time: Thursday, April 4, 2024 from 7:00 pm to 8:00 pm

Meeting Location: Knightdale Recreation Center, 101 Lawson Ridge Rd, Knightdale, NC 27545

Development Team: D.R. Horton – Reese Bridges, Anne Oakley

Parker Poe - Ashley Honeycutt Terrazas

Urban Design Partners - Brian Richards, Lexi Chacalos

Ramey Kemp - Caroline Cheeves

Town of Knightdale: -

Community Members: 12

Introduction: *The Development Team explained the rezoning and PUD process, the purpose of the neighborhood meeting, housing types, anticipated rezoning schedule, Comprehensive Plan Consistency, housing types and trails, planned amenities, potential commercial uses on Bethlehem Rd.*

Caroline Cheeves discussed the findings of the TIA and the recommended improvements that would be phased in with the project.

Question/Concern #1: Will Bethlehem Rd need to be widened for Old Faison/Bethlehem Rd improvement?

Transportation engineer clarified that the improvement had not been designed yet, and that design would happen later. Typically, there is enough existing right-of-way for at least one additional turn lane.

Question/Concern #2: Concerns about tractor trailers and heavy equipment driving over roundabouts on Bethlehem Rd

Transportation engineer clarified that roundabouts are constructed with mountable curb that trailers and heavy equipment can be driven over safely. Applicant clarified that Town preferred roundabouts and wants roundabouts at Crosscut/Bethlehem, and was also performing an additional study with respect to a roundabout at Old Faison/Bethlehem Rd. Applicant clarified that the TIA showed that a signal and turn lanes would operate better at Old Faison/Bethlehem Rd. That the project would be bringing at least a signal to that intersection, but the Town has made some indications that they still wanted a roundabout, so cannot guarantee it would not be a roundabout.

Question/Concern #3: Level of Service at Old Faison/Bethlehem Rd intersection

Transportation engineer clarified that this intersection currently has an LOS F, but that, even with the traffic from the proposed development, with a signal and turn lanes, it would operate at an LOS C.

Question/Concern #4: Queuing at future traffic signal on Old Faison/Bethlehem Rd blocking entrances to existing subdivisions on Old Faison Rd.

Transportation engineer clarified that, at three leg intersections, there are typically shorter cycle lengths and at the projected level of service, the waiting cars would clear with each cycle, so there should not be queuing in front of the Meadow Circle entrance. There would be breaks in traffic with the signal.

Question/ Concern #5: Concern about growth from other development in Knightdale and traffic

Applicant clarified that this background growth was taken into account for the TIA and that Applicant would be providing transportation improvements to mitigate impacts from other development that had already been approved and/or built. These improvements are not in NCDOT's nor the Town's budget, and will come much more quickly when applicant installs them.

Question/Concern #6: When would traffic improvements be installed? After neighborhood is built?

Transportation engineer clarified that NCDOT would require all improvements before neighborhood built out. After rezoning, improvement design and phasing for project is finalized, and then, typically, applicant submits a phasing plan to NCDOT to determine at which point during buildout improvements need to be installed.

Question/Concern #7: Where will sewer be coming from?

Applicant clarified that it is still working with City of Raleigh on the route of utilities. The most likely scenario is that it will be all gravity sewer, coming from under I-87. Based on feedback from CORPUD, a pump station is unlikely, but even if there were a pump station, it would be discreet and would be unlikely to impact surrounding neighbors.

Question/Concern #8: Who do neighbors contact if there are runoff or construction traffic issues during buildout?

There will be a sign on the site with a development manager's phone number for someone to call with any issues or concerns during construction.

Question/Concern #9: Which series/product type would be provided in the neighborhood?

Town is strict about architectural standards, and the plan is to provide a high-quality product. Exact product is not known at this time.

In today's terms, pricing would range from \$200,000 to \$600,000+, but could not commit to pricing for future market rates.

Question/Concern #10: How is density calculated? Which areas are being conserved?

Density is about 2.91 units/acre, based on the site's total acreage. Applicant clarified that trees in the stream buffers would be preserved and that it would preserve trees in perimeter buffers where possible.

Question/Concern #11: Buffers

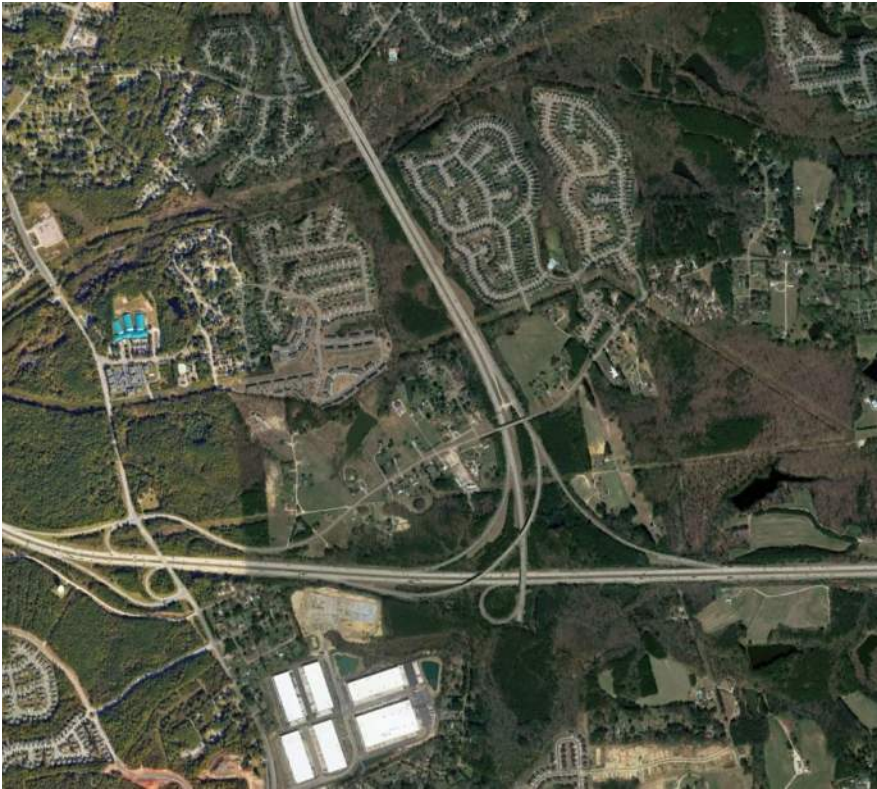
Applicant clarified that a 20' perimeter buffer with landscaping or existing vegetation between lots and adjacent properties would be provided and a 50' buffer with a berm on I-87.

Question/Concern #12: Where did project name come from?

"Lyndon" is the first name of one of the engineers that was originally working on the project, and the marketing department liked the name. They wanted to incorporate it into the project name.

Attendance List

Name	Address
Michael & Karen Palko	121 Meadows Cir
Leamon Dixon	107 Shady Cir
Brian & Kim King	125 Meadows Cir
Jerry & Gayle Liles	4040 Clifton Rd
Rhonda Neely	129 Meadows Cir
Janet Starly	
Barbara Sossomon	1124 Poplar Cir



Lyndon Oaks
Traffic Impact Analysis
Knightdale, North Carolina

TRAFFIC IMPACT ANALYSIS

FOR

LYNDON OAKS

LOCATED

IN

KNIGHTDALE, NC

Prepared For:
Urban Design Partners
1318 Central Avenue, Suite E6
Charlotte, NC 28205

Prepared By:
Infrastructure Consulting Services, Inc.
dba

Ramey Kemp Associates
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11-29-2023

NOVEMBER 2023

RKA Project No. 22584

Prepared By: CDS

Reviewed By: CTS

TRAFFIC IMPACT ANALYSIS
LYNDON OAKS
KNIGHTDALE, NORTH CAROLINA

EXECUTIVE SUMMARY

1. Development Overview

A Traffic Impact Analysis (TIA) was conducted for the proposed Lyndon Oaks subdivision development in accordance with the Knightdale (Town) Unified Development Ordinance (UDO) and North Carolina Department of Transportation (NCDOT) capacity analysis guidelines. The proposed development is to be located on the west side of Bethlehem Road between Interstate 87 and Old Faison Road in Knightdale, North Carolina. The proposed development, anticipated to be completed in 2031, is proposed to consist of 308 single-family homes, 192 townhomes, and 15,000 square feet of commercial space. Site access is proposed via one (1) full-movement driveway on Old Faison Road at the existing location of Tart Farm Road and one roundabout at the existing intersection of Bethlehem Road and Crosscut Place.

The study analyzes traffic conditions during the weekday AM and PM peak hours for the following scenarios:

- 2023 Existing Traffic Conditions
- 2031+1 No-Build Traffic Conditions
- 2031+10 No-Build Traffic Conditions
- 2031+1 Build Traffic Conditions
- 2031+10 Build Traffic Conditions

2. Existing Traffic Conditions

The study area for the TIA was determined through coordination with the Town and NCDOT and consists of the following existing intersections:

- Old Faison Road and Bethlehem Road
- Old Faison Road and Tart Farm Road
- Old Faison Road and Pleasant Colony Drive
- Old Faison Road and Quiet Way

- Bethlehem Road and Crosscut Place
- US 64 EB Ramps and Hodge Road
- US 64 WB Ramps and Old Faison Road
- Smithfield Road and Old Ferrell Road
- Bethlehem Road and Poole Road

Existing peak hour traffic volumes were determined based on traffic counts conducted at the study intersection listed below, in April of 2023 by Quality Counts during a typical weekday AM (7:00 AM – 9:00 AM) and PM (4:00 PM – 6:00 PM) peak periods:

- Old Faison Road and Bethlehem Road
- Old Faison Road and Tart Farm Road
- Old Faison Road and Pleasant Colony Drive
- Old Faison Road and Quiet Way
- Bethlehem Road and Crosscut Place
- US 64 EB Ramps and Hodge Road
- US 64 WB Ramps and Old Faison Road
- Smithfield Road and Old Ferrell Road
- Bethlehem Road and Poole Road

Traffic volumes were balanced between study intersections, where appropriate.

3. Site Trip Generation

The proposed development is assumed to consist of 308 single-family homes, 192 townhomes, and 15,000 square feet of commercial space. Average weekday daily, AM peak hour, and PM peak hour trips for the proposed development were estimated using methodology contained within the ITE Trip Generation Manual, 11th Edition. Table E-1 provides a summary of the trip generation potential for the site.

Table E-1: Site Trip Generation

Land Use (ITE Code)	Intensity	Daily Traffic (vpd)	Weekday AM Peak Hour Trips (vph)		Weekday PM Peak Hour Trips (vph)	
			Enter	Exit	Enter	Exit
Single-Family Detached (210)	308 DU	2,840	52	155	180	106
Townhomes (215)	192 DU	1,414	24	70	66	45
Strip Retail Plaza (822)	10 KSF	652	17	12	39	39
High-Turnover Restaurant (932)	5 KSF	536	26	22	28	17
Total Trips		5,442	119	259	313	207
<i>Internal Capture (7% AM & 6% PM)*</i>			-9	-17	-20	-13
Total External Trips			110	242	293	194
<i>Pass-By Trips</i>			-0	-0	-19	-19
Total Primary (New) Trips			110	242	274	175

4. Future Traffic Conditions

Through coordination with NCDOT and the Town, it was determined that an annual growth rate of 3% would be used to generate 2032 projected weekday AM and PM peak hour traffic volumes. For the 2041 future analysis, a 1% growth rate was applied each year after the build +1 year analysis. The following adjacent developments were identified to be considered under future conditions:

- Riverview Commons
- Widewaters Expansion

5. Capacity Analysis Summary

The analysis considered weekday AM and PM peak hour traffic for 2023 existing, 2032 no-build, 2032 build, 2041 no-build, and 2041 build conditions. Refer to Section 7 of the TIA for the capacity analysis summary performed at each study intersection.

6. Recommendations

Based on the findings of this study, specific geometric and traffic control improvements have been identified at study intersections. The improvements are summarized below and are illustrated in Figure E-1.

Recommended Improvements by STIP W-5705AK Westbound I-87 Ramps and Hodge Road

- Construct westbound right turn lane with 100 feet of storage and appropriate decel and taper.
- Construct westbound left turn lane with 100 feet of storage and appropriate decel and taper.
- Modify signal timings and signal phasing

Recommended Improvements by Developer Old Faison Road and Bethlehem Road

- Coordinate with Town on desired improvements. Provide fee in lieu contribution as necessary

Old Faison Road and Tart Farm Road/Site Access A

- Construct an eastbound right turn lane with 75 feet of storage and appropriate decel and taper.
- Construct exclusive northbound left turn lane with full storage.
- Provide stop control for northbound approach.

Bethlehem Road and Crosscut Place/Site Access B

- Construct a single-lane roundabout.
- Construct eastbound approach with one ingress and one egress lane.
- Provide yield control for eastbound approach.

Westbound I-87 Ramps and Hodge Road

- Extend westbound left turn lane to contain 175 feet of storage and appropriate decel and taper.
- Extend southbound left turn lane to contain 275 feet of storage and appropriate decel and taper.

- Extend eastbound left-through lane to contain 275 feet of storage and appropriate decel and taper.

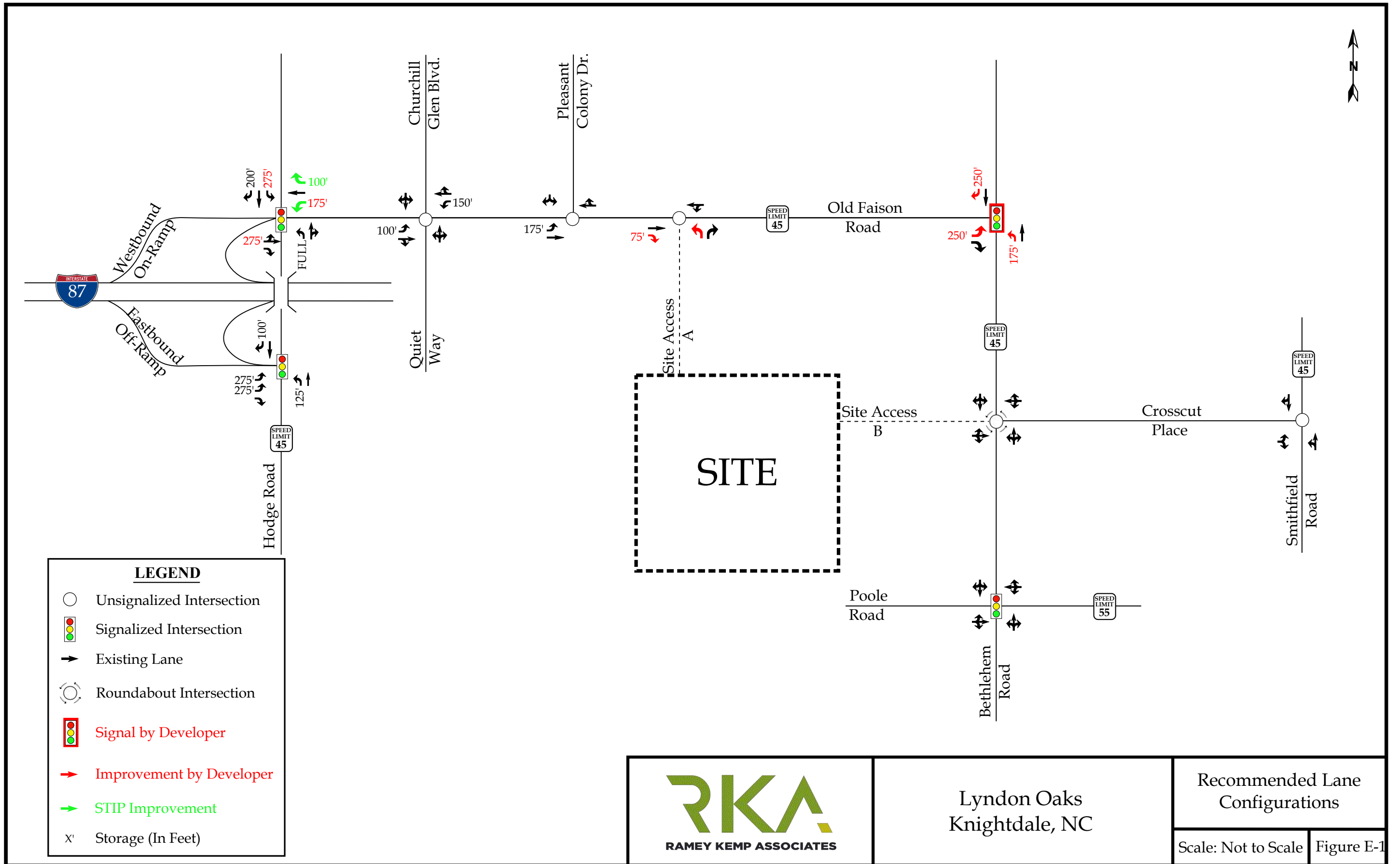


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- Appendix C: Signal Plans
- Appendix D: Adjacent Development Information
- Appendix E: Capacity Calculations – Old Faison Road and Bethlehem Road
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- Appendix H: Capacity Calculations – Old Faison Road and Churchill Glen Boulevard/Quiet Way
- Appendix I: Capacity Calculations – Bethlehem Road and Crosscut Place/Site Access B
- Appendix J: Capacity Calculations – I-87 Eastbound Ramps and Hodge Road
- Appendix K: Capacity Calculations – I-87 Westbound Ramps and Hodge Road
- Appendix L: Capacity Calculations – Smithfield Road and Old Ferrell Road
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TRAFFIC IMPACT ANALYSIS
LYNDON OAKS
KNIGHTDALE, NORTH CAROLINA

1. INTRODUCTION

The contents of this report present the findings of the Traffic Impact Analysis (TIA) conducted for the proposed Lyndon Oaks subdivision development to be located on the west side of Bethlehem Road between Interstate 87 and Old Faison Road in Knightdale, North Carolina. The purpose of this study is to determine the potential impacts to the surrounding transportation system created by traffic generated by the proposed development, as well as recommend improvements to mitigate the impacts.

The proposed development, anticipated to be completed in 2031, is proposed to consist of 308 single-family homes, 192 townhomes, and 15,000 square feet of commercial space. At the time that this TIA was completed, the commercial uses were unknown; however, for a conservative approach to the analysis, 10,000 square feet of strip retail and 5,000 square feet of high-turnover restaurant were assumed.

Per the Town of Knightdale (Town) Unified Development Ordinance (UDO), future analysis should include the build year + 1, as well as a future (build year + 10 years) scenario. The study analyzes traffic conditions during the weekday AM and PM peak hours for the following scenarios:

- 2023 Existing Traffic Conditions
- 2031+1 No-Build Traffic Conditions
- 2031+10 No-Build Traffic Conditions
- 2031+1 Build Traffic Conditions
- 2031+10 Build Traffic Conditions

1.1. Site Location and Study Area

The development is proposed to be located on the west side of Bethlehem Road between Interstate 87 and Old Faison Road in Knightdale, North Carolina. Refer to Figure 1 for the site location map.

The study area for the TIA was determined through coordination with the North Carolina Department of Transportation (NCDOT) and the Town and consists of the following existing intersections:

- Old Faison Road and Bethlehem Road
- Old Faison Road and Tart Farm Road
- Old Faison Road and Pleasant Colony Drive
- Old Faison Road and Quiet Way
- Bethlehem Road and Crosscut Place
- US 64 EB Ramps and Hodge Road
- US 64 WB Ramps and Old Faison Road
- Smithfield Road and Old Ferrell Road
- Bethlehem Road and Poole Road

Refer to Appendix A for the approved scoping documentation.

1.2. Proposed Land Use and Site Access

The proposed development is assumed to consist of the following uses:

- 308 single-family detached homes
- 192 townhomes
- 10,000 square feet (s.f.) of strip retail
- 5,000 s.f. of high-turnover restaurant

Site access is proposed via one (1) full-movement driveway on Old Faison Road at the existing location of Tart Farm Road and one roundabout at the existing intersection of Bethlehem Road and Crosscut Place. This roundabout is in concordance with the Town's code. Refer to Figure 2 for a copy of the preliminary site plan.

1.3. Adjacent Land Uses

The proposed development is located in an area consisting primarily of residential development.

1.4. Existing Roadways

Existing lane configurations (number of traffic lanes on each intersection approach), lane widths, storage capacities, and other intersection and roadway information within the study area are shown in Figure 3. Table 1 provides a summary of this information, as well.

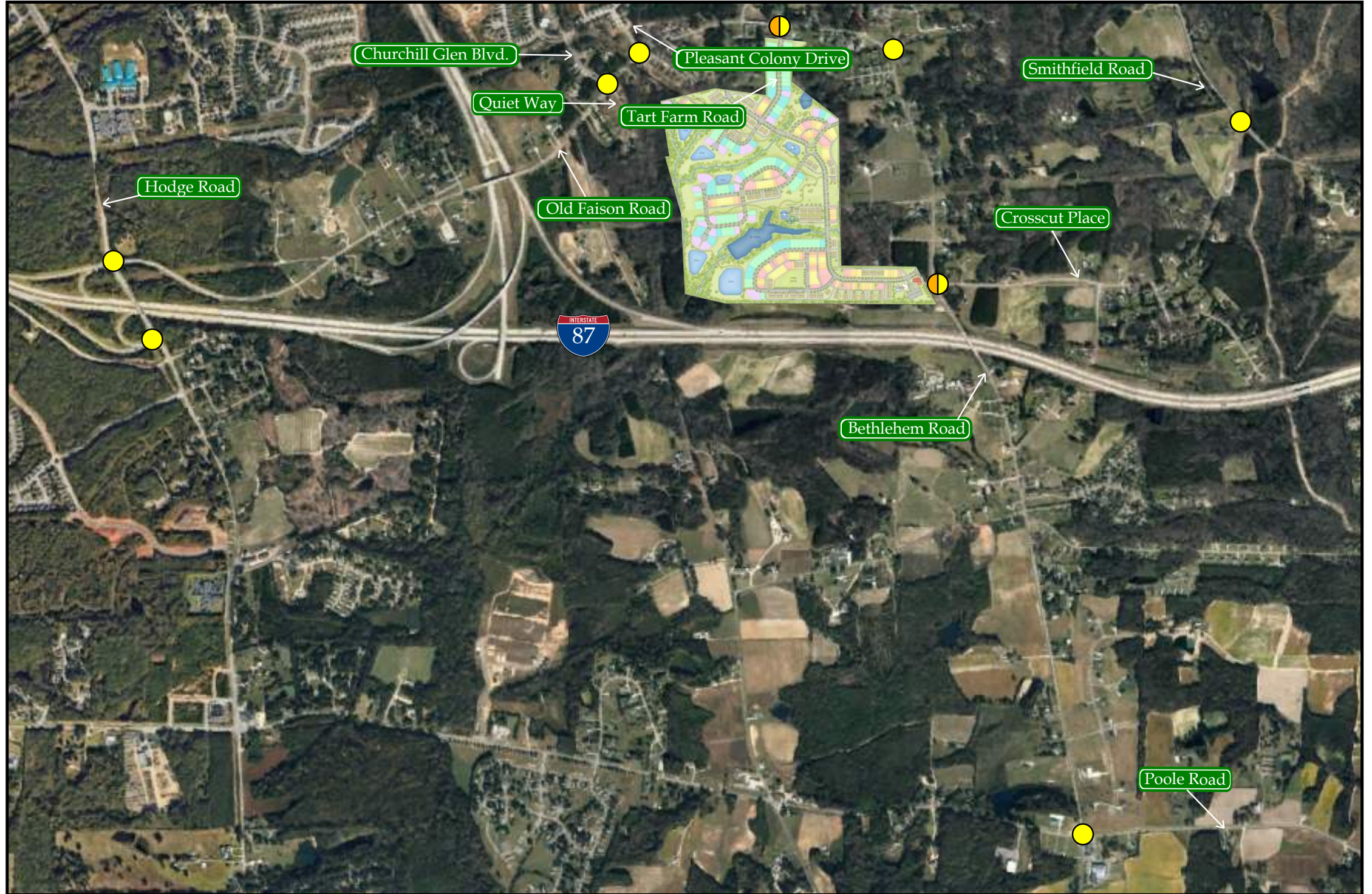
Table 1: Existing Roadway Inventory

Road Name	Route Number	Typical Cross Section	Speed Limit	2019 AADT (vpd)
I-495	I-495	8-lane divided	65 mph	88,500
Old Faison Road	SR 2515	2-lane undivided	45 mph	4,100**
Bethlehem Road	SR 5270	2-lane undivided	45 mph	6,400
Crosscut Place/Old Ferrell Road	SR 2512	2-lane undivided	45 mph	760**
Tart Farm Road	N/A	2-lane undivided	Not Posted	40***
Pleasant Colony Drive	N/A	2-lane undivided	25 mph	1,010***
Quiet Way	N/A	2-lane undivided	Not Posted	90***
Hodge Road	SR 2516	2-lane undivided	Not Posted	9,900
Poole Road	SR 1007	2-lane undivided	55 mph	4,600
Smithfield Road	SR 2233	2-lane undivided	45 mph	8,200

*ADT from 2021

**ADT from 2015

***ADT based on the traffic counts from 2023 and assuming the weekday PM peak hour volume is 10% of the average daily traffic.



LEGEND

- Study Intersection
- Proposed Site Access
- Study Area

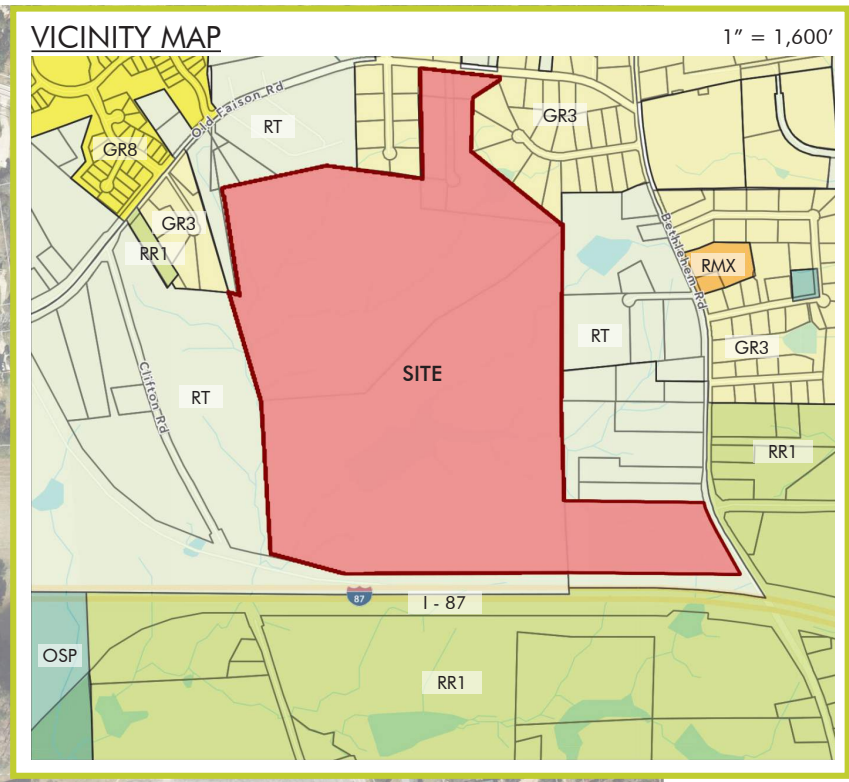


RKA
RAMEY KEMP ASSOCIATES

Lyndon Oaks
Knightdale, NC

Site Location Map

Scale: Not to Scale Figure 1



- NOTES**
1. THIS PLAN IS CONCEPTUAL IN NATURE AND IS SUBJECT TO CHANGE.
 2. THIS EXHIBIT WAS PREPARED USING AVAILABLE RECORD INFORMATION, ALTA SURVEY, GIS MAPS, RECORD PLANS, AERIAL IMAGERY, AND LAND RECORDS.
 3. THIS PLAN WILL BE SUBJECT TO REVIEW AND APPROVAL BY LOCAL AND STATE PLANNING AND ENGINEERING REVIEW AGENCIES.
 4. THE WORK OF THIS PRODUCT IS THE PROPERTY OF URBAN DESIGN PARTNERS, PLLC. NO USE OR REPRODUCTION OF THIS PLAN IS PERMITTED WITHOUT WRITTEN AUTHORIZATION FROM URBAN DESIGN PARTNERS, PLLC.

SITE DATA

ACRES:	± 171.88 AC TOTAL
PIN:	1753-15-2116, 1743-95-3683, 1743-97-1085, 1743-98-6356, 1743-98-9384, 1753-07-1583, 1743-97-6575
EXISTING ZONING:	RT
PROPOSED ZONING:	PUD
BASE DISTRICT:	GR8

DEVELOPMENT SUMMARY

COMMERCIAL:	± 15,000 SF
RESIDENTIAL:	
FRONT-LOADED (SINGLE FAM. DETACHED)	
60' LOTS	75 DU (15.0%)
70' LOTS	33 DU (6.6%)
80' LOTS	26 DU (5.2%)
REAR-LOADED (SINGLE FAM. DETACHED)	
30' LOTS	77 DU (15.4%)
35' LOTS	47 DU (9.4%)
40' LOTS	50 DU (10.0%)
TOWNHOMES (REAR-LOADED)	
20' UNITS	84 DU (16.8%)
22' UNITS	88 DU (17.6%)
24' UNITS	20 DU (4.0%)
TOTAL:	500 DU
REQ. PARKING (MINIMUM):	
• SINGLE FAMILY	616 SPACES
• TOWNHOMES	384 SPACES
• COMMERCIAL	53 SPACES
» EV	2 SPACES
» BICYCLE	2 SPACES
REQ. OPEN SPACE:	± 20.9 AC
500 DU x 3.5 BEDS =	1,750 BEDS
1,750 BEDS x 520sf =	910,000sf (20.9 AC)



LYNDON OAKS | SKETCH PLAN

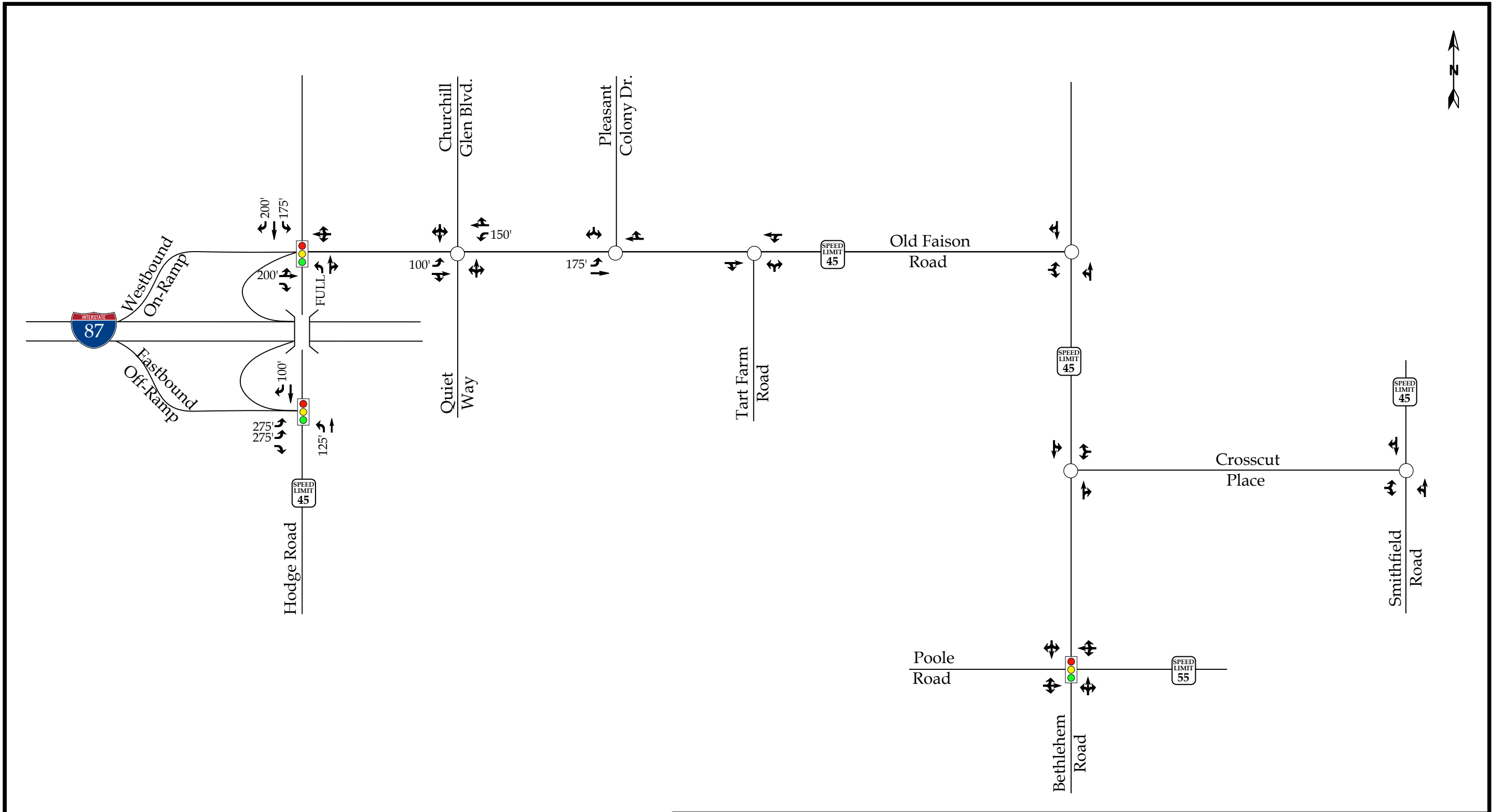
KNIGHTDALE, NC



OCTOBER 13, 2023

22-RDU-083

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Lyndon Oaks
Knightdale, NC

2023 Existing Lane Configurations	
Scale: Not to Scale	Figure 3

2. 2023 EXISTING PEAK HOUR CONDITIONS

2.1. 2023 Existing Peak Hour Traffic Volumes

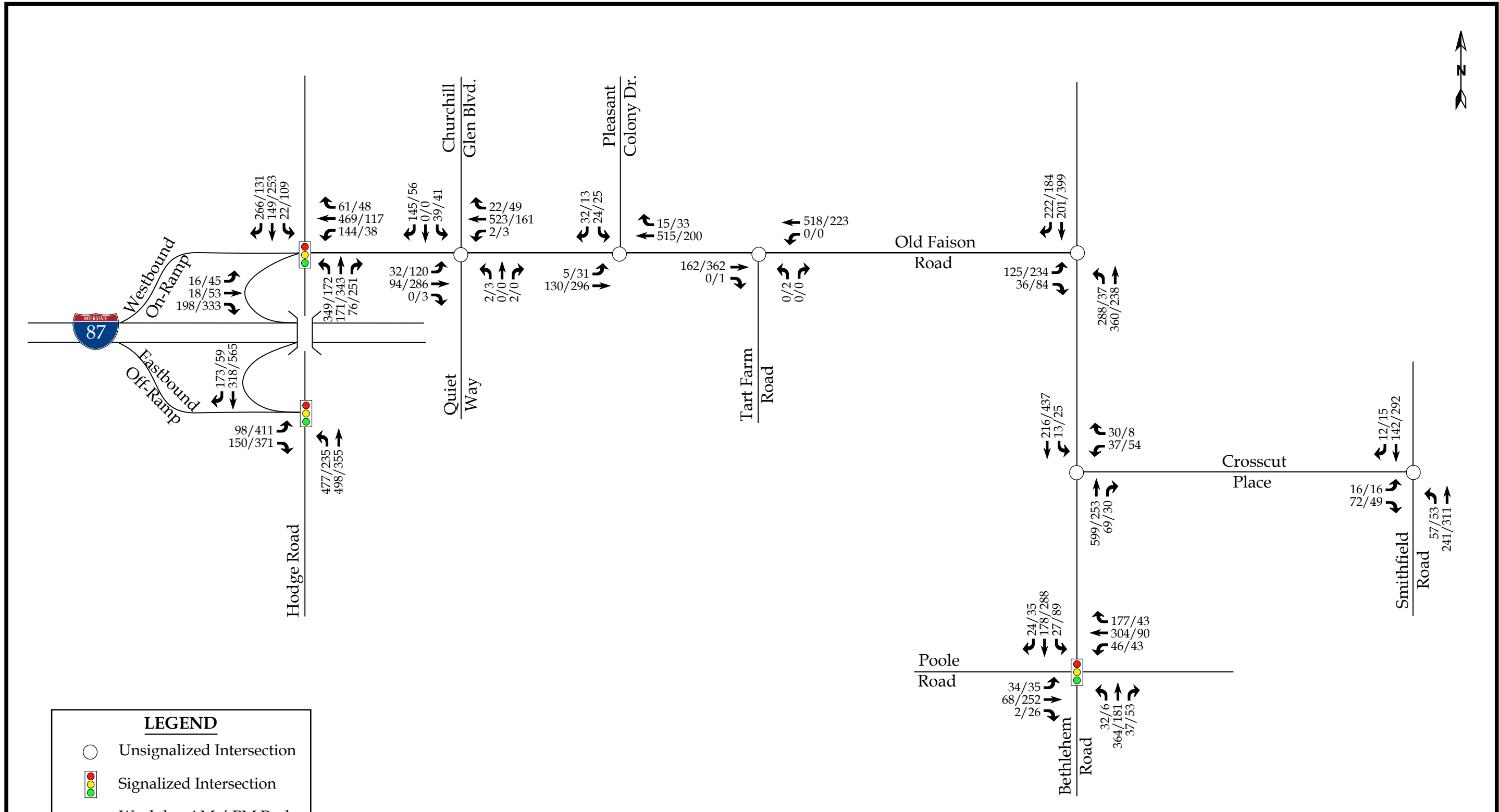
Existing peak hour traffic volumes were determined based on traffic counts conducted at the study intersections listed below, in April of 2023 by Quality Counts during a typical weekday AM (7:00 AM – 9:00 AM) and PM (4:00 PM – 6:00 PM) peak periods while schools were in session:

- Old Faison Road and Bethlehem Road
- Old Faison Road and Tart Farm Road
- Old Faison Road and Pleasant Colony Drive
- Old Faison Road and Quiet Way
- Bethlehem Road and Crosscut Place
- US 64 EB Ramps and Hodge Road
- US 64 WB Ramps and Old Faison Road
- Smithfield Road and Old Ferrell Road
- Bethlehem Road and Poole Road

Weekday AM and PM traffic volumes were balanced between study intersections, where appropriate. Refer to Figure 4 for 2023 existing weekday AM and PM peak hour traffic volumes. A copy of the count data is located in Appendix B of this report.

2.2. Analysis of 2023 Existing Peak Hour Traffic Conditions

The 2023 existing weekday AM and PM peak hour traffic volumes were analyzed to determine the current levels of service at the study intersections under existing roadway conditions. Signal information was obtained from NCDOT and is included in Appendix C. The results of the analysis are presented in Section 7 of this report.



LEGEND

- Unsignalized Intersection
- Signalized Intersection
- X / Y → Weekday AM / PM Peak Hour Traffic

Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

	Lyndon Oaks Knightdale, NC	2023 Existing Peak Hour Traffic	
		Scale: Not to Scale	Figure 4

3. NO-BUILD PEAK HOUR CONDITIONS

In order to account for growth of traffic and subsequent traffic conditions at a future year, no-build traffic projections are needed. No-build traffic is the component of traffic due to the growth of the community and surrounding area that is anticipated to occur regardless of whether or not the proposed development is constructed. No-build traffic is comprised of existing traffic growth within the study area and additional traffic created as a result of adjacent approved developments.

3.1. Ambient Traffic Growth

Through coordination with NCDOT and the Town, it was determined that an annual growth rate of 3% would be used to generate 2032 projected weekday AM and PM peak hour traffic volumes. For the 2041 future analysis, a 1% growth rate was applied each year after the build +1 year analysis. Refer to Figure 5a for 2032 projected peak hour traffic and Figure 5b for 2041 projected peak hour traffic.

3.2. Adjacent Development Traffic

Through coordination with NCDOT and the Town, Riverview Commons should be considered as an approved adjacent development in this study. A residential development located near the Widewaters expansion that is not yet approved by NCDOT was also to be considered in the analysis. Site trips for this development were generated based off of methodology contained in the ITE *Trip Generation Manual*, 11th Edition then distributed based on the regional distributions for the proposed site., Table 2, on the following page, provides a summary of the adjacent developments.

Table 2: Adjacent Development Information

Development Name	Location	Build-Out Year	Land Use / Intensity	TIA Performed
Riverview Commons	North of Poole Road, west of Hodge Road	2023	364 dwelling units of apartments and 30 townhomes	Davenport
Widewaters Expansion	North of Old Faison Road, West of Woodfield Lane	Before 2032	257 single-family attached, 98 single-family attached	N/A

It should be noted that including trips from the above adjacent developments on top of an annually compounded growth rate is anticipated to provide conservative results in this area, as local development growth is the most impactful for more rural areas. Additionally, there is expected to be interaction between some of the adjacent developments and the proposed development based on the different land uses; however, no reduction in adjacent development trips was proposed to provide a conservative estimation of future traffic volumes. It should be noted that the adjacent developments were approved, during scoping, by NCDOT and the Town. Adjacent development trips are shown in Figure 6. Additional adjacent development information can be found in Appendix D.

3.3. Future Roadway Improvements

Based on coordination with NCDOT and the Town, State Transportation Improvement Program (STIP) project W-5705AK was to be included in all future analyses. STIP W-5705AK is expected to widen the westbound approach of Old Faison Road (at Hodge Road) to provide exclusive left and right turn lanes. Updated signal plans for the intersection of Hodge Road and Old Faison Road can be found in Appendix C showing the improvements proposed by W-5705AK.

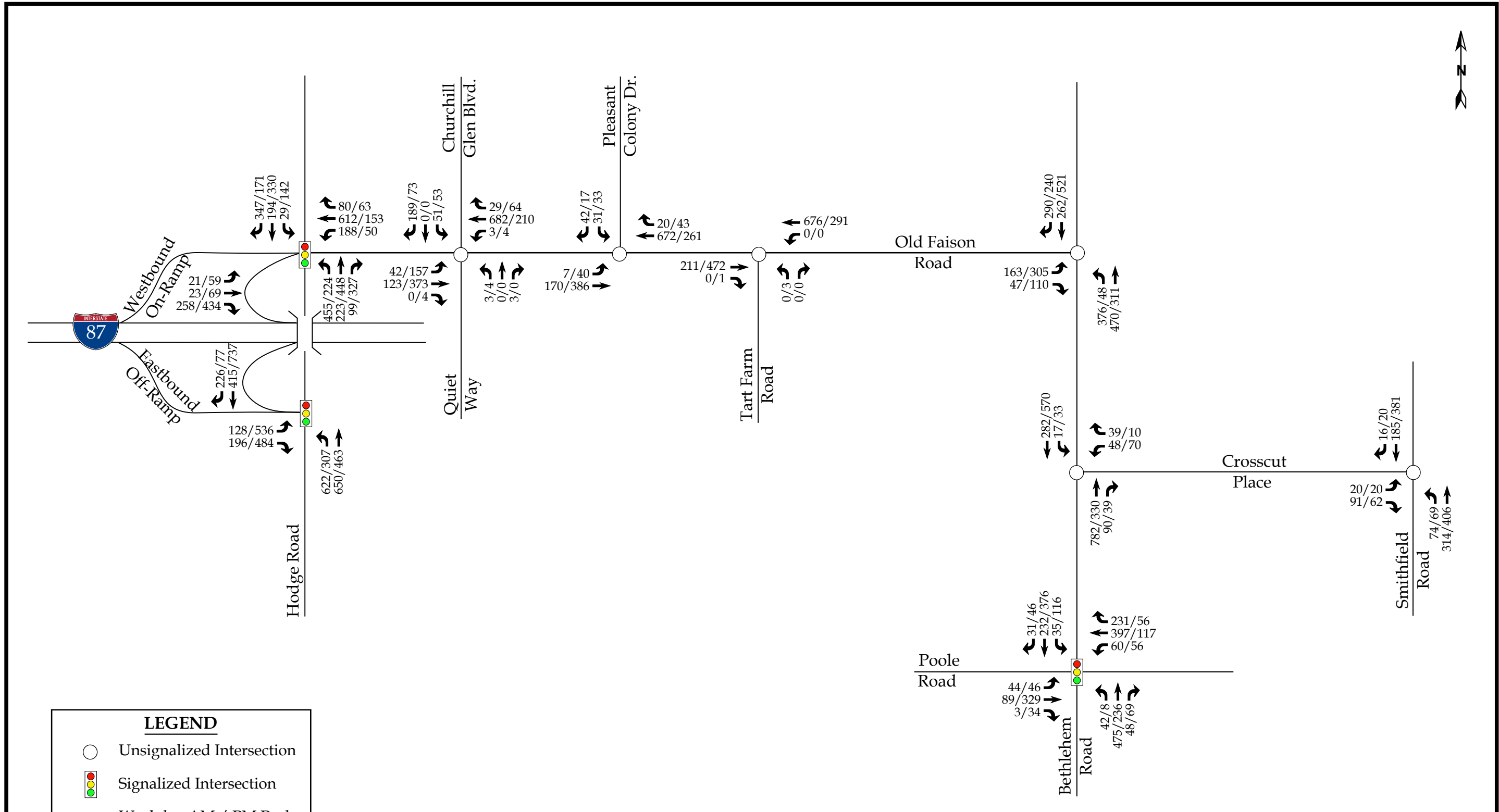
3.4. No-Build Peak Hour Traffic Volumes

The no-build traffic volumes were determined by projecting the 2023 existing peak hour traffic to the respective build year as discussed in section 3.1 and adding the adjacent

development trips. Refer to Figure 7a for an illustration of the 2032 no-build peak hour traffic volumes 7b for an illustration of the 2041 no-build peak hour traffic.

3.5. Analysis of- No-Build Peak Hour Traffic Conditions

The 2032 and 2041 no-build AM and PM peak hour traffic volumes at the study intersections were analyzed with future geometric roadway conditions and traffic control, including improvements required to be provided by the aforementioned adjacent developments, as well as STIP W-5705AK. The analysis results are presented in Section 7 of this report.

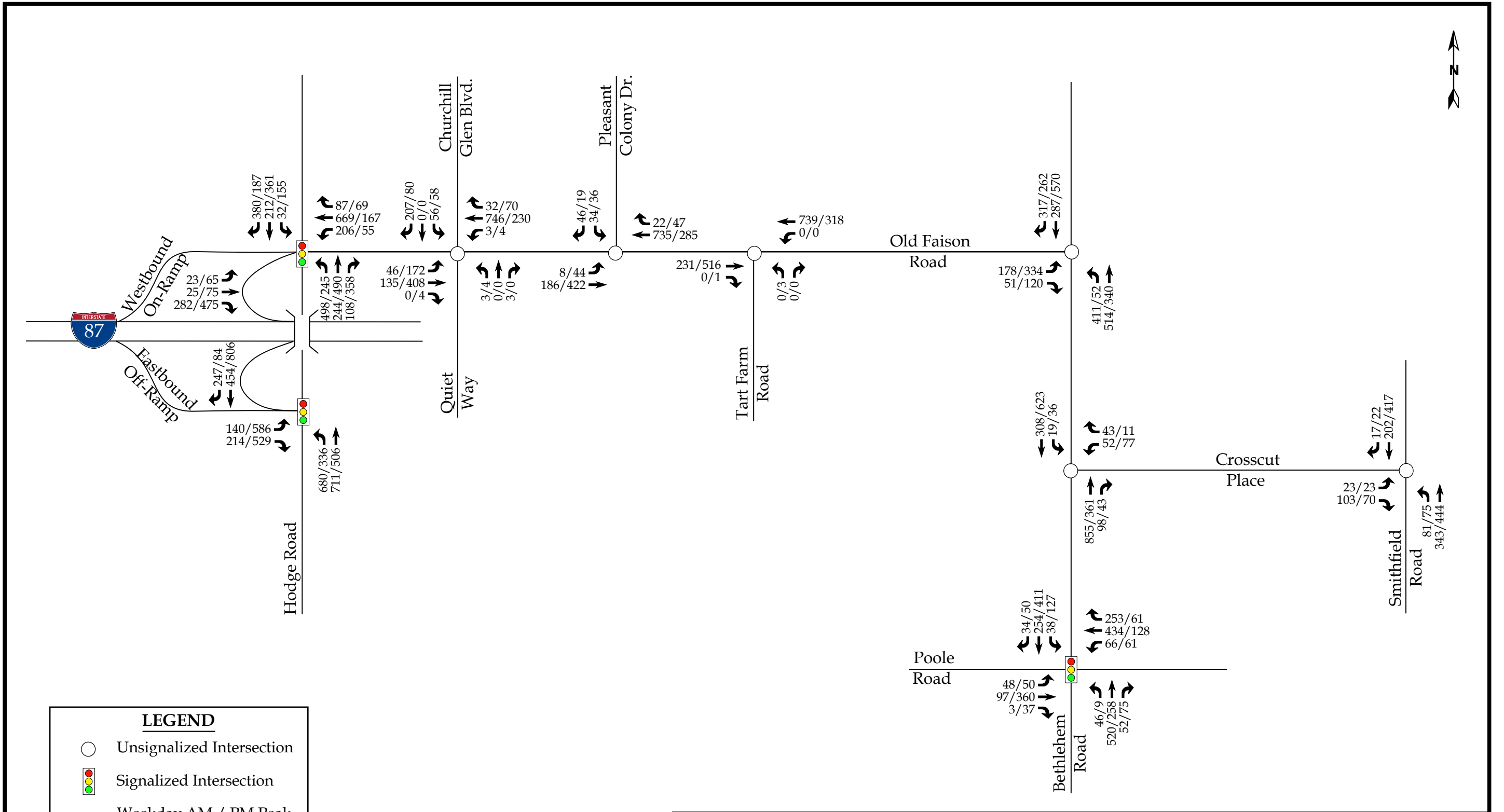


LEGEND

- Unsignalized Intersection
- 🚦 Signalized Intersection
- X / Y → Weekday AM / PM Peak Hour Traffic

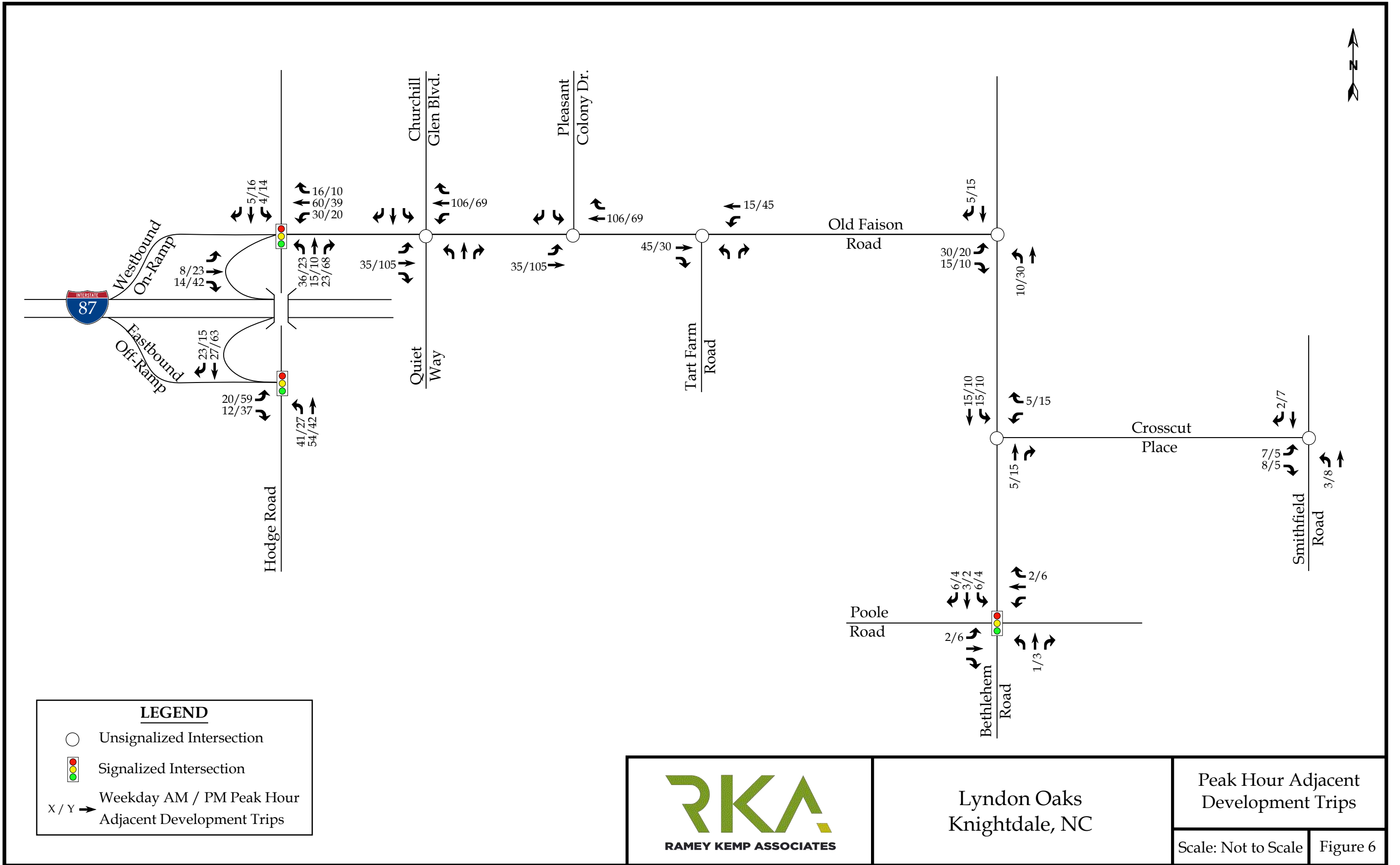
Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

	<p>Lyndon Oaks Knightdale, NC</p>	<p>2032 Projected Peak Hour Traffic</p>
	<p>Scale: Not to Scale Figure 5a</p>	



Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

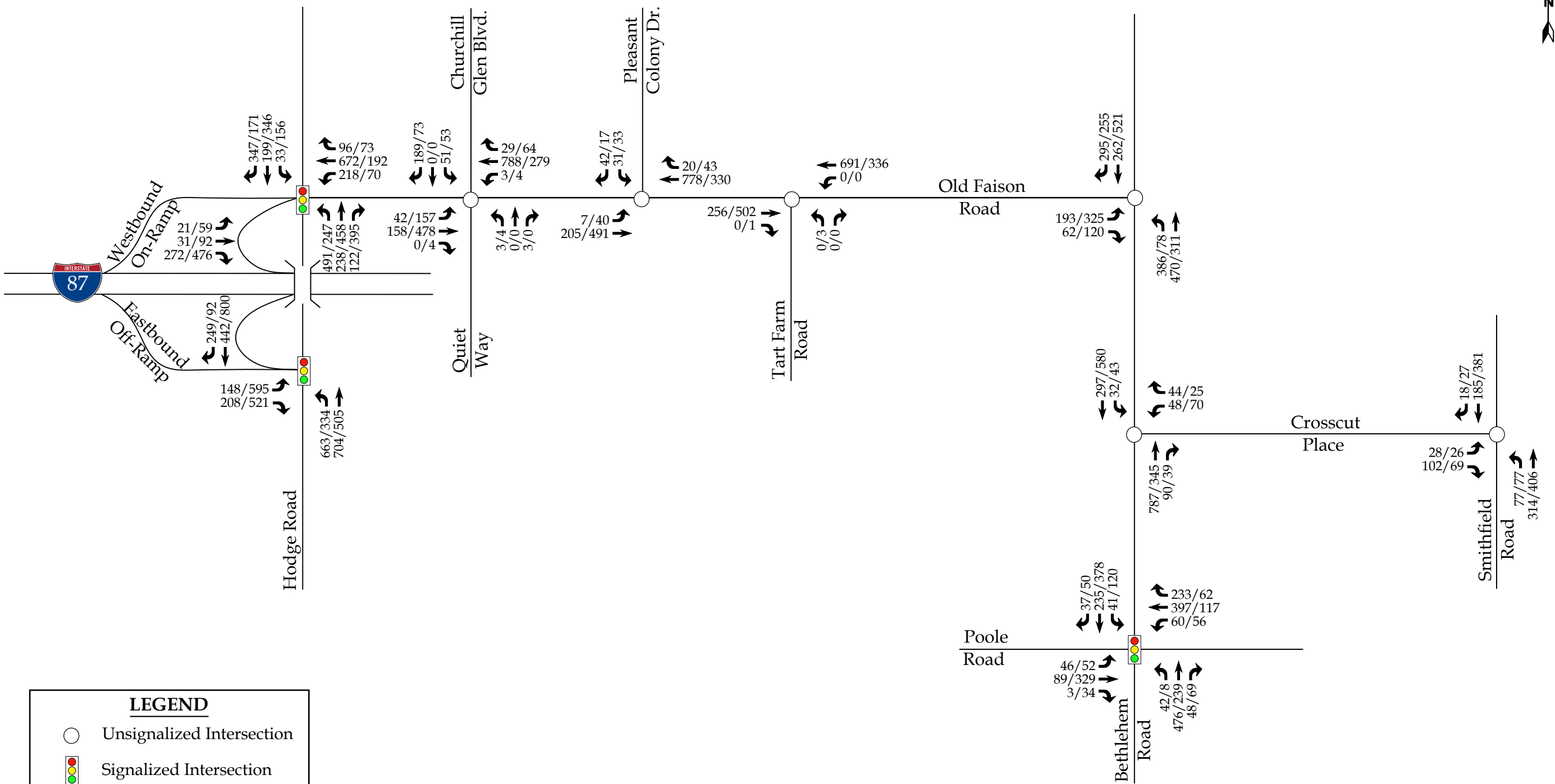
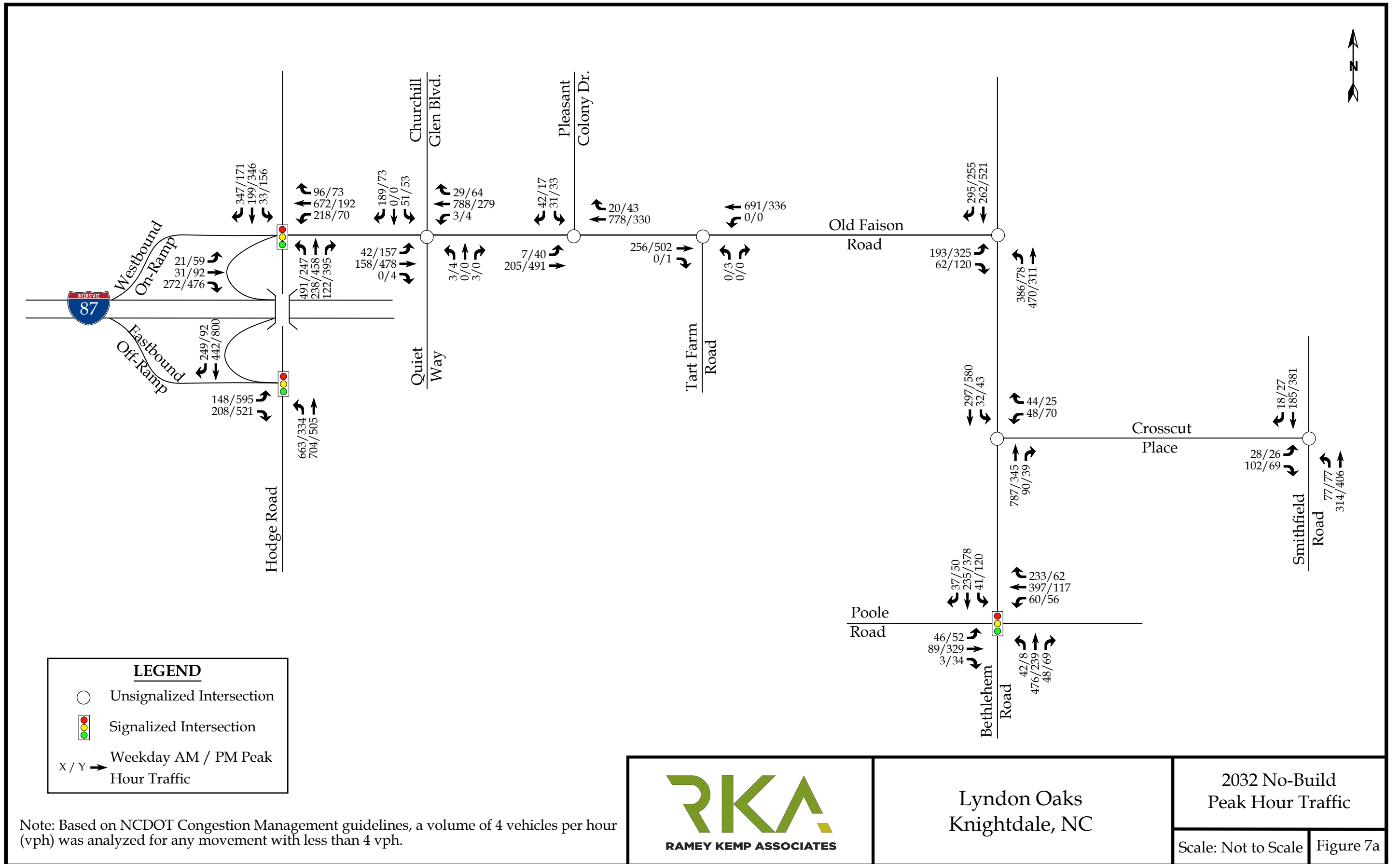
	Lyndon Oaks Knightdale, NC	2041 Projected Peak Hour Traffic	
		Scale: Not to Scale	Figure 5b

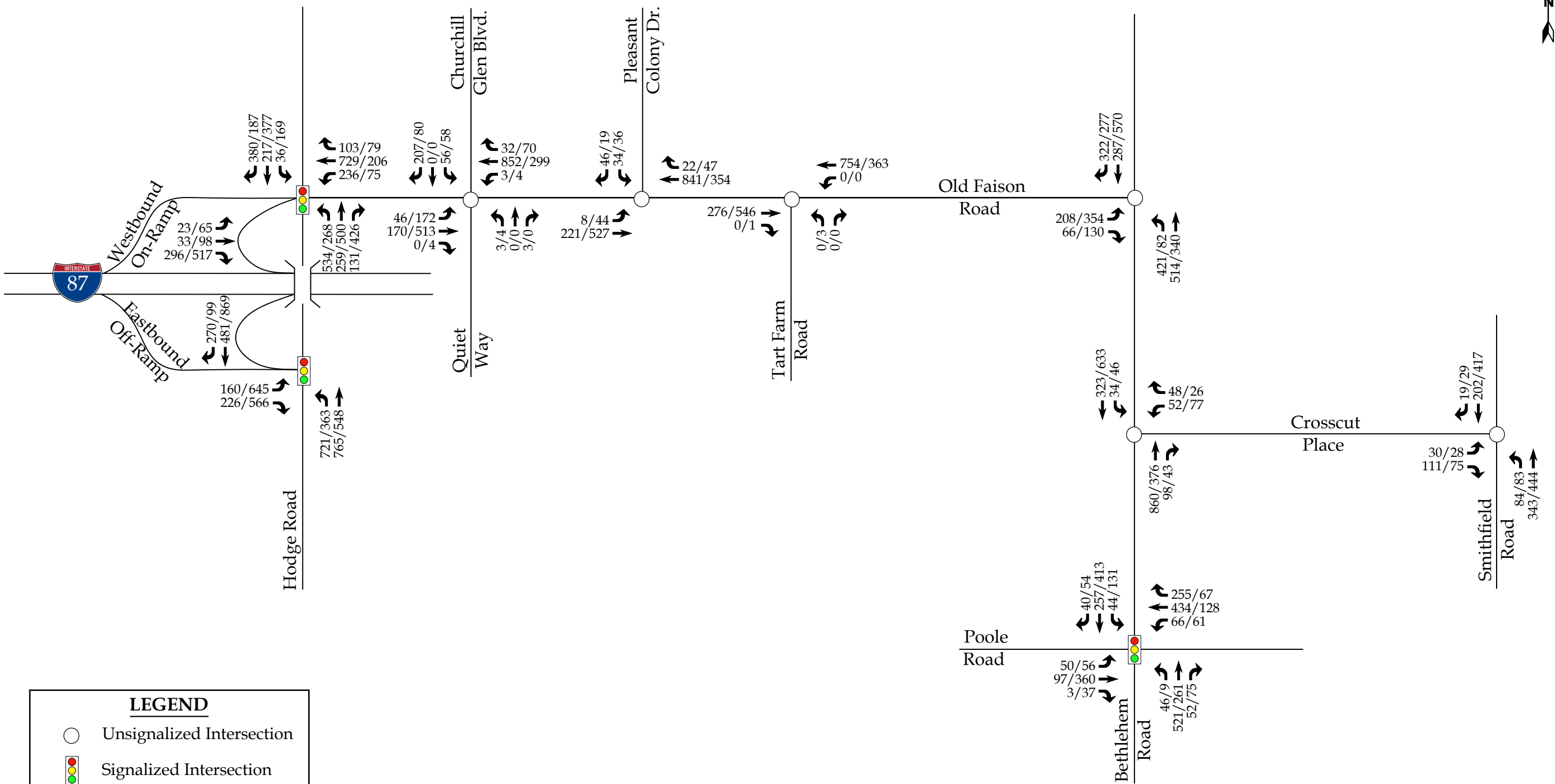
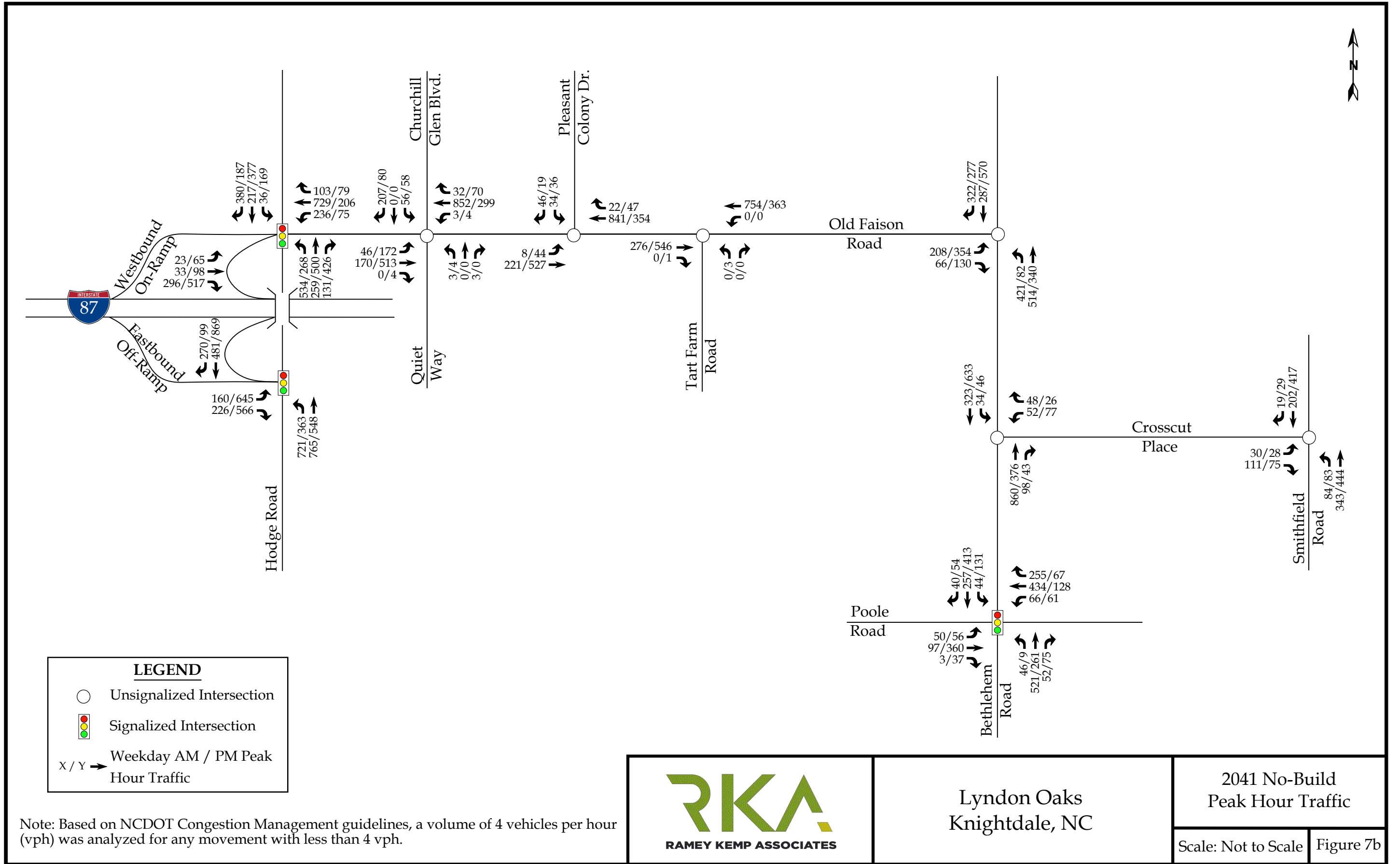


Lyndon Oaks
Knightdale, NC

Peak Hour Adjacent
Development Trips

Scale: Not to Scale Figure 6





4. SITE TRIP GENERATION AND DISTRIBUTION

4.1. Trip Generation

Average weekday daily, AM peak hour, and PM peak hour trips for the proposed development were estimated using methodology contained within the ITE *Trip Generation Manual*, 11th Edition. Table 3 provides a summary of the trip generation potential for the site.

Table 3: Trip Generation Summary

Land Use (ITE Code)	Intensity	Daily Traffic (vpd)	Weekday AM Peak Hour Trips (vph)		Weekday PM Peak Hour Trips (vph)	
			Enter	Exit	Enter	Exit
Single-Family Detached (210)	308 DU	2,840	52	155	180	106
Townhomes (215)	192 DU	1,414	24	70	66	45
Strip Retail Plaza (822)	10 KSF	652	17	12	39	39
High-Turnover Restaurant (932)	5 KSF	536	26	22	28	17
Total Trips		5,442	119	259	313	207
<i>Internal Capture (7% AM & 6% PM)*</i>			-9	-17	-20	-13
Total External Trips			110	242	293	194
<i>Pass-By Trips</i>			-0	-0	-19	-19
Total Primary (New) Trips			110	242	274	175

*Utilizing methodology contained in the NCHRP Report 684.

It is estimated that the proposed development will generate approximately 5,442 total site trips during a typical 24-hour weekday period. Of the daily traffic volume, it is anticipated that 378 trips (119 entering and 259 exiting) will occur during the weekday AM peak hour and 520 (313 entering and 207 exiting) will occur during the weekday PM peak hour. However, not all trips generated are expected to be 'new' trips. A portion of the trips are expected to be captured internally (i.e. internal capture), while some trips are expected to be pulled from traffic that is already utilizing the adjacent roadway network (i.e. pass-by).

Internal capture of trips between the residential, retail, and restaurant uses was considered in this study. Internal capture is the consideration for trips that will be made within the site between different land uses, so the vehicle technically never leaves the internal site but can still be considered as a trip to that specific land use. Based on the National Collaborative Highway Research Program (NCHRP) Report 684 methodology under full build conditions, a weekday AM peak hour internal capture rate of 7% and a weekday PM peak hour internal capture rate of 6% was applied to the total trips. The internal capture reductions are expected to account for approximately 26 trips (9 entering and 17 exiting) during the weekday AM peak hour and 33 trips (20 entering and 13 exiting) during the weekday PM peak hour. Refer to Appendix A for scoping documentation containing the NCHRP 684 sheets for each peak hour.

Pass-by trips were also taken into consideration in this study. Pass-by trips are made by the traffic already using the adjacent roadway, entering the site as an intermediate stop on their way to another destination. Pass-by percentages are applied to site trips after adjustments for internal capture. Pass-by trips are expected to account for approximately 38 trips (19 entering and 19 exiting) during the weekday PM peak hour. It should be noted that the pass-by trips were balanced, as it is likely that these trips would enter and exit in the same hour.

The total primary (new) site trips are the calculated site trips after the reduction for internal capture and pass-by trips. Primary site trips are expected to account for approximately 352 trips (110 entering and 242 exiting) during the weekday AM peak hour and 449 trips (274 entering and 175 exiting) during the weekday PM peak hour under full build conditions.

4.2. Site Trip Distribution and Assignment

Trip distribution percentages used in assigning site traffic for this development were estimated based on a combination of existing traffic patterns, population centers adjacent to the study area, adjacent development traffic studies, and engineering judgment.

It is estimated that the residential site trips will be regionally distributed as follows:

- 40% to/from the west via I-87
- 15% to/from the east via I-87
- 10% to/from the north via Hodge Road
- 10% to/from the north via Bethlehem Road
- 5% to/from the north via Smithfield Road
- 5% to/from the south via Smithfield Road
- 5% to/from the south via Hodge Road
- 4% to/from the west via Poole Road
- 4% to/from the east via Poole Road
- 2% to/from the south via Bethlehem Road

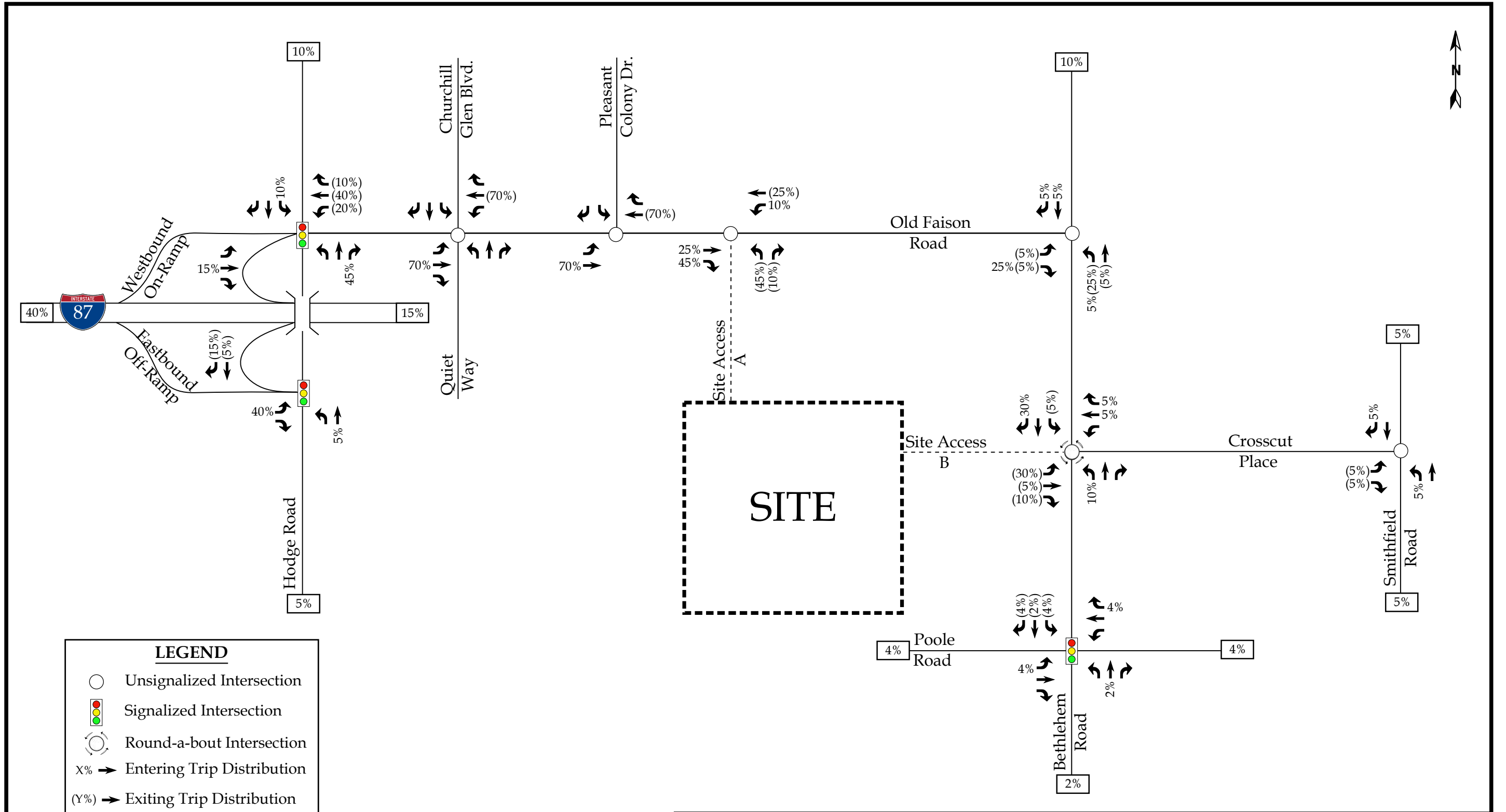
It is estimated that the commercial site trips will be regionally distributed as follows:

- 25% to/from the north via Bethlehem Road
- 20% to/from the west via I-87
- 15% to/from the east via I-87
- 10% to/from the north via Hodge Road
- 10% to/from the north via Smithfield Road
- 10% to/from the south via Smithfield Road
- 5% to/from the south via Hodge Road
- 2% to/from the west via Poole Road
- 2% to/from the east via Poole Road
- 1% to/from the south via Bethlehem Road

The residential site trip distribution is shown in Figure 8a, and the commercial site trip distribution is shown in Figure 8b. Refer to Figure 9a for the residential site trip assignment and Figure 9b for commercial site trip assignment.

The pass-by site trips were distributed based on existing traffic patterns with consideration given to the proposed driveway access and site layout. Refer to Figure 10 for the pass-by site trip distribution. Pass-by site trips are shown in Figure 11.

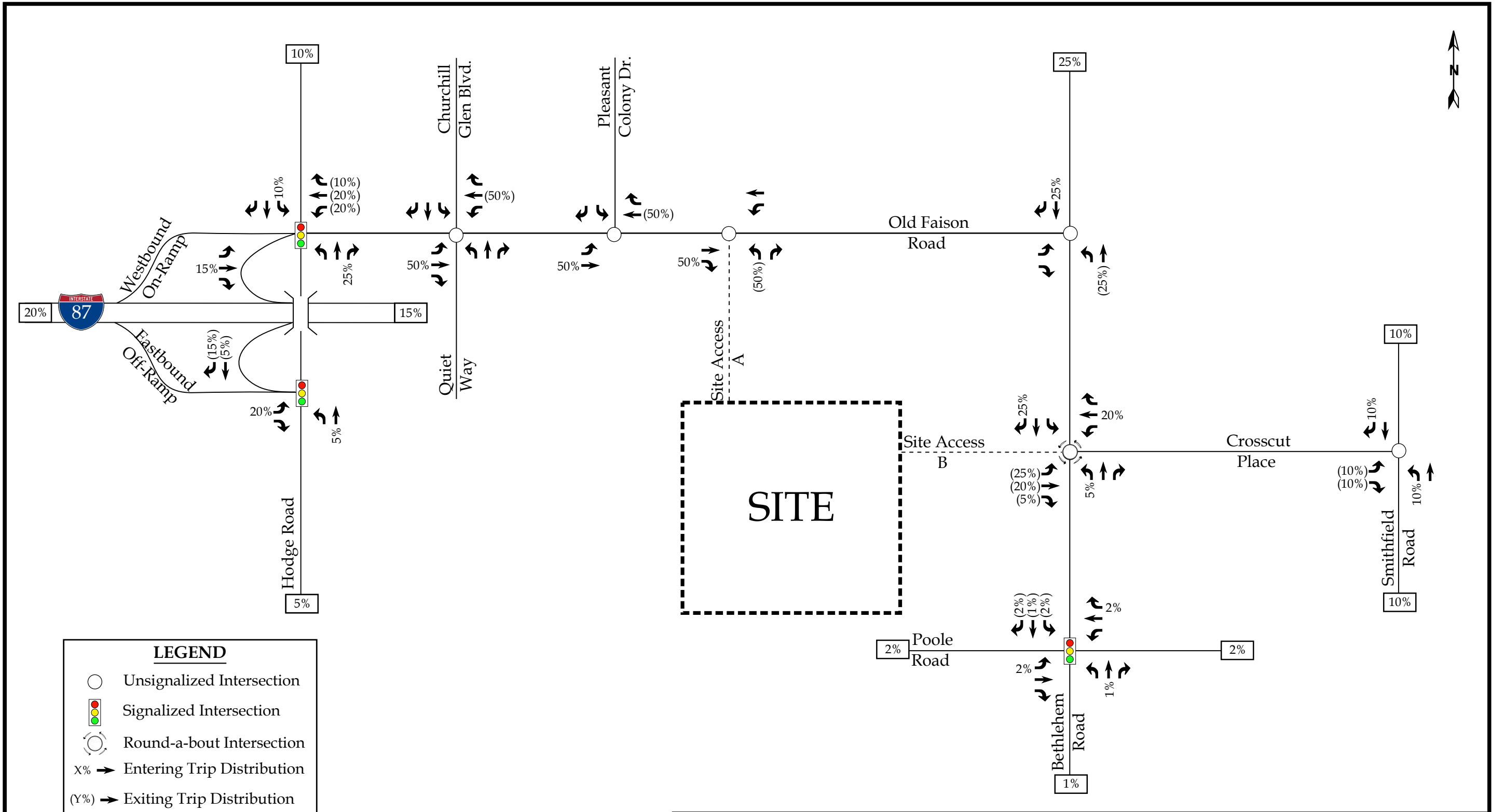
The total site trips were determined by adding the primary site trips and the pass-by site trips. Refer to Figure 12 for the total peak hour site trips at the study intersections.



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Site Trip Distribution
- Residential

Scale: Not to Scale Figure 8a



LEGEND

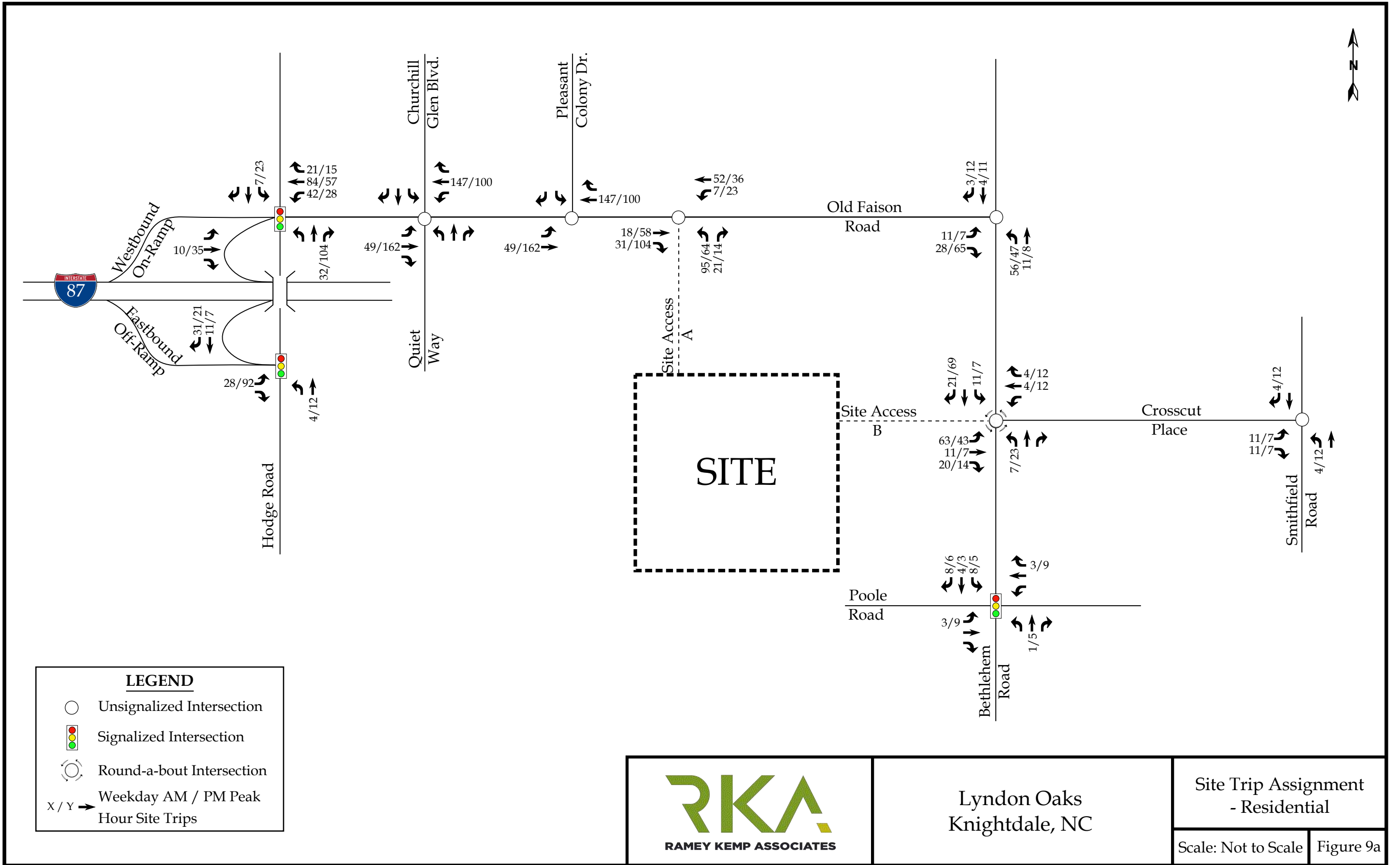
	Unsignalized Intersection
	Signalized Intersection
	Round-a-bout Intersection
X% →	Entering Trip Distribution
(Y%) →	Exiting Trip Distribution
XX%	Regional Trip Distribution

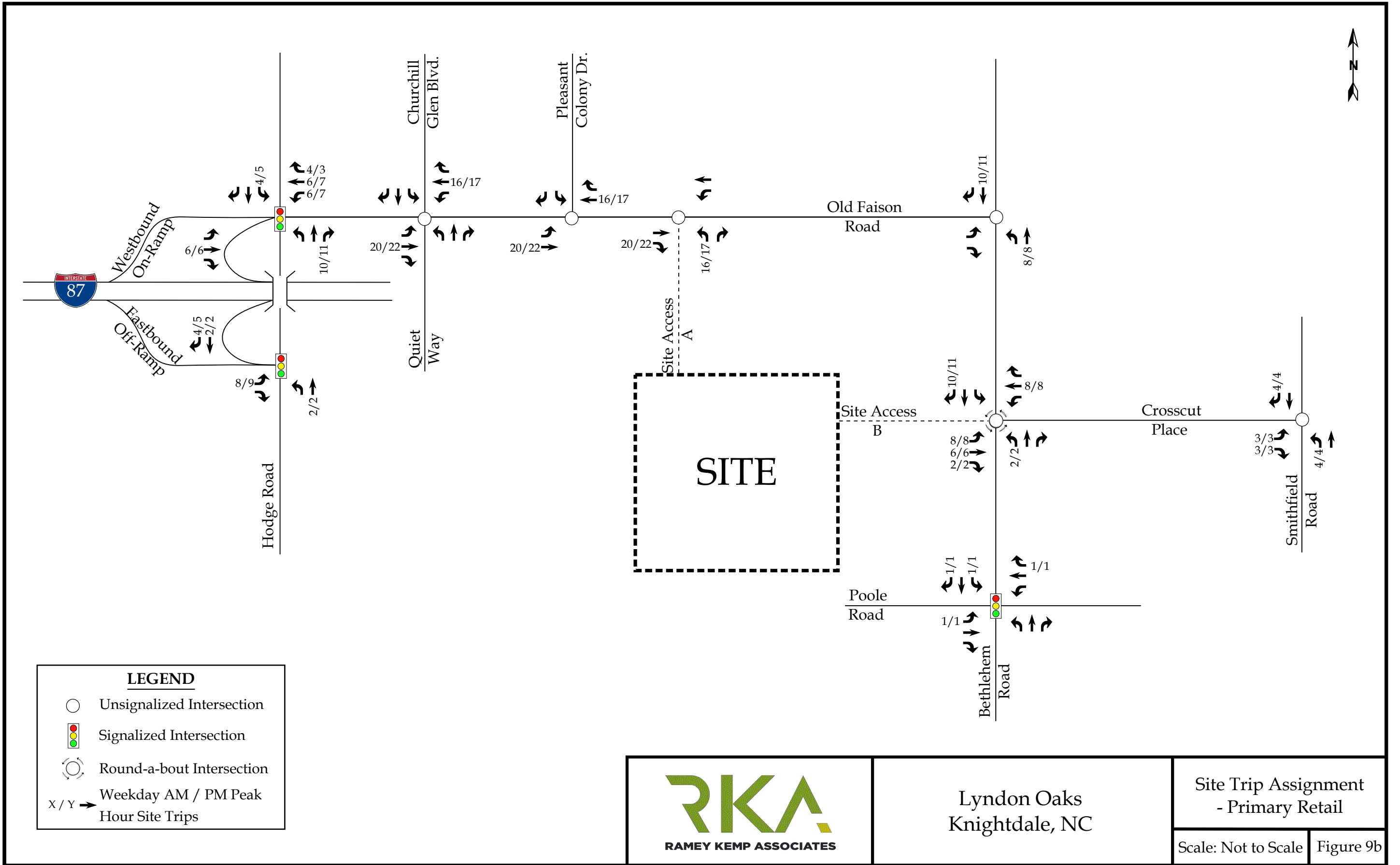


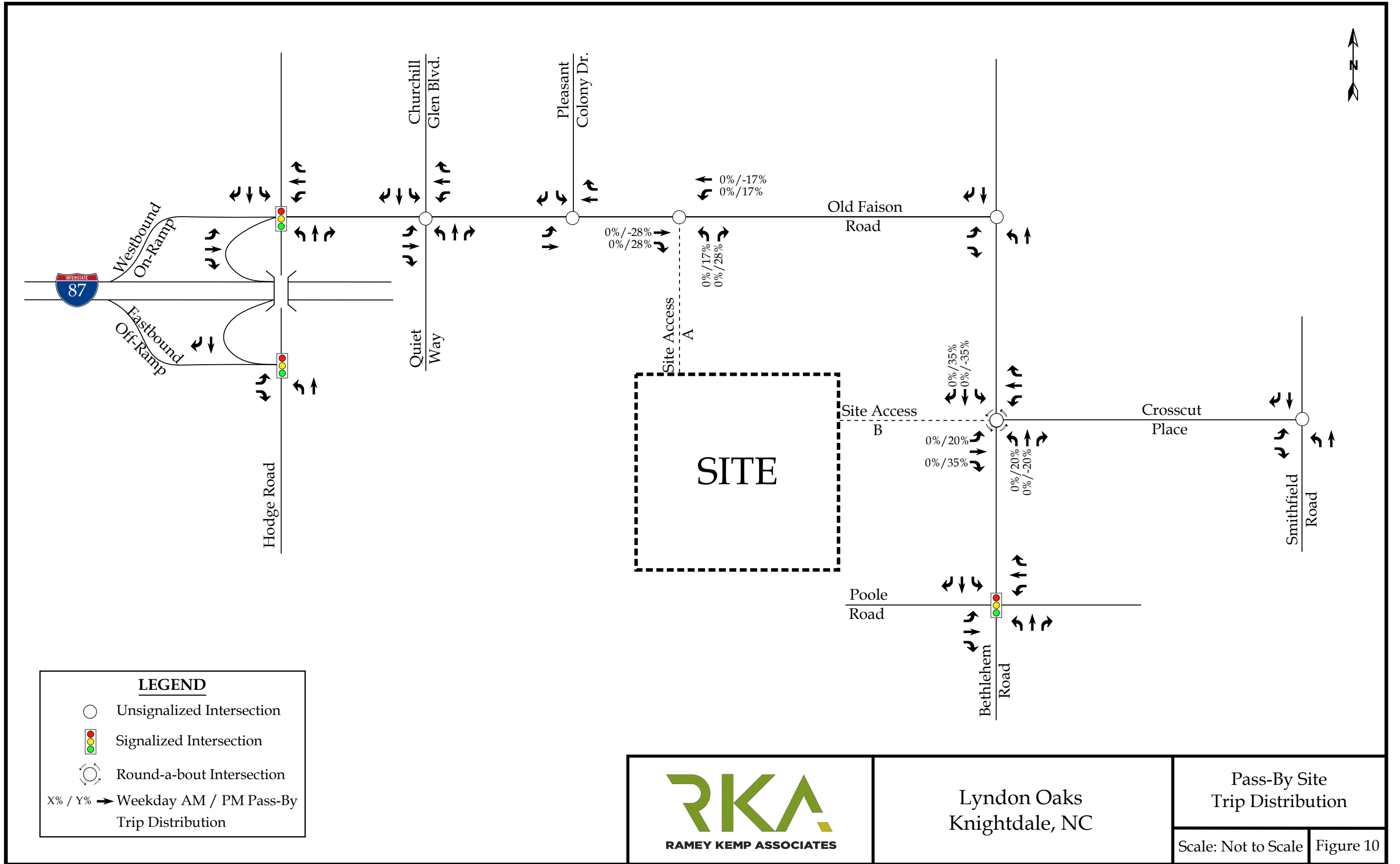
Lyndon Oaks
Knightdale, NC

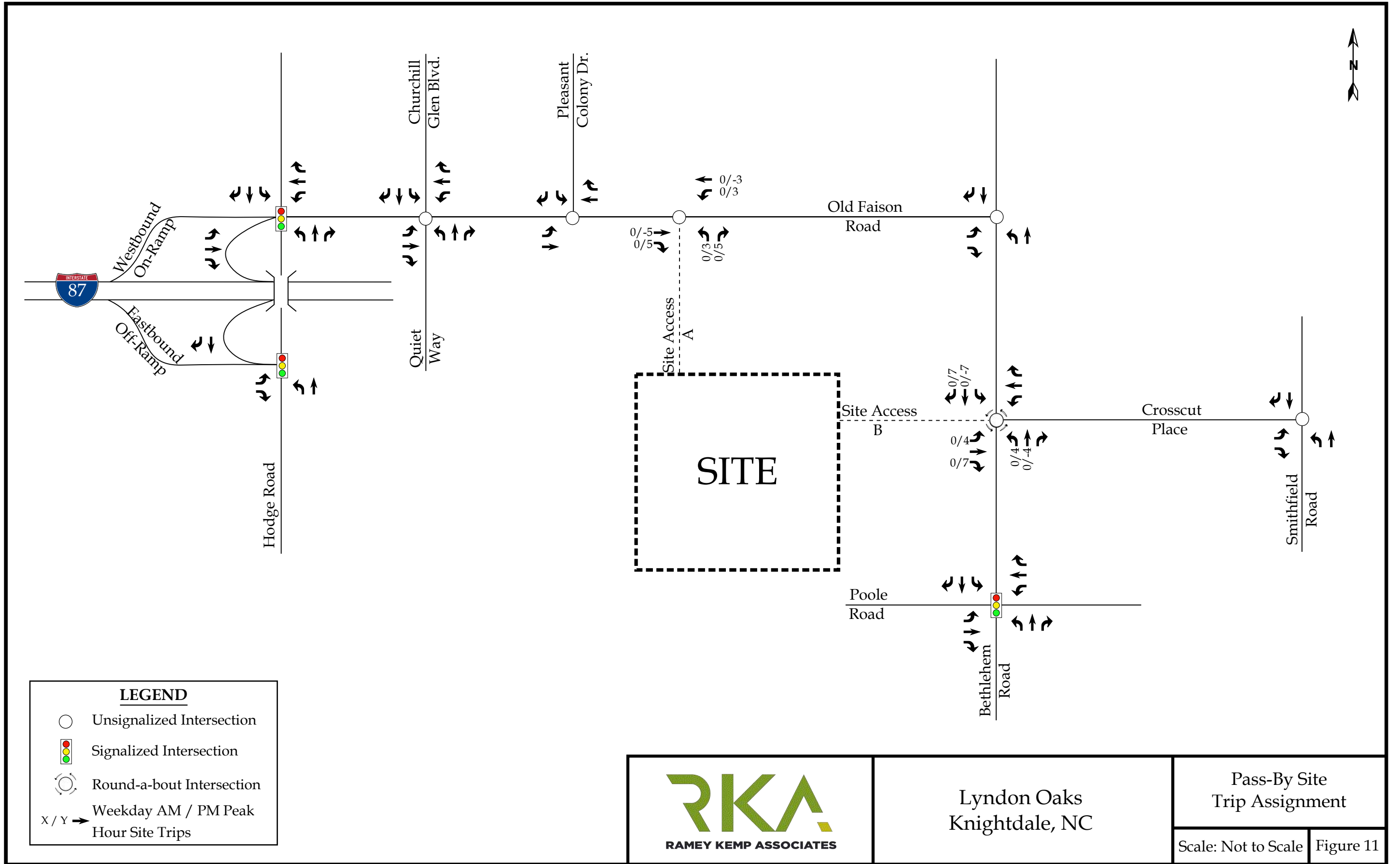
Site Trip Distribution
- Retail/Restaurant

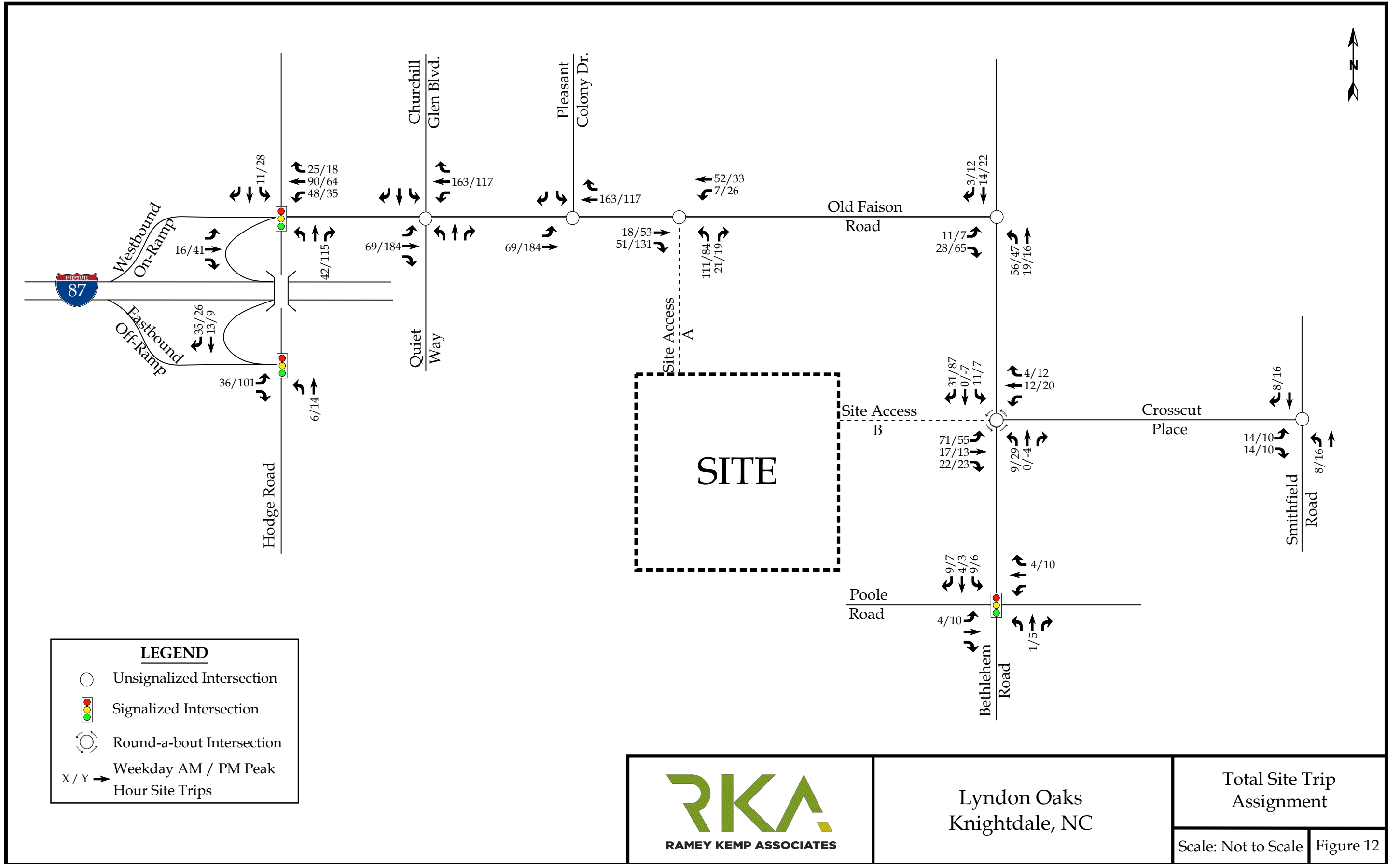
Scale: Not to Scale Figure 8b











Lyndon Oaks
Knightdale, NC

Total Site Trip
Assignment

Scale: Not to Scale Figure 12

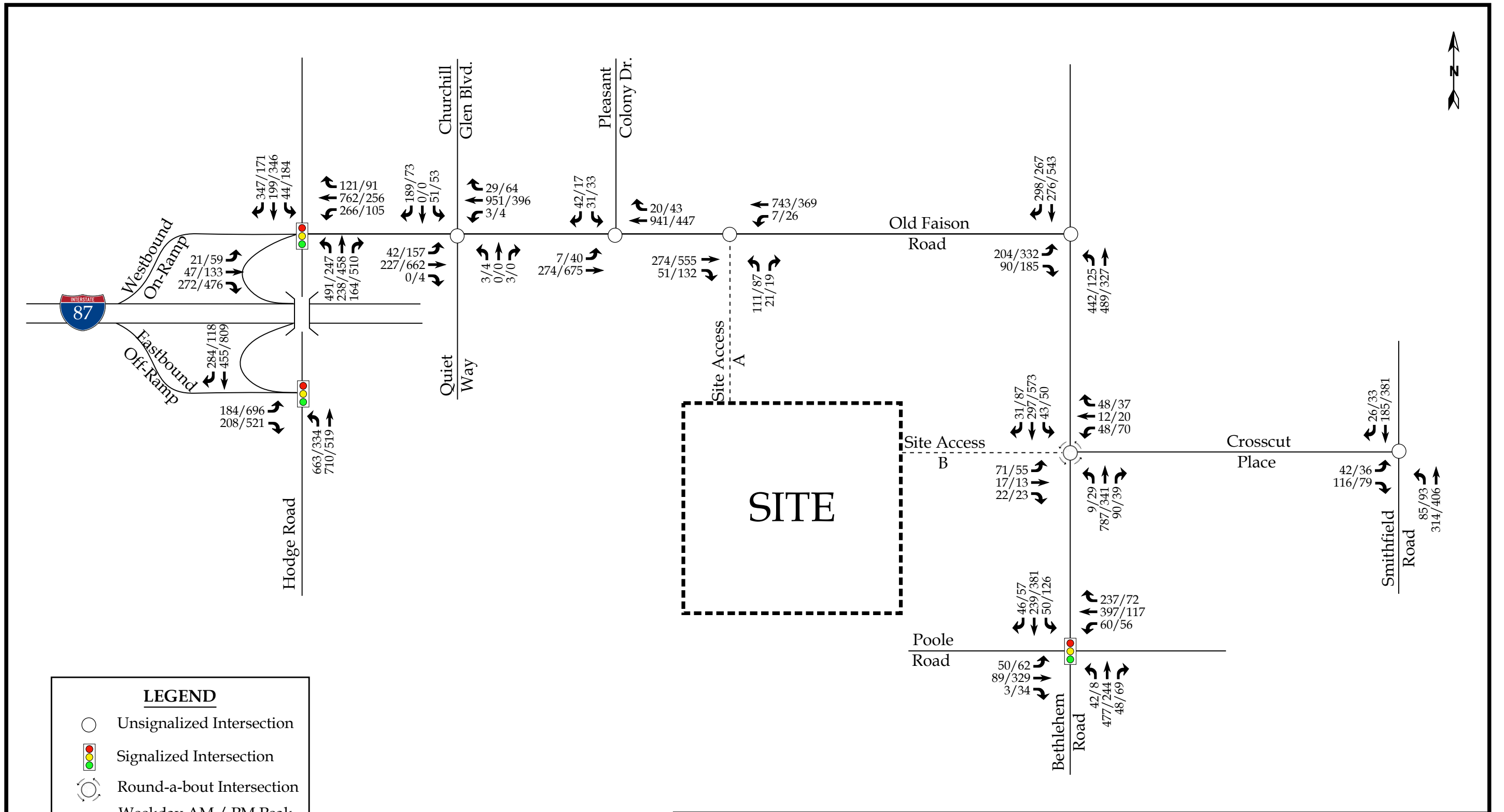
5. BUILD TRAFFIC CONDITIONS

5.1. Build Peak Hour Traffic Volumes

To estimate traffic conditions with the site fully built-out, the total site trips were added to the 2032 no-build traffic volumes and the 2041 no-build traffic volumes, respectively. Refer to Figure 13a for an illustration of the 2032 build peak hour traffic volumes with the proposed site fully developed and Figure 13b for an illustration of the 2041 build peak hour traffic volumes.

5.2. Analysis of Build Peak Hour Traffic Conditions

Study intersections were analyzed with the 2032/2041 build traffic volumes using the same methodology previously discussed for the no-build traffic conditions. Intersections were analyzed with improvements necessary to accommodate future traffic volumes. The results of the capacity analysis for each intersection are presented in Section 7 of this report.

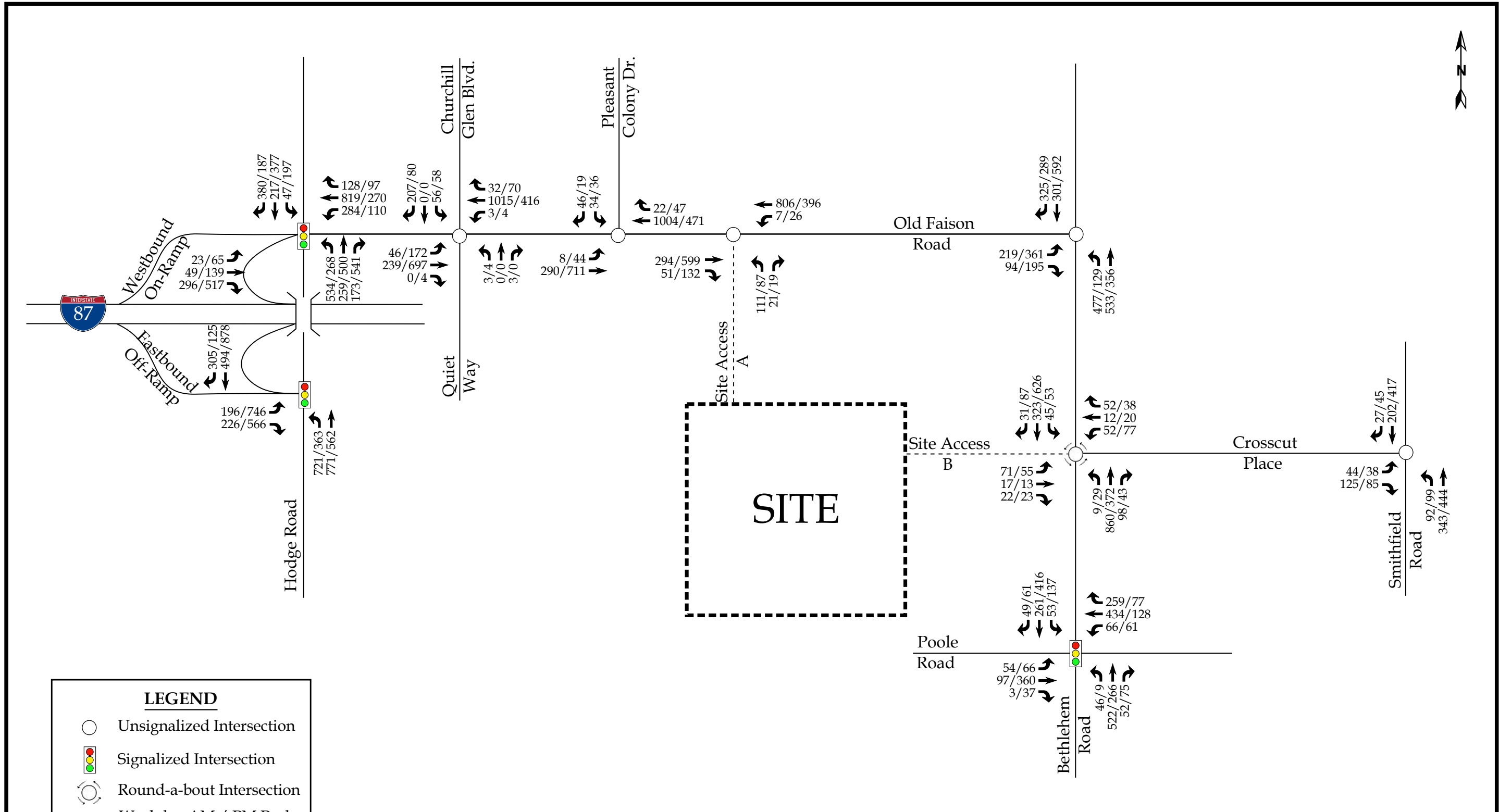


LEGEND

- Unsignalized Intersection
- ⬆️⬆️⬆️ Signalized Intersection
- ⦿ Round-a-bout Intersection
- X / Y → Weekday AM / PM Peak Hour Traffic

Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

	Lyndon Oaks Knightdale, NC	2032 Build Peak Hour Traffic
	Scale: Not to Scale Figure 13a	



LEGEND

- Unsignalized Intersection
- 🚦 Signalized Intersection
- ⦿ Round-a-bout Intersection
- X / Y → Weekday AM / PM Peak Hour Traffic

Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

	Lyndon Oaks Knightdale, NC	2041 Build Peak Hour Traffic
	Scale: Not to Scale Figure 13b	

6. TRAFFIC ANALYSIS PROCEDURE

Study intersections were analyzed using the methodology outlined in the *Highway Capacity Manual* (HCM), 6th Edition published by the Transportation Research Board. Capacity and level of service are the design criteria for this traffic study. A computer software package, Synchro (Version 10.3), was used to complete the analyses for the study area intersections. Please note that the unsignalized capacity analysis does not provide an overall level of service for an intersection; only delay for an approach with a conflicting movement. The software package SIDRA INTERSECTION 9 was utilized for all roundabout analysis. For roundabout intersections, Sidra provides LOS calculations for all approaches and an overall resulting LOS. Synchro capacity analysis results for unsignalized intersections do not provide an overall LOS, but rather a LOS for movements approaches that have a conflicting movement.

The HCM defines capacity as “the maximum hourly rate at which persons or vehicles can reasonably be expected to traverse a point or uniform section of a lane or roadway during a given time period under prevailing roadway, traffic, and control conditions.” Level of service (LOS) is a term used to represent different driving conditions, and is defined as a “qualitative measure describing operational conditions within a traffic stream, and their perception by motorists and/or passengers.” Level of service varies from Level “A” representing free flow, to Level “F” where breakdown conditions are evident. Refer to Table 4 for HCM levels of service and related average control delay per vehicle for both signalized and unsignalized intersections. Control delay as defined by the HCM includes “initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay”. An average control delay of 50 seconds at a signalized intersection results in LOS “D” operation at the intersection.

Table 4: Highway Capacity Manual – Levels-of-Service and Delay

UNSIGNALIZED/ROUNDBOUT INTERSECTION		SIGNALIZED INTERSECTION	
LEVEL OF SERVICE	AVERAGE CONTROL DELAY PER VEHICLE (SECONDS)	LEVEL OF SERVICE	AVERAGE CONTROL DELAY PER VEHICLE (SECONDS)
A	0-10	A	0-10
B	10-15	B	10-20
C	15-25	C	20-35
D	25-35	D	35-55
E	35-50	E	55-80
F	>50	F	>80

6.1. Adjustments to Analysis Guidelines

Capacity analysis at all study intersections was completed according to the NCDOT Congestion Management Guidelines.

7. CAPACITY ANALYSIS

7.1. Old Faison Road and Bethlehem Road

The existing unsignalized intersection was analyzed under all traffic conditions with the lane configurations and traffic control shown in Table 5. Refer to Table 5 for a summary of the analysis results. Refer to Appendix E for the Synchro capacity analysis reports.

Table 5: Analysis Summary of Old Faison Road and Bethlehem Road

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	F ² A ¹ --	N/A	F ² A ¹ --	N/A
2032 No-Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	F ² B ¹ --	N/A	F ² B ¹ --	N/A
2032 Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	F ² B ¹ --	N/A	F ² B ¹ --	N/A
2032 Build – Scenario A	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	A ³ E ³ C ³	D (29)	B ³ B ³ C ³	B (14)
2032 Build – Scenario B	EB NB SB	1 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	D C D	C (29)	D B C	C (29)
2041 No-Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	F ² B ¹ --	N/A	F ² B ¹ --	N/A
2041 Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	F ² B ¹ --	N/A	F ² B ¹ --	N/A
2041 Build – Scenario A	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	A ³ F ³ D ³	E (46)	E ³ B ³ C ³	C (24)
2041 Build – Scenario B	EB NB SB	1 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	D C D	C (33)	D C C	C (30)

Improvements to lane configurations are shown in bold.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.
3. Level of service for approaching a roundabout.

Capacity analysis of all traffic conditions (with the existing lane geometry and traffic control) indicates the major street left-turn movement on Bethlehem Road is expected to operate at LOS B or better during both weekday AM and PM peak hours. The minor-street approach of Old Faison Road is expected to operate at LOS F under all analysis scenarios during the AM and PM peak hours. Significant queuing is expected at the intersection on all approaches. Refer to Appendix O for SimTraffic queuing reports.

Based on information provided by the Town, the Town of Knightdale's Comprehensive Transportation Plan (CTP) calls for a roundabout at this intersection. Under Scenario A improvements, the intersection is modeled as a single-lane roundabout. Under 2032 build conditions, the roundabout is expected to operate at an overall LOS D during the AM peak hour and LOS B during the PM peak hour. All approaches are expected to operate at LOS D or better, except for the northbound approach during the AM peak hour. It should be noted that the v/c ratio of the roundabout is expected to have a vehicle to capacity ratio of 0.968 during the AM peak hour under 2032 build conditions. Under 2041 build conditions, the roundabout is expected to operate at an overall LOS E during the AM peak hour and LOS C during the PM peak hour. Increased approach delays are expected with the additional increase in traffic.

The intersection was also analyzed as a signalized intersection (under Scenario B) to compare the efficiency of the installation of signal to that of a roundabout. A peak hour signal warrant was analyzed to determine if a signal is warranted. The peak hour signal warrant is met under existing, no-build, and build conditions. Refer to Appendix O for additional signal warrant data. Under this scenario, lane configuration improvements are also necessary to accommodate queuing due to the stopping of movements on Bethlehem Road that are currently free-flow. An exclusive northbound left, southbound right, and eastbound left are considered in the analysis. Under Scenario B, the signalized intersection is expected to operate at an overall LOS C under 2032 build and 2041 build conditions.

Since the intersection operates at a lower delay and manageable queues with a signal, signalizing the intersection is recommended over the roundabout alternative. A fee-in-lieu

contribution towards improvements at this intersection is recommended by the developer as NCDOT turn lane warrants are met for both right and left turn lanes without the proposed development traffic and the intersection meets the peak hour signal warrants under existing conditions.

7.2. Old Faison Road and Tart Farm Road/Site Access A

The existing unsignalized intersection was analyzed under all traffic conditions with the lane configurations and traffic control shown in Table 6. Refer to Table 6 for a summary of the analysis results. Refer to Appendix F for the Synchro capacity analysis reports.

Table 6: Analysis Summary of Old Faison Road and Tart Farm Road/Site Access A

ANALYSIS SCENARIO	A P P R O A C H	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB WB NB	1 TH-RT 1 LT-TH 1 LT-RT	-- A ¹ B ²	N/A	-- A ¹ B ²	N/A
2032 No-Build	EB WB NB	1 TH-RT 1 LT-TH 1 LT-RT	-- A ¹ B ²	N/A	-- A ¹ B ²	N/A
2032 Build	EB WB NB	1 TH, 1 RT 1 LT-TH 1 LT, 1 RT	-- A ¹ E ²	N/A	-- A ¹ D ²	N/A
2041 No-Build	EB WB NB	1 TH-RT 1 LT-TH 1 LT-RT	-- A ¹ C ²	N/A	-- A ¹ C ²	N/A
2041 Build	EB WB NB	1 TH, 1 RT 1 LT-TH 1 LT, 1 RT	-- A ¹ E ²	N/A	-- A ¹ D ²	N/A

Improvements to lane configurations are shown in bold.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of all traffic conditions indicates the major street left-turn movement on Old Faison Road is expected to operate at LOS A during both weekday AM and PM peak hours. With a two-lane egress, the minor-street approach of existing Tart Farm Road (future Site Access A) is expected to operate at LOS D or better under all future build traffic conditions during the PM peak hour. The minor street approach is expected to operate at LOS E or better during the AM under 2032 and 2041 build conditions. No significant queuing is expected at the intersection.

Turn lane warrants on Old Faison Road were analyzed using the Policy On Street and Driveway Access to North Carolina Highways to determine if exclusive turn lanes were warranted. An eastbound right turn lane with 75 feet of storage and appropriate decel and taper is warranted at this intersection. Refer to Appendix P for the turn lane warrant charts.

No other improvements are recommended by the developer.

7.3. Old Faison Road and Pleasant Colony Drive

The existing unsignalized intersection was analyzed under all traffic conditions with the existing lane configurations and traffic control shown in Table 7. Refer to Table 7 for a summary of the analysis results. Refer to Appendix G for the Synchro capacity analysis reports.

Table 7: Analysis Summary of Old Faison Road and Pleasant Colony Drive

ANALYSIS SCENARIO	A P P R O A C H	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB WB SB	1 LT, 1 TH 1 TH-RT 1 LT-RT	A ¹ -- B ²	N/A	A ¹ -- B ²	N/A
2032 No-Build	EB WB SB	1 LT, 1 TH 1 TH-RT 1 LT-RT	A ¹ -- C ²	N/A	A ¹ -- C ²	N/A
2032 Build	EB WB SB	1 LT, 1 TH 1 TH-RT 1 LT-RT	B ¹ -- D ²	N/A	A ¹ -- D ²	N/A
2041 No-Build	EB WB SB	1 LT, 1 TH 1 TH-RT 1 LT-RT	B ¹ -- D ²	N/A	A ¹ -- C ²	N/A
2041 Build	EB WB SB	1 LT, 1 TH 1 TH-RT 1 LT-RT	B ¹ -- E ²	N/A	A ¹ -- D ²	N/A

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of all traffic conditions indicates the major street left-turn movement on Old Faison Road is expected to operate at LOS B or better during both weekday AM and PM peak hours. The minor-street approach is expected to operate at LOS D or better with the exception of the AM peak hour under 2041 build conditions.

The proposed site is only expected to contribute trips to the major-street through volumes along Old Faison Road at the intersection. Due to the minimal impacts of the proposed development on the intersection, no improvements are recommended by the developer.

7.4. Old Faison Road and Churchill Glen Boulevard/Quiet Way

The existing unsignalized intersection was analyzed under all traffic conditions with the existing lane configurations and traffic control shown in Table 8. Refer to Table 8 for a summary of the analysis results. Refer to Appendix H for the Synchro capacity analysis reports.

Table 8: Analysis Summary of Old Faison Road and Churchill Glen Boulevard/Quiet Way

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB	1 LT, 1 TH-RT	A ¹	N/A	A ¹	N/A
	WB	1 LT, 1 TH-RT	A ¹			
	NB	1 LT-TH-RT	C ²			
	SB	1 LT-TH-RT	C ²			
2032 No-Build	EB	1 LT, 1 TH-RT	B ¹	N/A	A ¹	N/A
	WB	1 LT, 1 TH-RT	A ¹			
	NB	1 LT-TH-RT	E ²			
	SB	1 LT-TH-RT	F ²			
2032 Build	EB	1 LT, 1 TH-RT	B ¹	N/A	A ¹	N/A
	WB	1 LT, 1 TH-RT	A ¹			
	NB	1 LT-TH-RT	F ²			
	SB	1 LT-TH-RT	F ²			
2041 No-Build	EB	1 LT, 1 TH-RT	B ¹	N/A	A ¹	N/A
	WB	1 LT, 1 TH-RT	A ¹			
	NB	1 LT-TH-RT	F ²			
	SB	1 LT-TH-RT	F ²			
2041 Build	EB	1 LT, 1 TH-RT	B ¹	N/A	A ¹	N/A
	WB	1 LT, 1 TH-RT	A ¹			
	NB	1 LT-TH-RT	F ²			
	SB	1 LT-TH-RT	F ²			

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of all traffic conditions indicates the major street left-turn movements on Old Faison Road are expected to operate at LOS B or better during both weekday AM and PM peak hours. The minor-street approaches are expected to experience heavier delays under all future traffic conditions, even without the proposed development. It should be noted that it is not uncommon for minor-street approaches at unsignalized intersections to experience

high delays, especially during the peak times of the day when mainline volumes are the heaviest.

The proposed site is only expected to contribute trips to the major-street through volumes at the intersection. Congestion management requires all turning movements to have a minimum of four vehicles. For this intersection, this rule was applied for several movements including the northbound and southbound through movements crossing from Churchill Glen Boulevard to Quiet Way and vice versa. This leads to inflated delays due to the difficulty of these movements (that are not currently being made).- Additionally, exclusive turn lanes are already provided on Old Faison Road at this intersection. Due to the minimal impacts of the proposed development on the intersection, no improvements are recommended by the developer.

7.5. Bethlehem Road and Crosscut Place/Site Access B

The existing unsignalized intersection was analyzed under existing and no-build traffic conditions with the existing lane configurations and traffic control shown in Table 9. With the additional proposed Site Access B as the fourth leg to the intersection, the Town is requiring the installation of a roundabout if any improvements are necessary. Therefore, the intersection was analyzed as a single lane roundabout under the build with improvements and future scenarios. Refer to Table 9 for a summary of the analysis results. Refer to Appendix I for the Synchro capacity analysis reports.

Table 9: Analysis Summary of Bethlehem Road/Crosscut Place (Site Access B)

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	WB NB SB	1 LT-RT 1 TH-RT 1 LT-TH	C ² -- A ¹	N/A	C ² -- A ¹	N/A
2032 No-Build	WB NB SB	1 LT-RT 1 TH-RT 1 LT-TH	E ² -- B ¹	N/A	D ² -- A ¹	N/A
2032 Build	EB WB NB SB	1 LT-TH-RT 1 LT-TH-RT 1 LT-TH-RT 1 LT-TH-RT	F ² F ² A ¹ B ¹	N/A	F ² F ² A ¹ A ¹	N/A
2032 Build Roundabout	EB WB NB SB	1 LT-TH-RT 1 LT-TH-RT 1 LT-TH-RT 1 LT-TH-RT	A ³ B ³ C ³ A ³	C (16)	A ³ A ³ A ³ B ³	A (10)
2041 No-Build	WB NB SB	1 LT-RT 1 TH-RT 1 LT-TH	F ² -- B ¹	N/A	E ² -- A ¹	N/A
2041 Build Roundabout	EB WB NB SB	1 LT-TH-RT 1 LT-TH-RT 1 LT-TH-RT 1 LT-TH-RT	A ³ B ³ D ³ A ³	C (22)	A ³ A ³ A ³ B ³	B (11)

Improvements to lane configurations and traffic control are shown in bold.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.
3. Level of service for approaching the proposed roundabout improvement.

Capacity analysis of existing and no-build conditions indicates the major street left-turn movement on Bethlehem Road is expected to operate at LOS B or better during both weekday AM and PM peak hours. Under 2032 build conditions, with the addition of the proposed Site Access B as the fourth leg to the intersection, and with no improvements the minor street approaches are expected to operate at LOS F during the AM and PM peak hours.

As mentioned previously, through coordination with NCDOT and the Town, a roundabout intersection is required to be constructed with the addition of proposed Site Access B. Under build conditions with the roundabout, the intersection is expected to operate at an overall LOS C or better during the AM and PM peak hours.

Other than the installation of a single lane roundabout, no other improvements are recommended by the developer.

7.6. I-87 Eastbound Ramps and Hodge Road

The existing signalized intersection was analyzed under all traffic conditions with the existing lane configurations and traffic control shown in Table 10. Refer to Table 10 for a summary of the analysis results. Refer to Appendix J for the Synchro capacity analysis reports.

Table 10: Analysis Summary of I-87 Eastbound Ramps and Hodge Road

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB NB SB	2 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	A A B	A (10)	B B C	B (17)
2032 No-Build	EB NB SB	2 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	C C C	C (27)	D D E	D (49)
2032 Build	EB NB SB	2 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	C C C	C (29)	E D E	E (57)
2032 Build – <i>Field Conditions</i>	EB NB SB	2 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	C B C	C (23)	D D D	D (50)
2041 No-Build	EB NB SB	2 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	C C D	C (32)	E D E	E (62)
2041 Build	EB NB SB	2 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	D C D	C (34)	E E F	E (72)
2041 Build – <i>Field Conditions</i>	EB NB SB	2 LT, 1 RT 1 LT, 1 TH 1 TH, 1 RT	C C D	C (28)	E E E	E (64)

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of all future traffic conditions indicates the signalized intersection is expected to operate at an overall LOS C during the weekday AM peak hour and LOS E during

and the weekday PM peak hour. Significant queuing is expected at the intersection under all future traffic conditions.

Congestion Management capacity analysis guidelines require all permitted/protected phases to be changed to protected only which results in a higher delay at the study intersection. An alternative analysis showing how the intersection operates in the field was performed to better simulate how the intersection functions. The protected phase was changed back to permitted/protected and signal splits were optimized for the intersection. Right-turn-on-red (RTOR) was also permitted in the field conditions analysis.

Due to the proposed site's minimal impacts on the intersection, no improvements are recommended.

7.7. I-87 Westbound Ramps/Old Faison Road and Hodge Road

The existing signalized intersection was analyzed under all traffic conditions with lane configurations and traffic control shown in Table 11. Refer to Table 11 for a summary of the analysis results. Refer to Appendix K for the Synchro capacity analysis reports.

Table 11: Analysis Summary of I-87 Westbound Ramps/Old Faison Road and Hodge Road

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB	1 LT-TH, 1 RT	A	C (29)	B	B (19)
	WB	1 LT-TH-RT	D			
	NB	1 LT, 1 TH-RT	C			
	SB	1 LT, 1 TH, 1 RT	B			
2032 No-Build	EB	1 LT-TH, 1 RT	B	E (56)	D	D (40)
	WB	<u>1 LT, 1 TH, 1 RT</u>	E			
	NB	1 LT, 1 TH-RT	E			
	SB	1 LT, 1 TH, 1 RT	B			
2032 Build	EB	1 LT-TH, 1 RT	B	E (71)	D	E (67)
	WB	<u>1 LT, 1 TH, 1 RT</u>	F			
	NB	1 LT, 1 TH-RT	E			
	SB	1 LT, 1 TH, 1 RT	C			
2032 Build – Field Conditions	EB	1 LT-TH, 1 RT	A	D (47)	C	D (47)
	WB	<u>1 LT, 1 TH, 1 RT</u>	E			
	NB	1 LT, 1 TH-RT	D			
	SB	1 LT, 1 TH, 1 RT	B			
2041 No-Build	EB	1 LT-TH, 1 RT	B	E (76)	C	E (71)
	WB	<u>1 LT, 1 TH, 1 RT</u>	F			
	NB	1 LT, 1 TH-RT	F			
	SB	1 LT, 1 TH, 1 RT	B			
2041 Build	EB	1 LT-TH, 1 RT	B	F (91)	D	F (112)
	WB	<u>1 LT, 1 TH, 1 RT</u>	F			
	NB	1 LT, 1 TH-RT	F			
	SB	1 LT, 1 TH, 1 RT	C			
2041 Build – Field Conditions	EB	1 LT-TH, 1 RT	B	E (60)	C	E (61)
	WB	<u>1 LT, 1 TH, 1 RT</u>	F			
	NB	1 LT, 1 TH-RT	D			
	SB	1 LT, 1 TH, 1 RT	B			

Lane modifications by STIP W-5705AK are underlined.

Even with the improvements associated with NCDOT STIP W-5705AK, capacity analysis of all future traffic conditions indicates that the intersection is expected to operate at an overall E or better, with the exception of the 2041 build conditions. Significant queuing is expected under all future scenarios.

As mentioned previously, Congestion Management capacity analysis guidelines, require all permitted/protected movements at future intersections to be changed to protected phasing only. This would change some of the signal improvements installed by the STIP. An alternative analysis was performed to include the improvements by the STIP and more accurately model field conditions. RTOR was also permitted under the field conditions analysis. Signal timings were optimized for the purposes of this analysis.

Under the field conditions scenario, the intersection is expected to operate at LOS D or better under 2032 build conditions. To mitigate queue lengths exceeding storage lengths, turn lane extensions are recommended by the developer based on 95th percentile queuing. The westbound left-turn lane should be extended to contain 175 feet of storage with appropriate decel and taper. The eastbound left-through lane should be extended to contain 275 feet of storage. The southbound left turn lane should be extended to contain 275 feet of storage.

7.8. Smithfield Road and Old Ferrell Road

The existing unsignalized intersection was analyzed under all traffic conditions with existing lane configurations and traffic control shown in Table 12. Refer to Table 12 for a summary of the analysis results. Refer to Appendix L for the Synchro capacity analysis reports.

Table 12: Analysis Summary of Smithfield Road and Old Ferrell Road

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	B ² A ¹ --	N/A	B ² A ¹ --	N/A
2032 No-Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	B ² A ¹ --	N/A	C ² A ¹ --	N/A
2032 Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	B ² A ¹ --	N/A	C ² A ¹ --	N/A
2041 No-Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	B ² A ¹ --	N/A	C ² A ¹ --	N/A
2041 Build	EB NB SB	1 LT-RT 1 LT-TH 1 TH-RT	B ² A ¹ --	N/A	C ² A ¹ --	N/A

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of all traffic conditions indicates that the major street left turn movement on Smithfield Road is expected to operate at LOS A during the AM and PM peak hour. The minor street approach is expected to operate at LOS C or better during the AM and PM peak hour under all conditions. No significant queuing is expected.

No improvements are recommended by the developer.

7.9. Bethlehem Road and Poole Road

The existing signalized intersection was analyzed under all traffic conditions with the existing lane configurations and traffic control shown in Table 13. Refer to Table 13 for a summary of the analysis results. Refer to Appendix M for the Synchro capacity analysis reports.

Table 13: Analysis Summary of Bethlehem Road and Poole Road

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2023 Existing	EB	1 LT-TH-RT	A	D (36)	A	C (34)
	WB	1 LT-TH-RT	B		A	
	NB	1 LT-TH-RT	E		B	
	SB	1 LT-TH-RT	C		E	
2032 No-Build	EB	1 LT-TH-RT	A	F (152)	B	F (104)
	WB	1 LT-TH-RT	B		A	
	NB	1 LT-TH-RT	F		C	
	SB	1 LT-TH-RT	F		F	
2032 Build	EB	1 LT-TH-RT	A	F (168)	B	F (115)
	WB	1 LT-TH-RT	B		A	
	NB	1 LT-TH-RT	F		C	
	SB	1 LT-TH-RT	F		F	
2041 No-Build	EB	1 LT-TH-RT	A	F (226)	B	F (142)
	WB	1 LT-TH-RT	B		A	
	NB	1 LT-TH-RT	F		C	
	SB	1 LT-TH-RT	F		F	
2041 Build	EB	1 LT-TH-RT	A	F (246)	B	F (156)
	WB	1 LT-TH-RT	B		A	
	NB	1 LT-TH-RT	F		C	
	SB	1 LT-TH-RT	F		F	

Capacity analysis of all future traffic conditions indicates that the intersection is expected to operate at LOS F during the AM and PM peak hour. Significant queuing is expected under all future traffic conditions.

The proposed development is expected to account for less than 2 percent of the total traffic at the intersection during the AM peak hour and less than 3 percent during the PM peak hour. Therefore, due to the proposed site's minimal impacts on the intersection, no improvements are recommended by the developer.

8. CONCLUSIONS

The contents of this report present the findings of the Traffic Impact Analysis (TIA) conducted for the proposed Lyndon Oaks subdivision development to be located south of Old Faison Road and to the west of Bethlehem Road in Knightdale, North Carolina. The proposed development is proposed to consist of 308 single-family homes, 192 townhomes, and 15,000 square feet of commercial space and be built out in 2031. Site access is proposed via one (1) full-movement driveway at the existing location of Old Faison Road and Tart Farm Road and one roundabout at the existing intersection of Bethlehem Road and Crosscut Place.

The study analyzes traffic conditions during the weekday AM and PM peak hours for the following scenarios:

- 2023 Existing Traffic Conditions
- 2031+1 No-Build Traffic Conditions
- 2031+1 Build Traffic Conditions
- 2031+10 No-Build Traffic Conditions
- 2031+10 Build Traffic Conditions

Trip Generation

It is estimated that the proposed development will generate approximately 5,442 total site trips during a typical 24-hour weekday period. Of the daily traffic volume, it is anticipated that 378 trips (119 entering and 259 exiting) will occur during the weekday AM peak hour and 520 trips (313 entering and 207 exiting) will occur during the weekday PM peak hour. Not all trips are expected to occur as new trips as some are expected to be captured internally to the site, while others are expected to be drawn from traffic that is already on the adjacent roadway network.

Adjustments to Analysis Guidelines

Capacity analysis at all study intersections was completed according to NCDOT Congestion Management Guidelines. Refer to section 6.1 of this report for a detailed description of any adjustments to these guidelines made throughout the analysis.

9. RECOMMENDATIONS

Based on the findings of this study, specific geometric improvements have been identified and are recommended to accommodate future traffic conditions. See a more detailed description of the recommended improvements below. Refer to Figure 14 for an illustration of the recommended lane configuration for the proposed development.

Recommended Improvements by STIP W-5705AK

Westbound I-87 Ramps and Hodge Road

- Construct westbound right turn lane with 100 feet of storage and appropriate decel and taper.
- Construct westbound left turn lane with 100 feet of storage and appropriate decel and taper.
- Modify signal timings and signal phasing

Recommended Improvements by Developer

Old Faison Road and Bethlehem Road

- Coordinate with Town on desired improvements. Provide fee in lieu contribution as necessary

Old Faison Road and Tart Farm Road/Site Access A

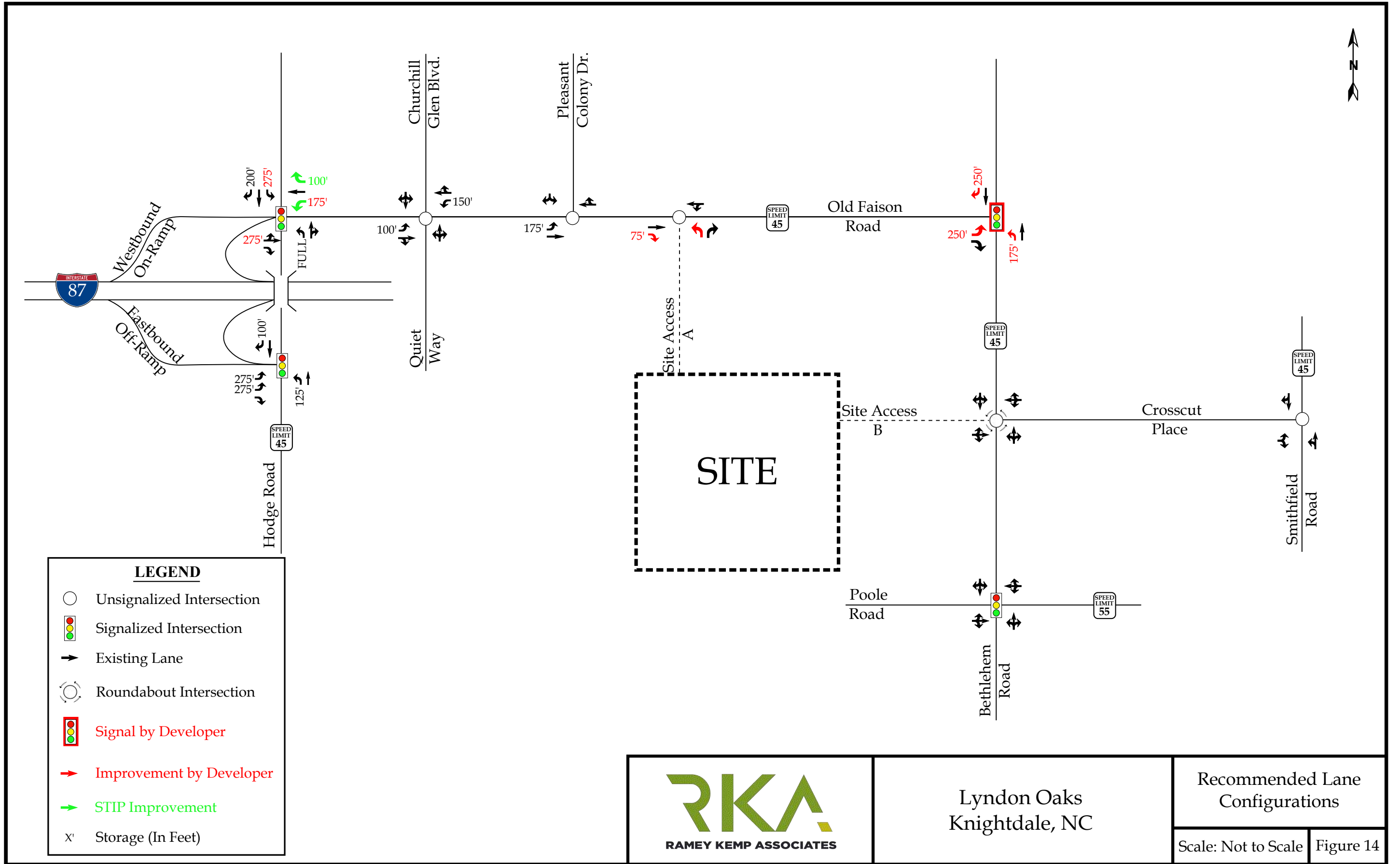
- Construct an eastbound right turn lane with 75 feet of storage and appropriate decel and taper.
- Construct exclusive northbound left turn lane with full storage.
- Provide stop control for northbound approach.

Bethlehem Road and Crosscut Place/Site Access B

- Construct a single-lane roundabout.
- Construct eastbound approach with one ingress and one egress lane.
- Provide yield control for eastbound approach.

Westbound I-87 Ramps and Hodge Road

- Extend westbound left turn lane to contain 175 feet of storage and appropriate decel and taper.
- Extend southbound left turn lane to contain 275 feet of storage and appropriate decel and taper.
- Extend eastbound left-through lane to contain 275 feet of storage and appropriate decel and taper.



Lyndon Oaks
Knightdale, NC

Recommended Lane
Configurations

Scale: Not to Scale Figure 14

