



**Knightdale Town Council
Meeting Agenda**

March 20, 2025, 7:00 p.m.

950 Steeple Square Court, Knightdale, North Carolina 27545

1. WELCOME

2. ADOPTION OF AGENDA

3. JOINT PUBLIC HEARINGS

If you anticipate speaking at tonight's joint public hearing, print your name and address on the form at the entry table. You must speak from the podium to assure an accurate record of testimony. Before speaking, please state your name and address for the record. Speaker comments are limited to three (3) minutes and large groups are asked to designate a spokesperson. Written public comments will be accepted up to 24 hours prior to the meeting and can be submitted online ([Knightdale Public Comment Form](#)).

a. ZMA-16-22 Robertson Crossing Amendment

DS Director

b. ZMA-4-23 Terravita

Senior Planner

c. ZMA-1-24 Old Faison Place

Senior Planner

4. ADJOURNMENT



Town of Knightdale

Staff Report Cover Sheet

Title: ZMA-1-24 – Old Faison Place Planned Unit Development

Staff: Gideon Smith, Senior Planner – Current

Date: March 20, 2025

PURPOSE

- The purpose of this staff report is to provide an overview of a Zoning Map Amendment for the proposed Old Faison Place Planned Unit Development.

STRATEGIC PLAN PRIORITY AREA(S)

- Connected & Inclusive

GENERAL STATUTE REFERENCE(S), *if applicable*

- N.C.G.S. 160D-605

TYPE OF PUBLIC HEARING, *if applicable*

- Legislative

FUNDING SOURCE(S), *if applicable*

- N/A

ATTACHMENT(S)

- Staff Report
- Application, Master Plan, & PUD Document
- Neighborhood Meeting Information
- Transportation Impact Analysis

STAFF RECOMMENDATION

- Hold a joint public hearing;
- Following public comment, close the public hearing; and
- Refer case ZMA-1-24 to the April 14, 2025, Land Use Review Board for review and recommendation.

Knightdale Strategic Priorities



Safe



Connected &
Inclusive



Sustainable



Active & Healthy



Organizational
Excellence



Town of Knightdale

Staff Report

Title: Old Faison Place Planned Unit Development (ZMA-1-24)

Staff: Gideon Smith, Senior Planner

Date: March 20, 2025

I. REQUEST:

Rivers & Associates, on behalf of Ashbury & the Holdings Company, LLC has submitted a petition requesting a Zoning Map Amendment to rezone approximately ± 20.90 acres, consisting of two tracts of land located on the north side of Old Faison Road (Wake County PINs 1743-46-3116 & 1743-46-6606), from General Residential – Medium Density (GR8) District to General Residential – Medium Density Planned Unit Development (GR8 PUD) to allow for the construction of approximately 117 residential units and to reserve approximately ± 0.78 -acres for future commercial uses.

II. PROJECT PROFILE:

| | |
|---------------------------|---|
| PROPERTY LOCATION: | 0 Buffaloe Road (North Side of Old Faison Road) |
| WAKE COUNTY PINs: | 1743-46-3116 & 1743-46-6606 |
| CURRENT ZONING DISTRICT: | General Residential - Medium Density (GR8) |
| PROPOSED ZONING DISTRICT: | General Residential - Medium Density Planned Unit Development (GR8 PUD) |
| DENSITY PERMITTED: | 8 units/acre |
| NAME OF PROJECT: | Old Faison Place |
| APPLICANT: | Stephen Ballentine, Rivers & Associates |
| PROPERTY OWNERS: | Ashbury & the Holdings Company, LLC |
| DEVELOPER: | Glenwood Homes |
| PROPERTY SIZE: | 20.90 acres |
| CURRENT LAND USE: | Vacant |
| PROPOSED LAND USE: | 117 Residential Units (12 Single-Family; 105 Townhomes); ± 0.78 -acre outparcel reservation for future commercial uses |
| PROPOSED DENSITY: | 5.6 units/acre |

III. BACKGROUND INFORMATION:

The Planned Unit Development District (PUD) is a rezoning process which is designed to encourage master planning of development and to coordinate such development to manage the impacts of the development on the provision of Town Services and infrastructure. The PUD encourages creativity and innovation in the design of developments, but in return for this flexibility the expectation is for communities to provide exceptional design, character, and quality; provide high quality community amenities; incorporate creative design in the layout of buildings; ensure compatibility with surrounding land uses and neighborhood character; encourage the creation of mixed density neighborhoods, neighborhood nodes, and mixed use centers; further the goals of the KnightdaleNext 2035 V.2 2035 Comprehensive Plan, including the Urban Small Town Framework and Future Place Type Maps; and provide greater efficiency in the layout and provision of roads, utilities, and other infrastructure.

There are several provisions which are required to be addressed by the Applicant in the PUD, including, but not limited to design guidelines, proposed alternative means of compliance, public facilities, recreational open space, Comprehensive Plan consistency, among others. The Applicant's specific exceptions are detailed in **Section VII** of this Staff Report.



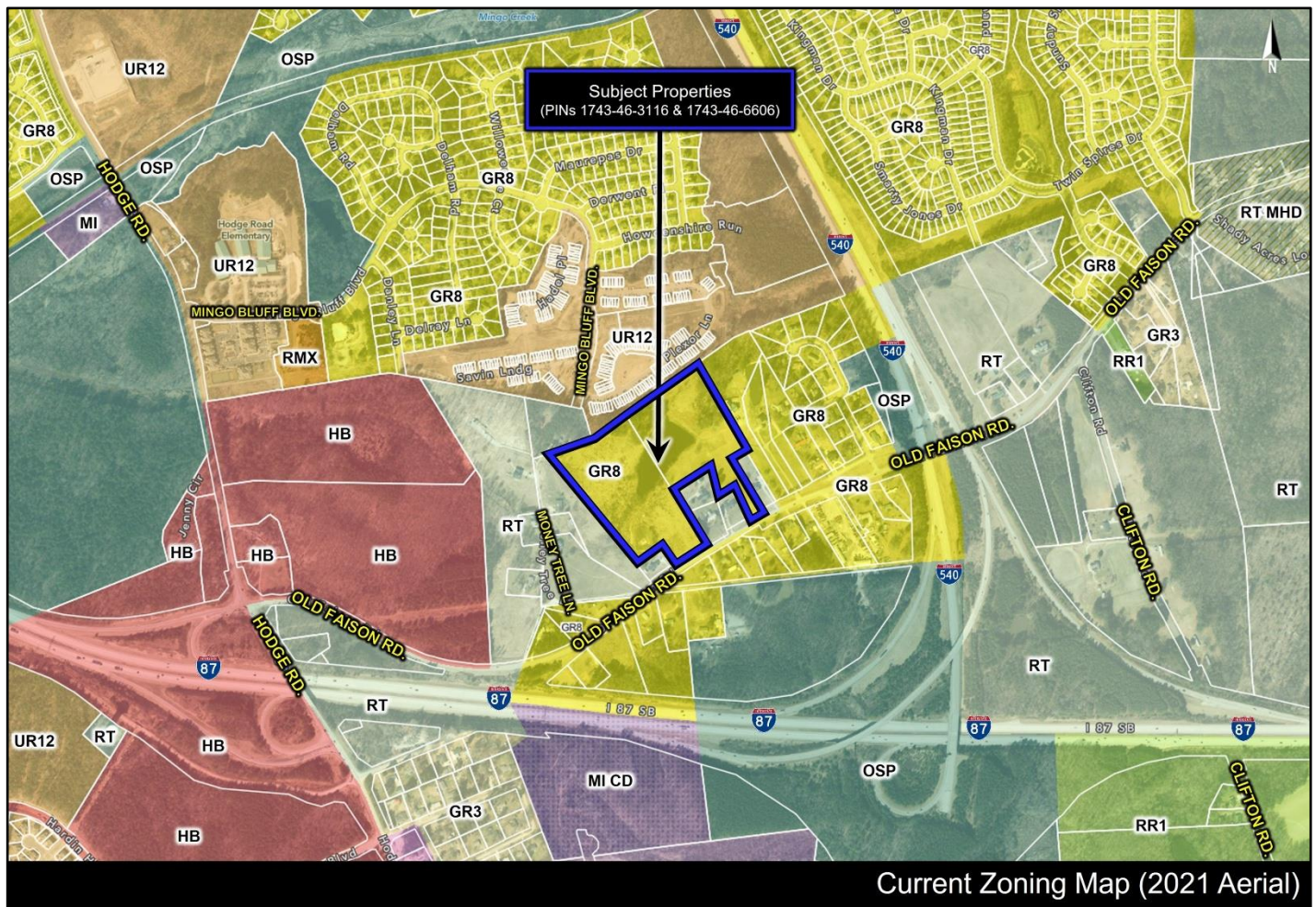
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IV. PROJECT SETTING – SURROUNDING ZONING DISTRICTS AND LAND USES:

The proposed rezoning features two parcels located on the northern side of Old Faison Road, about midway between I-540 to the east and Hodge Road to the west. These parcels are located within the Town's Extra Territorial Jurisdiction and, if approved, would require annexation into Corporate Limits.

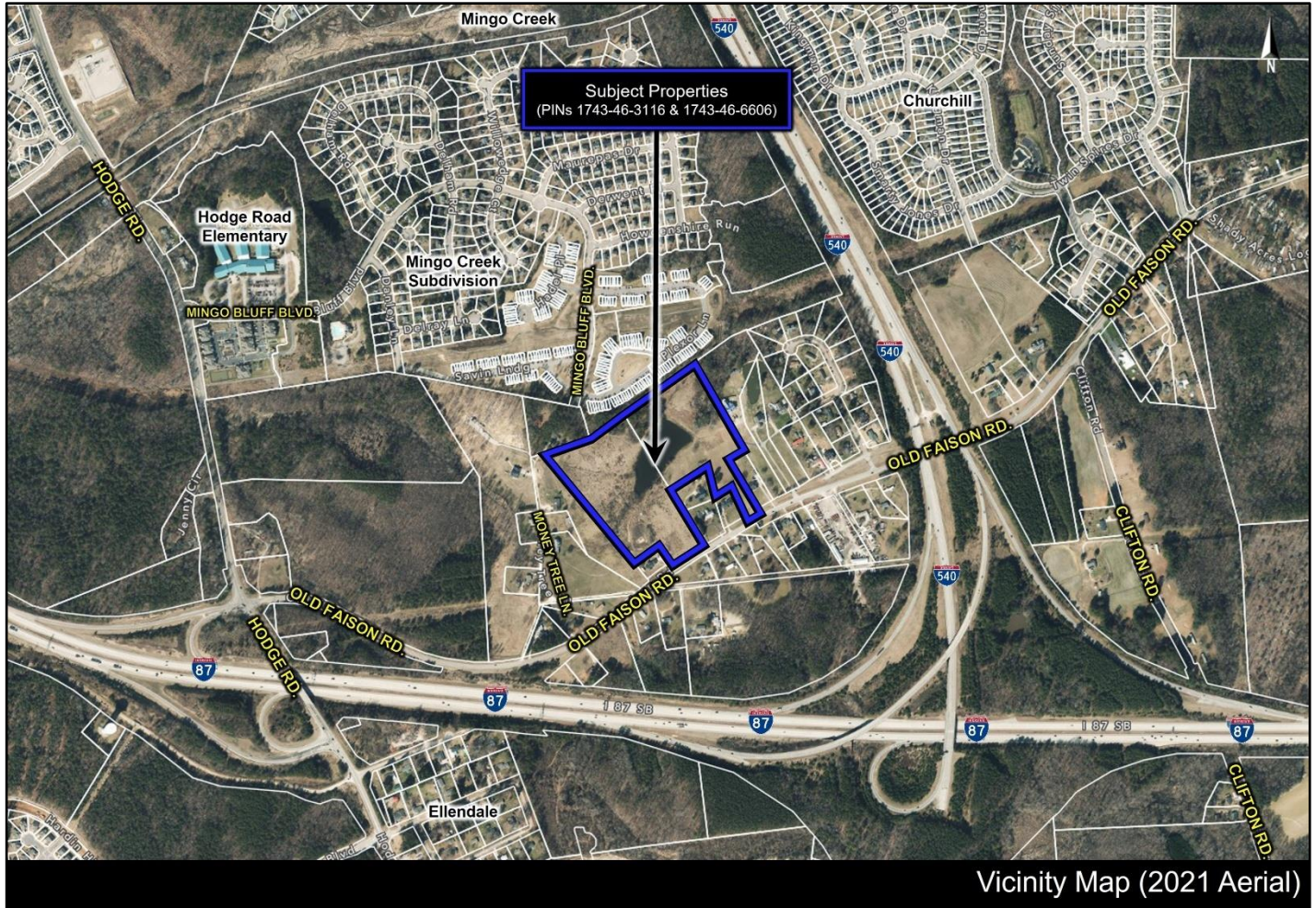
| DIRECTION | LAND USE | ZONING |
|-----------|---|----------|
| North | Residential (Mingo Creek Subdivision & Other Single-Family Dwellings) | UR12; RT |
| South | Residential (Single-Family Dwellings) | RT; GR8 |
| East | Residential (Single-Family Dwellings) | GR8 |
| West | Residential (Single-Family Dwellings) | RT |





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V. PROPOSED MASTER PLAN:

The Applicant has submitted a full Master Plan in accordance with Section 12.3.F of the UDO. The Applicant is proposing a residential development consisting of 117 lots that features 12 detached single-family homes and 105 attached townhomes with the active recreational open space amenities dedicated to the Town as a public park (~1.67 acres). In addition, the Applicant proposes to reserve an approximately 0.78-acre commercial outparcel along Old Faison Road for an undefined use or tenant.

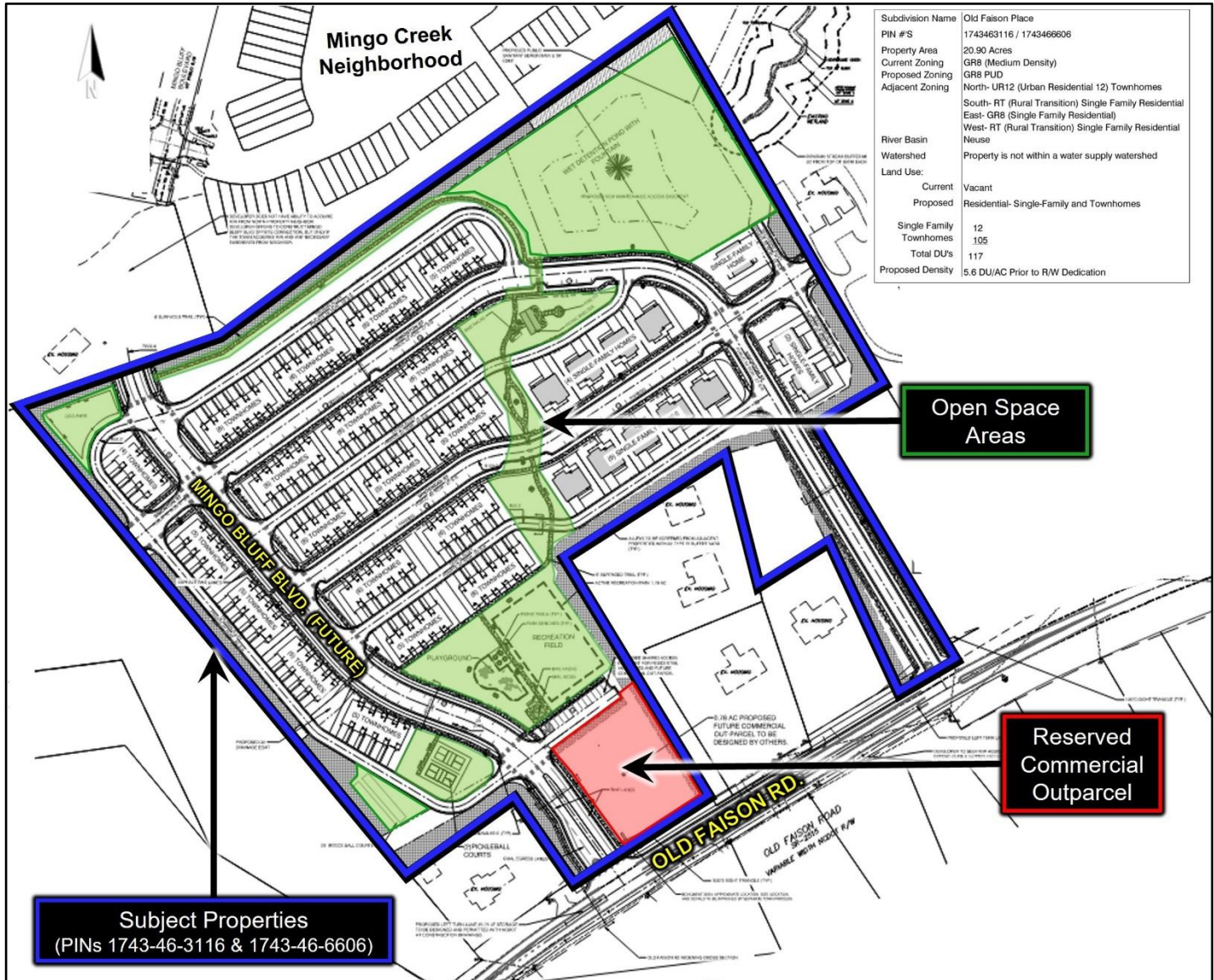
Additional information regarding the permitted uses, dimensional standards, design standards, zoning conditions, etc. is detailed in **Section VII** of this Staff Report. The developer and staff met multiple times early in and throughout the process as the plan evolved. Further analysis of the Master Plan is detailed below.



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Old Faison Place Proposed Master Plan



STAFF SITE PLAN ANALYSIS:

Staff have reviewed the plan for UDO consistency and found the Master Plan is generally compliant with the required provisions. However, further modifications to the Master Plan are required as noted herein. The following items are being highlighted for Council review and further consideration.

Landscaping & Tree Preservation

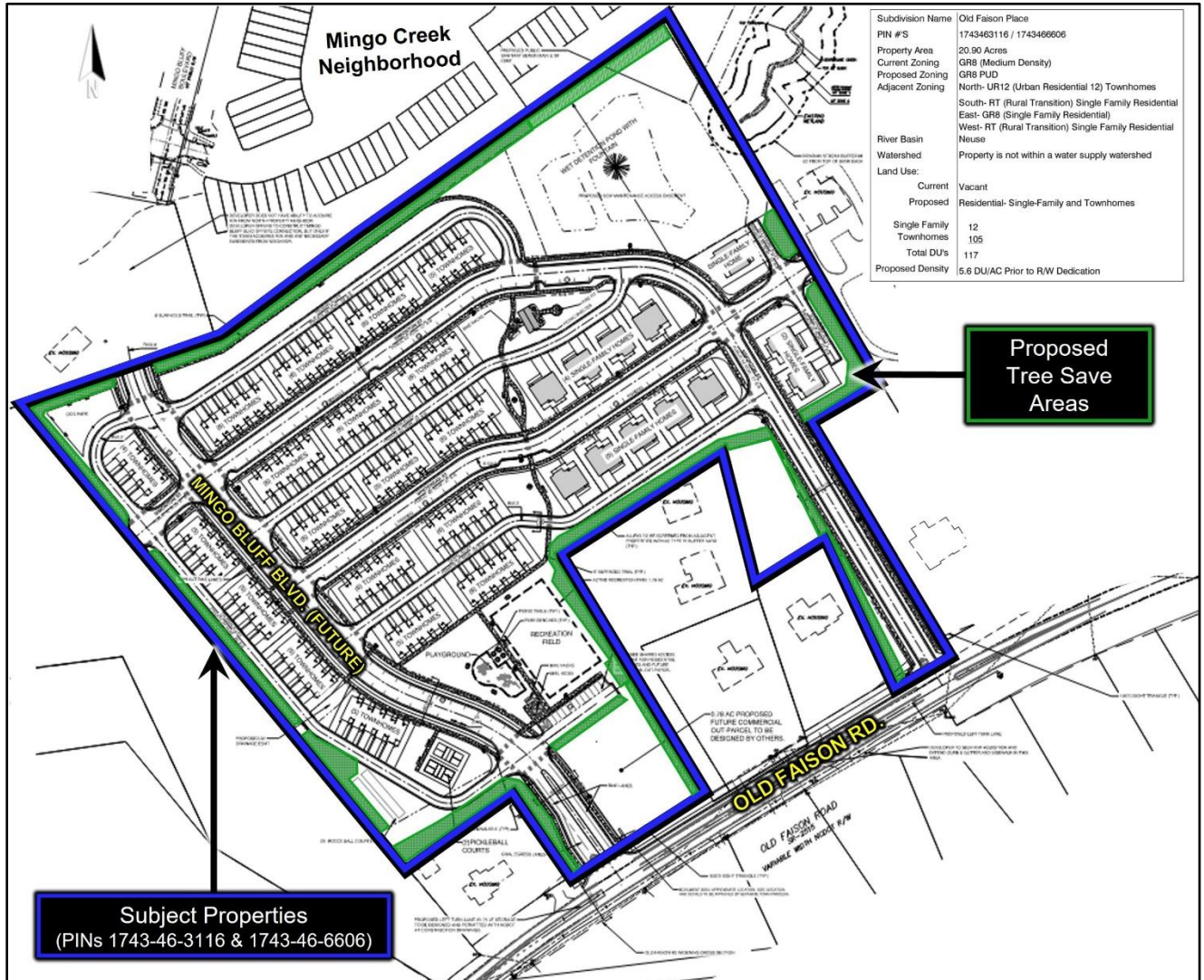
The Applicant has demonstrated general compliance with the Buffer Yard and Screening requirements across the entire site. As proposed, a 20-foot Type B Buffer, made up of existing vegetation and supplemented with new plantings where necessary, will remain around the perimeter of the site. The 20-foot Type B Buffer will also serve as screening for alleys and other infrastructure along the perimeter of the subject properties. At the Neighborhood



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Meeting, there were concerns about the impact of the new roadways on adjacent properties, specifically the three properties between the two proposed neighborhood entrances and the two properties on either side of said entrances. *Staff recommend that the Applicant continue discussions with these neighbors and propose a solution that lessens the impact of the new roadways and that is amenable to all parties.*



Based on preliminary calculations, a tree cover area of 10% of the site (~2.1 acres) is required. These areas of preserved tree cover are generally around the perimeter of the subject properties. *Although general compliance with this regulation has been demonstrated, Staff recommend that the Applicant continue to look for ways to preserve more existing tree canopy through construction. Staff also recommend that larger diameter street trees are planted as the roadways are constructed throughout the development.*

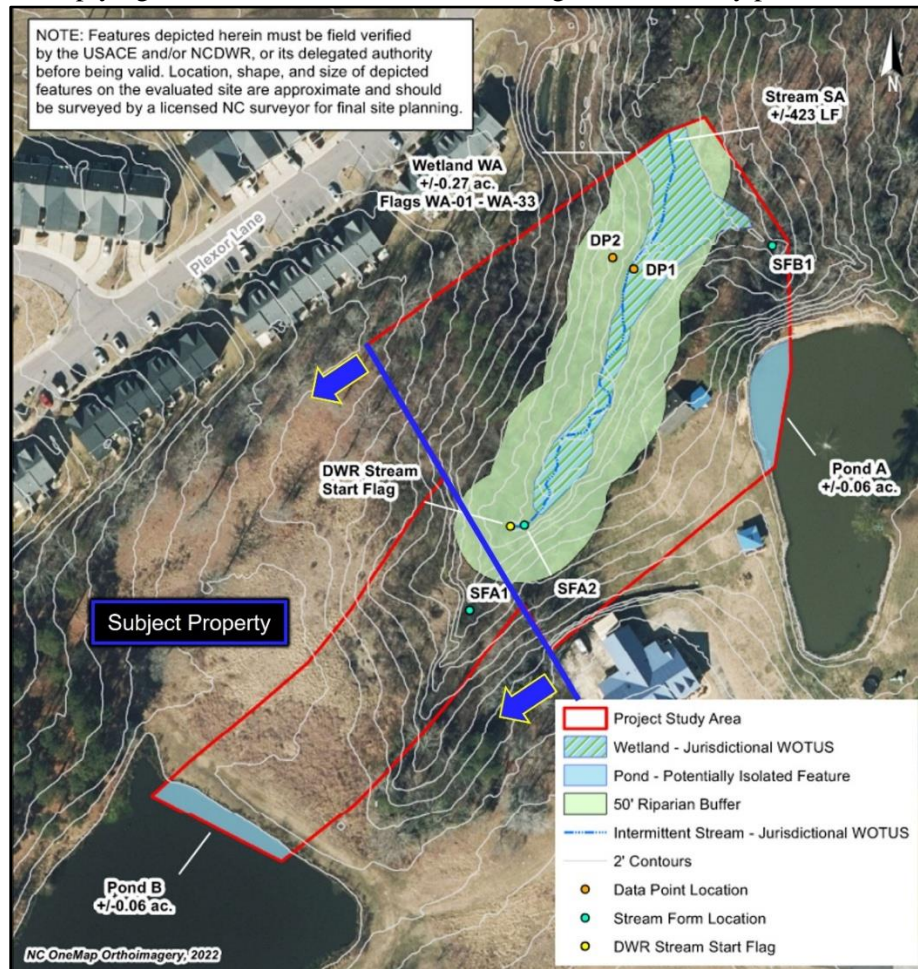


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Environmental Features

According to the NCDEQ Buffer Determination Letter and supporting map, there is a riparian stream on the neighboring property to the east, which projects a limited amount of a riparian stream buffer onto the northeast corner of the subject properties. No impacts are anticipated through construction. According to the Master Plan, the existing pond towards the center of the site will be filled and homes will be constructed. The Applicant will be responsible for complying with all federal, state, and local regulations as they pertain to streams and wetlands.



Open Spaces

A total of 3.64 acres of active and passive open spaces are required for the development based on the proposed Master Plan (50% active; 50% passive). The Applicant proposes a total of approximately 4.49 acres of open space, which exceeds the total amount required.

A variety of recreational open space amenities are provided throughout the development, including active open spaces such as two pickleball courts, bocce ball courts, a multi-purpose field, and a playground. Passive open spaces include a dog park, pavilions/gazebos/shelters, benches, fire pit with landscaped and hardscape areas, and a 6-foot-wide private trail (*Staff recommend modifying to be a 10-foot-wide public trail*). Additional passive recreational open space elements are required to comply with Section 11.2.C.7 of the UDO, specifically elements



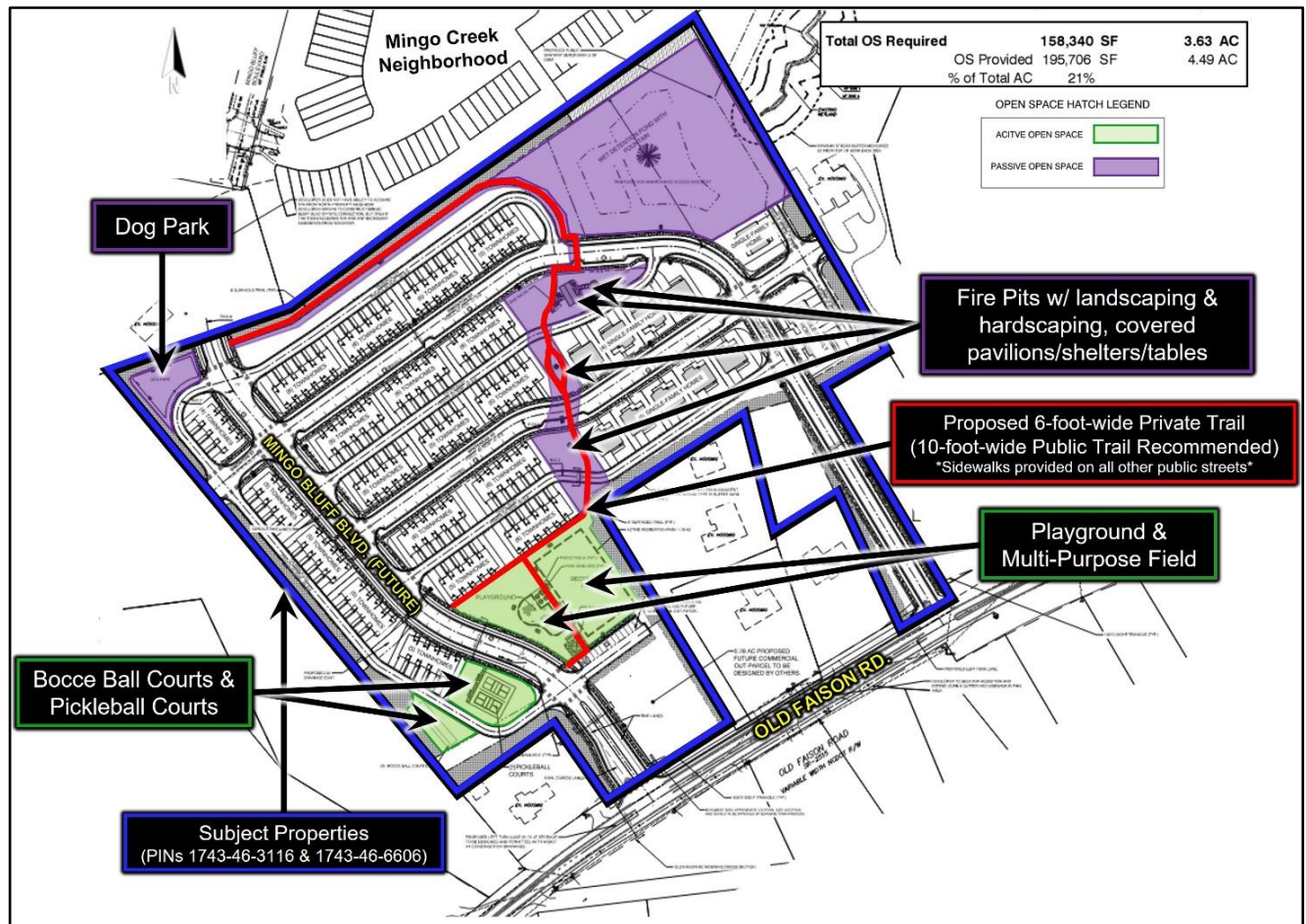
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from the Environmental and Creative categories. *Staff recommend that the Applicant update their PUD Document to include example imagery of these recreational open space elements to demonstrate the quality of the proposed amenities. In addition, Staff recommend that the SCM is amenitized to meet the intent of passive open spaces.*

According to the Applicant, the Developer is willing to build all of the proposed active recreational open spaces, then dedicate the land area and associated facilities to the Town for continued operation and maintenance. According to the Parks and Recreation Master Plan, limited public park facilities exist in the vicinity of the proposed project. Therefore, a public park within the proposed development could help fill in this service gap and provide additional recreational opportunities not only to the future residents, but also the existing residents in the area. Staff have initiated conversations with Town Parks, Recreation, & Cultural Programs and Public Works departments to discuss the viability of a public park and will continue those conversations with Town Staff and the Applicant.

The Applicant has illustrated the general location of all active and passive open spaces on the Master Plan and has demonstrated that there is sufficient land area to accommodate the required recreational areas; however, further definition of the proposed recreation elements within each open space area are necessary to comply with the Section 11.2.B.7 of the UDO. *Additional detail on how the open spaces will be programmed is also recommended by Staff.*





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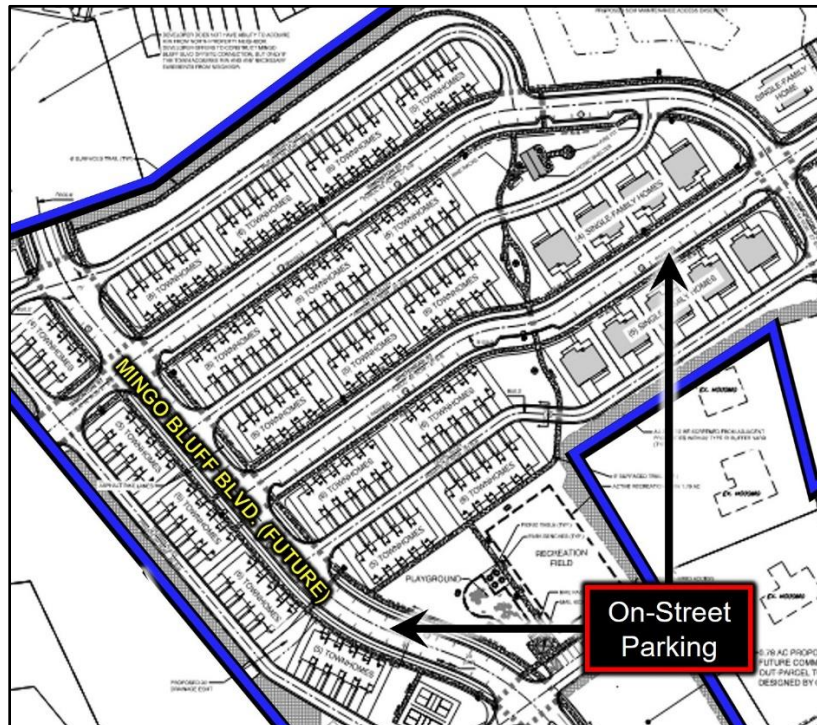
Phasing Plan

The submitted Master Plan and PUD Document do not include a phasing plan. However, according to the Transportation Impact Analysis (TIA), the 12 single-family homes and 105 townhomes will be constructed as a single phase, with the undefined commercial space/tenant coming online as Phase 2 at some point around 2028.

Parking

The Applicant proposes to satisfy the parking requirements for the residential portion of the site through 25-foot-long driveways and private garages for rear-loaded single-family lots and a minimum driveway length of 21 feet for rear-loaded townhome lots. It is important to note that the Applicant requests a Site Development Allowance to reduce the driveway length from 35-feet for rear-loaded housing types. This specific request is outlined in Site Development Allowance Request #2 on Page 15 of this Staff Report.

Although the UDO does not prescribe an on-street parking standard, staff recommend 0.5 dedicated parking spaces beyond what is provided on each lot by way of driveways and garages. The proposed Master Plan shows at least 86 on-street parking spaces are distributed across the majority of the public streets throughout the development. Additional visitor parking and other parking areas have also been proposed (13 parking spaces) near the central open space area and commercial outparcel. Please refer to the proposed Master Plan on Page 4 of this Staff Report. *Staff recommend that the Applicant further define the potential allowable uses, as well as the parameters for the commercial outparcel reservation, and evaluate whether additional parking is needed in this area to account for the variety of future uses. In addition, as previously mentioned, the Applicant has offered to construct all of the required active recreational open space amenities and dedicate those facilities to the Town for continued operation and maintenance. Because these areas are being offered as public spaces, Staff recommend that the Applicant continue to work with Staff on the appropriate number of parking spaces for all of these future uses.*





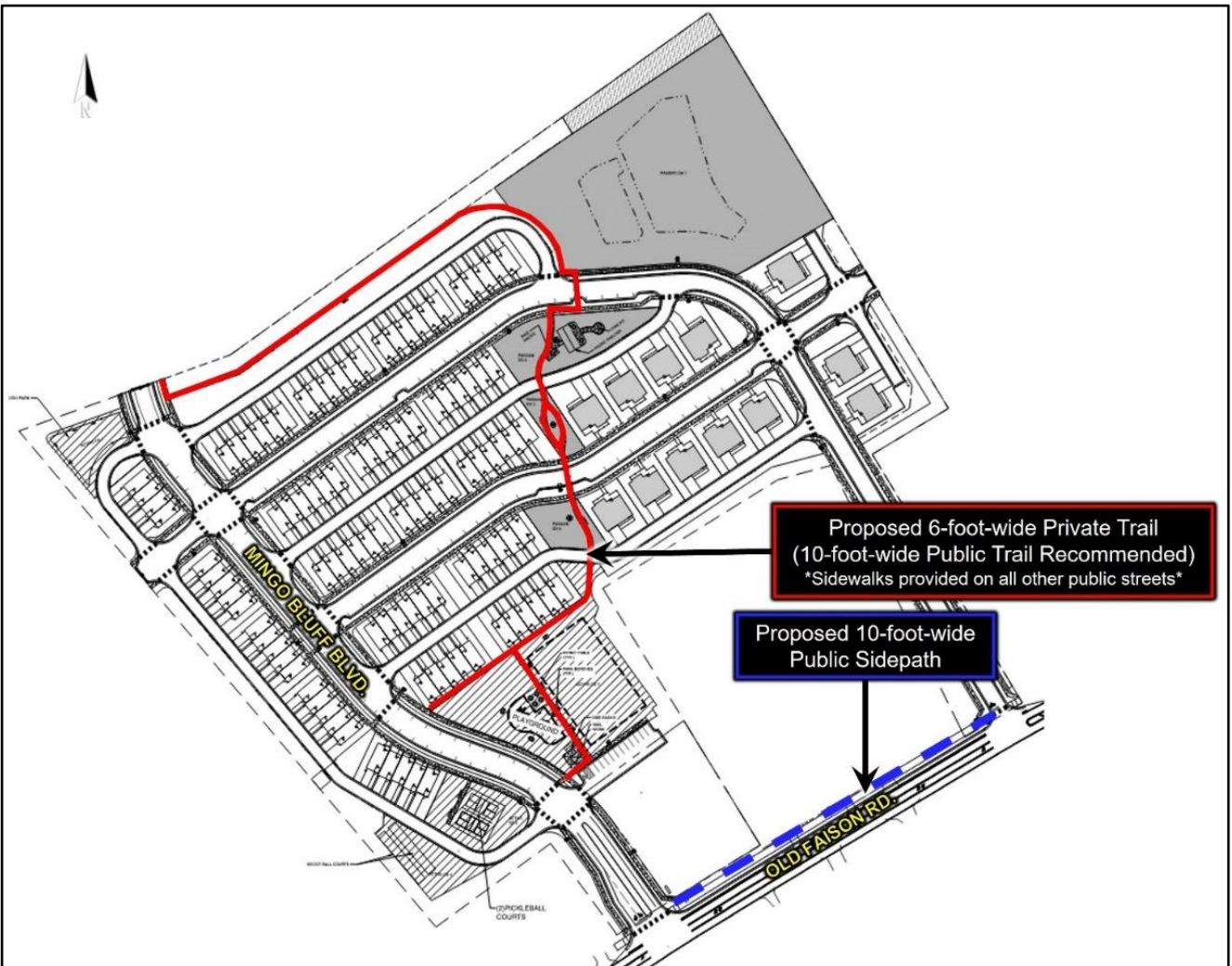
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Pedestrian Connectivity

In order for walking to be a viable transportation choice for local trips, circulation routes must be safe, convenient, and highly connected. Pedestrian circulation and connectivity should primarily take place adjacent to planned streets within the right-of-way; however, a considerable amount of pedestrian activity also takes place on-site, where vehicle speeds are lower but the numbers of potential conflict points are higher.

The Applicant proposes a residential neighborhood with 5- and 6-foot-wide sidewalks along all public roads, except for Mingo Bluff Boulevard extension, which will be built to the Urban Main Street (Wide Sidewalk) roadway cross-section standard with a 10-foot-wide sidewalk on both sides. In addition, the Applicant proposes a 6-foot-wide private trail that meanders through the subject site. *Staff recommend that this corridor is increased to 10 feet to align with the UDO, which requires both public and private trails to be 10-feet wide and paved.*





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Architectural Standards & Elevations

The North Carolina General Assembly enacted legislation in 2024 (SB 382/SL 2024-57) that removed a developer's ability to utilize design standards within development proposals to achieve compliance with utility allocation. As a result, the Town began a two-phase update to amend the 2016 Water Allocation Policy, which removed Residential Architectural Standards from the Bonus Point categories and introduced new items such as affordable housing and green stormwater infrastructure.

Although the Applicant submitted a detailed list of architectural standards for all residential buildings throughout the proposed development, these standards will not be considered for utility allocation. Example elevations from the Applicant are provided in the attached PUD Document and on Pages 11 & 12 of this Staff Report. Upon submission of building permits, should elevations be inconsistent with previous approvals, permits may not be issued.

PROPOSED ARCHITECTURAL STANDARDS – SINGLE FAMILY

1. Single-family units shall have a variety of siding materials, which shall include a minimum of three of the following:
 - a. Fiber cement lap siding
 - b. Fiber cement shake
 - c. Fiber cement board & batten
 - d. Fiber cement smooth panel
 - e. Stone veneer
 - f. Adhered stone veneer
 - g. Brick veneer
2. Single-family units will all have recessed entry with minimum 6" door trim.
3. Single-family units shall have a variety of siding colors. No adjacent unit shall be painted the same color.
4. Single-family units shall have architectural style roof shingles.
5. Single-family unit roofs shall have a minimum main roof pitch of 5:12.
 - a. Forward facing Gables shall be between 8:12-14:12 roof pitch.
 - b. Accent and porch roofs shall be between 3:12-6:12 pitch.
 - c. Accent elements consisting of gable dormers or shed dormers are to be utilized when consistent with the style of the home.
6. Metal accent roofs are encouraged.
7. Single-family units shall have front covered porches, a minimum of 5' deep x 4' wide, with a 6" minimum width pillar/post/column.
8. Single-family unit roofs will all have either a dormer or gable accent.
9. Single-family units will have at a minimum a covered porch and window trim of 4" minimum in width.
10. Single-family units shall have 18% of glazing on the front.
11. Single-family units shall have rear loaded garages.
12. Single-family units shall have a minimum of 1,500 heated square feet and shall be two stories.
13. Single-family roof eave overhang shall be a minimum of 12".
14. 18" foundation brick or stone veneer.



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Example Elevations of Proposed Single-Family Housing Product (Rear-Loaded)



PROPOSED ARCHITECTURAL STANDARDS - TOWNHOMES

1. Single-family units shall have a variety of siding materials, which shall include a minimum of three of the following:
 - a. Fiber cement lap siding
 - b. Fiber cement shake
 - c. Fiber cement board & batten
 - d. Fiber cement smooth panel
 - e. Stone veneer
 - f. Adhered stone veneer
 - g. Brick veneer
2. Townhouse units will all have recessed entry with minimum 6" door trim.
3. Townhouse units shall have a variety of siding colors.
 - a. No adjacent unit shall be painted the same color.
4. Townhouse units shall have architectural style roof shingles.
5. Townhouse unit roofs shall have a minimum main roof pitch of 5:12.
 - a. Forward facing gables shall be between 8:12-14:12 roof pitch.
 - b. Accent and porch roofs shall be between 3:12-6:12 pitch.
 - c. Accent elements consisting of gable dormers or shed dormers are to be utilized when consistent with the style of the home.
6. Metal accent roofs are encouraged.
7. Townhouse units will all be three stories.
8. Townhouse units shall have front covered porches, a minimum of 5' deep x 4' wide, with a 6" minimum width pillar/post/column.
9. Townhouse unit roofs will all have either a dormer or gable accent.
10. Townhouse units will have at a minimum a covered porch and window trim of 4" minimum in width.
11. Townhouse units shall have 18% of glazing on the front.
12. Townhouse units shall have rear loaded garages.



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13. Adjacent townhouse units shall have staggered setbacks as shown on the plan.
14. Townhomes shall be a minimum of 19'-8" wide and a minimum of 1,500 heated square feet.
15. Roof eave overhang shall be a minimum of 12".
 - a. Tight rake overhangs are permitted on porches and main roofs between townhome units.
 - b. 12" minimum rake overhangs on end unit exposed elevation.
16. 18" foundation brick or stone veneer.

Example Elevations of Proposed Townhome Product (20-foot-wide; 3-stories; Rear-Loaded)





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VI. LEGISLATIVE CASE PROCEDURES:

Staff met with the development team in 2022 and 2023 to discuss the potential of a project at this location and worked with them through several preliminary sketch plan details. A Planned Unit Development rezoning is a legislative public hearing, which requires certain application procedures including having a pre-application meeting with staff and holding a neighborhood meeting with any property owners within 200 feet of the outer boundaries of the subject development. Below is a timeline of the required elements.

- Pre-application meeting: June 2023
- Required Neighborhood Meeting: October 15, 2024

The required neighborhood meeting was held virtually at 6:00 PM on October 15, 2024. Approximately nine members of the public attended this meeting. A copy of the mailed notice, the list of recipients, and a summary of the meeting are attached as part of the application packet.

Following the formal submittal and review of the proposed Master Plan and PUD document, the Applicant met with the Development Review Committee (DRC) on November 14, 2024 to discuss the technical comments and details associated with the proposed plan. Staff discussed with the Applicant concerns involving general roadway design, including North Carolina Fire Code requirements, required on-site and off-site transportation requirements, and water, sewer, and stormwater infrastructure design. No action was taken by the DRC, as Town Council is the decision-making authority on Rezoning requests. If the rezoning is approved, all outstanding Master Plan comments from the DRC meeting must be addressed.

Additionally, the Town of Knightdale followed public hearing notice requirements as prescribed in the North Carolina State Statutes.

- First Class Letters Mailed: March 7, 2025
- Sign Posted on Property: March 7, 2025
- Legal Ad Published in the Wake Weekly: March 7 & 14, 2025

VII. PROPOSED PLANNED UNIT DEVELOPMENT:

In support of their Master Plan, the Applicant submitted a PUD Document that includes a vision for their proposed development, statements of plan consistency, design guidelines and dimensional standards, transportation impact information, open space information, Water Allocation Policy compliance, etc.

Staff have reviewed the submittal in accordance with UDO Sections 12.2.G.3.g and 12.3.F and found that all submittal requirements have been met. However, *Staff recommend the Applicant continue to review previously submitted PUD Rezoning applications and provide more details and example imagery that accurately portrays the vision of the proposed development.*

Staff also reviewed the plans for conformance with the General Residential – Medium Density (GR8) Zoning District and other applicable UDO sections. *Although the Applicant did not include information pertaining to requested alternative standards or alternative means of compliance (i.e. Site Development Allowance requests) as part of their submittal, Staff has listed several instances where such a request may be necessary. Following the Joint Public Hearing, Staff recommend that the Applicant update their application to include any and all Site Development Allowance requests with supporting information for consideration.*



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Public Utilities/Water Allocation Policy

The Applicant is proposing public water and sewer connections. Additional information, detail, and Master Plan revisions are necessary to meet the required 50 points for public water allocation.

The North Carolina General Assembly enacted legislation in late 2024 that removed the Town's ability to tie utility allocation to certain design standards within development proposals. As a result, the Town revised the Water Allocation Policy to comply with these legislative changes. The primary goal of this update was to remove and replace certain bonus point items following changes to North Carolina General Statutes, specifically Residential Architectural Standards. Additional changes included removal of underutilized bonus point items, and the introduction of new items such as affordable housing and green stormwater infrastructure. Many changes are proposed to align the Policy with recently adopted and other updated plans, along with minor edits for clarity and consistency. Further updates are expected in 2025.

| | Point Total |
|--|----------------|
| Major Subdivision Base Points | 15 |
| Bonus Point Item | |
| Construct a Fountain or other stormwater amenity within the SCM | 4 |
| Provision of On-Street Public Parking* | 4 |
| Residential Architectural Standards for Single-Family and Townhouse* | 15 |
| Pickleball and Tennis Courts | 5 |
| Enhanced Roadside Landscaping & Hardscaping | 2 |
| IPEMA Certified Playground Equipment | 4 |
| 1,000 LF of 6-Ft Wide Path* | 1 |
| Total Water Allocation Policy Points | 30 pts. |

* No longer included in the Water Allocation Policy as of January 1, 2025.

Some of the Bonus Point category elements from the 2016 Water Allocation Policy were modified with the 2024 update, while some were completely removed. Applicable changes to the Policy that are related to the proposal include the removal of the on-street parking category and architectural standards. *Staff recommend that the Applicant revise the proposal to include additional Water Allocation Policy bonus point elements to make up for these 19 points.*

In addition, the enhanced roadside landscaping category was expanded to include hardscaping. Because of the subject properties' limited roadway frontage along Old Faison Road (See Master Plan on Page 4 of this Staff Report), this bonus point allocation request is generally not supported by Staff as proposed. However, *if enhanced roadside landscaping and hardscaping can be installed along the entire frontage of the neighborhood, including the two properties between the two neighborhood entrances, Staff could consider awarding these two bonus points.*

Finally, the 1,000 LF of 6-foot-wide path bonus point category has been removed since the Unified Development Ordinance was updated in 2024 to require all public and private greenways to be built as 10-foot-wide multi-use paths. As noted in the Open Space section of this Staff Report, the Applicant has offered, as part of the proposal, to build the active recreational open space amenities when the neighborhood is constructed, then dedicate those areas to the Town, in the form of a new public park, for operation and maintenance. *Staff recommend that the proposed 6-foot-wide path is increased to a 10-foot-wide path to better align with the UDO and the Water Allocation Policy.*



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Site Development Allowances

In accordance with UDO Section 12.2.F.3.g, a Planned Unit Development allows the Applicant to request exceptions to certain standards identified in the base zoning district (GR8). These requests should be fair and reasonable, and the proposed alternate means of compliance should meet the spirit and intent of the UDO. As mentioned previously, the Applicant did not provide an explicit list of requested Site Development Allowance with the submittal. For the purposes of this Staff Report and the Joint Public Hearing, Staff have highlighted several UDO requirements that may need further consideration by the Applicant. *Following the Joint Public Hearing, Staff recommend that the Applicant update their application to include any and all Site Development Allowance requests with supporting information for consideration.*

1. **Dimensional Standards** (Section 3.4):

- **Required:** In accordance with UDO Section 3.4, all front-loaded lots shall be a minimum of 80-feet wide while rear-loaded lots cannot be less than 30-feet in width.
- **Requested:** The Applicant proposes a minimum 65-foot width for rear-loaded single-family lots, a minimum 20-foot width for rear-loaded townhome lots.

Staff Analysis: These widths are generally supported by Staff; however, based on recent approvals, and continuous Staff and Council comments, a greater variety of lot widths and housing types remain recommended.

2. **Dimensional Standards** (Section 3.4):

- **Required:** In accordance with UDO Section 3.4, all driveways shall be a minimum of 35-feet in length.
- **Requested:** The Applicant proposes a minimum driveway length of 25 feet for rear-loaded single-family lots and a minimum driveway length of 21 feet for rear-loaded townhome lots.

Staff Analysis: A minimum driveway length of 20-25 feet for all rear-loaded products is in line with recent Town Council approvals.

3. **Street Classification and Design** (Sec. 10.4.A, 10.4.A.2.c, & STD No. 3.05):

- **Required:** In accordance with UDO Section 10.4.A, new development with frontages on existing and new publicly maintained streets shall be required to upgrade all their frontages to meet the standards of this Chapter [10]. Old Faison Road has been identified as an Avenue Roadway cross-section by the CTP and Appendix A, Roadway Network Plan of the UDO. Per that standard cross-section, the middle turn lane/median is required to be 10-feet-wide.
- **Requested:** Since Old Faison Road is an NCDOT system, NCDOT will require this turn lane to be 11-feet-wide.

Staff Analysis: The Town supports this deviation as NCDOT has jurisdiction over the roadway design of Old Faison Road. Staff recommend a half foot is removed from the outer utility strip on either side of the road to make up for the one-foot turn lane/median width increase.

4. **Street Classification and Design** (Sec. 10.4.A, 10.4.A.2, & STD No. 3.03):

- **Required:** In accordance with UDO Section 10.4.A, new development with frontages on existing and new publicly maintained streets shall be required to upgrade all their frontages to meet the



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standards of this Chapter [10]. Mingo Bluff Boulevard has been identified as an Main Street Roadway cross-section by the CTP and Appendix A, Roadway Network Plan of the UDO.

- **Requested:** The portion of Mingo Bluff Boulevard that is constructed as part of the proposed development will be built to the Urban Main Street Roadway cross-section standards.

Staff Analysis: Even though this roadway connection is identified as a Main Street in the CTP, Staff determined that an Urban Main Street (Wide Sidewalk) would be an appropriate deviation in design, largely based upon the fact that townhomes are the predominant housing types proposed with this development. If approved, this design change from a Main Street to an Urban Main Street could result in an additional thirty on-street parking spaces being added for residents and visitors of the neighborhood.

5. Required Distribution of Uses (Section 11.1.B):

- **Required:** In accordance with UDO 11.1.B, new subdivisions are required to provide a variety of uses as detailed in Table 11.1.B. This is calculated as the net development area which excludes street rights-of-way and dedicated open space as detailed in Section 11.2 of the UDO. This standard was adopted in the new UDO to encourage more mixed-density and mixed-use neighborhoods in accordance with the recommendations found in the Comprehensive Plan.

| Table 11.1(B) Required Distribution of Uses | | |
|--|-----------------------------------|----------------------|
| Use Type | Minimum Distribution | Maximum Distribution |
| Dwelling-Duplex/Townhouse | 10% | 40% |
| Dwelling-Multifamily | 5% | 40% |
| Dwelling-Single Family | 15% | 60% |
| Mixed Use* | 10% | 100% |
| Lodging/Office/Service/Retail/Restaurant/Entertainment/Recreation* | 5% | 20% |
| Civic/Institutional* | As determined by the Town Council | |
| *Minimum and maximum distribution requirements shall apply only to nonresidential subdivisions or subdivisions with both residential and non residential uses. | | |

- **Requested:** The applicant proposes a neighborhood consisting of the following uses.

| Proposed Distribution of Uses | | |
|-------------------------------|-----------------|------------------------|
| Use Type | Area in Acreage | Percent of Development |
| Single-Family Dwelling | 3.13 | 15% |
| Townhome Dwelling | 8.1 | 39% |
| Multi-Family Dwelling | 0 | 0% |

*Gross Site Area: 20.90 acres

*Rights-of-Way Acreage: Data Not Provided

*Dedicated Open Space (Active/Passive): 4.49 acres

Staff Analysis: The total acreage in rights-of-way and the total area in dedicated open spaces needs to be removed to accurately calculate the proposed distribution of uses.

6. Intersection Design, Collector Streets (Section 11.3.B.4):

- **Required:** In accordance with UDO Section 11.3.B.4, the intersection of...a collector street and an arterial street shall be designed as a roundabout when under the jurisdiction of the Town of



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Knightdale. When under the jurisdiction of the NCDOT, roundabout shall be the Town's local preference.

- **Requested:** The Applicant proposes to design the intersection of Site Access A (Mingo Bluff Boulevard extension) and Old Faison Road as a stop-controlled intersection, as recommended by the Transportation Impact Analysis (TIA).

***Staff Analysis:** Staff made the Applicant aware of this requirement as they were preparing the TIA for the project. That Analysis found that a roundabout in this location does not meet the appropriate thresholds for NCDOT to recommend one be installed in lieu of a stop-controlled intersection or a signalized intersection. Staff also requested that the Applicant's Traffic Engineer perform a Signal Warrant Analysis for this intersection, which found that a signal is not warranted at this time; however, the Analysis recommends that the Applicant monitor for signalization upon completion of Phase 2. Additional information is provided under the Roadway Connectivity and Improvements Section of this Staff Report (Pages 19-20).*

VIII. TRANSPORTATION ANALYSIS:

The proposed roadway network associated with this development has been reviewed by Town Staff for consistency with the Comprehensive Transportation Plan (CTP). The Applicant is proposing a roadway design and hierarchy that is generally consistent with the CTP and Roadway Network Map included in Appendix A of the UDO; however, the Applicant is requesting several Site Development Allowances related to roadway standards (See Site Development Allowances on Pages 15-17 of this Staff Report).

In accordance with UDO Section 11.3, a Transportation Impact Analysis (TIA) was performed by DRMP, as the number of peak hour trips generated by this site exceeds the required 150 trips prescribed by the UDO. Staff met multiple times with the developer to refine the scope of the analysis and ensure the proper uses were being included.

Old Faison TIA – Phase 1 Site Trip Generation Estimates

| Table 2a: Site Trip Generation (Phase 1) | | | | | | |
|---|------------------|------------------------------------|---|-------------|---|-------------|
| Land Use (ITE Code) | Intensity | Daily Traffic (vpd) | Weekday AM Peak Hour Trips (vph) | | Weekday PM Peak Hour Trips (vph) | |
| | | | Enter | Exit | Enter | Exit |
| Single-Family Detached (210) | 12 DU | 144 | 3 | 8 | 9 | 5 |
| Single-Family Attached (215) | 105 DU | 750 | 12 | 37 | 35 | 24 |
| Total Trips | | 894 | 15 | 45 | 44 | 29 |

Based upon the guidance provided by the Comprehensive Plan (subject properties being located within a Mixed-Use Neighborhood Future Place Type), a non-residential component has been recommended by Staff for better alignment and consistency. According to the Applicant, specific land uses and tenants have not been identified for the reserved commercial outparcel; however, the TIA notes that for these reasons, a conservative approach to the analysis was taken, and for the purpose of the analysis, a 3,500 square foot fast-food restaurant with drive-through was assumed to be constructed on the commercial outparcel. This non-residential use was introduced at full build, following construction of the residential component of the proposal.



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Old Faison TIA – Phase 2 Site Trip Generation Estimates

| Table 2b: Site Trip Generation (Full Build) | | | | | | |
|--|-----------|---------------------------|--|------|--|------|
| Land Use (ITE Code) | Intensity | Daily Traffic (vpd) | Weekday AM Peak Hour Trips (vph) | | Weekday PM Peak Hour Trips (vph) | |
| | | | Enter | Exit | Enter | Exit |
| Single-Family Detached (210) | 12 DU | 144 | 3 | 8 | 9 | 5 |
| Single-Family Attached (215) | 105 DU | 750 | 12 | 37 | 35 | 24 |
| Fast-Food w/ Drive-Thru (934) | 3.5 KSF | 1,636 | 80 | 76 | 60 | 56 |
| Total Trips | | 2,530 | 95 | 121 | 104 | 85 |
| Pass-By Trips: Fast-Food Restaurant with Drive-Through (50% AM, 55% PM) | | | -39 | -39 | -32 | -32 |
| Total Primary Trips* | | | 56 | 82 | 72 | 53 |
| *No internal capture assumed; 5% of regional distributions assumed to originate from proposed development. | | | | | | |

Although Staff recommend the Applicant propose additional land uses and provide additional parameters for the commercial outparcel to follow, Staff generally concur with this conservative approach and support the improvements recommended by the analysis. To ensure safe and efficient travel patterns, Staff recommend a condition is crafted and agreed upon that requires further transportation analysis to be conducted once a tenant is known.

During the scoping phase of the TIA process, Town Staff, NCDOT, and the Development Team worked together to determine the study area – only study the two proposed neighborhood entrances. The TIA included recently approved or under construction projects including Silverstone, StoneRiver, Elevate Riverview (FKA Riverview Commons), and Lyndon Oaks background data to assign future trips. Additionally, the TIA assumed a 3% trip growth rate during the period of construction for this project.

The following improvements are recommended by the TIA to be completed by the developer and are generally supported by Town staff and NCDOT; however, additional improvements may be recommended as the project progresses.

Phase 1:

Old Faison Road and Site Access A

- Construct Site Access A as a full-movement access with one ingress lane and two egress lanes.
- Construct an exclusive eastbound left turn lane on Old Faison Road.
- Provide stop control for Site Access A

Old Faison Road and Site Access B

- Construct Site Access B as a full-movement access with one ingress lane and one egress lane.
- Provide stop control for Site Access B.

Phase 2:

Old Faison Road and Site Access A

- Construct an exclusive westbound right turn lane & monitor for signalization upon completion of Phase 2.

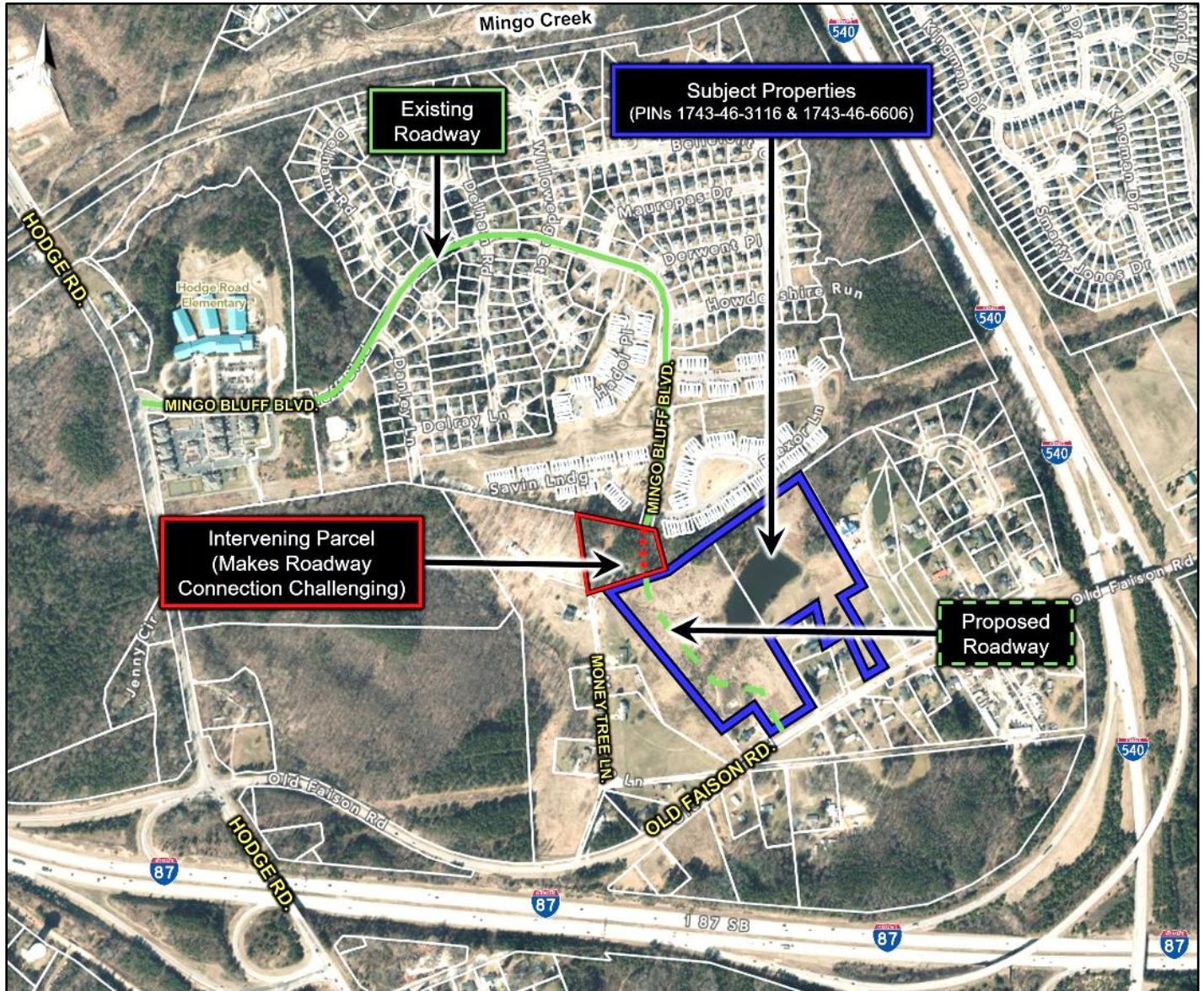


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Roadway Connectivity and Improvements

The proposed development could potentially extend Mingo Bluff Boulevard from its current terminus near Plexor Lane to the north, through the development, and connect to Old Faison Road; however, there is an intervening parcel that is not controlled by the subject property owner.



Absent of the subject property owner purchasing the intervening parcel, the subject property owner negotiating an appropriately sized right-of-way and/or easement(s), or some other agreed upon solution, this Mingo Bluff Boulevard roadway extension may be difficult to realize through the construction of the project. However, if this roadway connection were made, a new collector road would be completed that provides existing residents of the Mingo Creek subdivision, future residents of the proposed subdivision, as well as other traveling motorists with another routing option when entering or exiting their homes. According to the Applicant, the Developer does not have the ability to acquire the necessary right-



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of-way but offers to construct the Mingo Bluff Boulevard extension if the Town acquires said right-of-way and/or easements necessary for the Developer to construct the road.

Because this roadway extension completes a collector road and provides additional routing options for motorists, Staff recommend the Applicant be prepared to discuss specific actions taken to attempt to acquire said right-of-way and/or easements, to highlight other potential solutions and paths forward, and to highlight for Council how the proposal meets the expectations of the Town's adopted guiding documents, how it furthers the goals of the Town, and why the proposal helps achieve the vision of this area.

All other streets within the development will be dedicated to the Town as public streets, with the exception of certain private alleys throughout the development as shown on the Master Plan.

In terms of pedestrian connectivity and roadway improvements, there are two parcels between the proposed development's entrances along Old Faison Road that are not part of the proposal and will likely remain in private ownership. In an effort to increase pedestrian connectivity and to build larger sections of CTP roadways, Staff have worked with the Applicant to see if it would be feasible for all roadway improvements associated with the Avenue Roadway cross-section (curb & gutter, sidepath, street trees, street lights, etc.) to be installed between the two entrances when the neighborhood is built. According to the Applicant, the Developer is willing to construct these improvements as long as they are able to acquire the necessary rights-of-way from these adjacent property owners to allow said improvements.



In accordance with Section 11.3.B.4 of the UDO, where two collector streets intersect, said intersection is required to be designed as a roundabout when under the Town's jurisdiction. However, when one or more of the roads are NCDOT roads, a roundabout is the Town's preference. Specifically, Mingo Bluff Boulevard extension would be a Town road, and Old Faison Road would remain an NCDOT road. Therefore, Staff requested that the Applicant's Traffic Engineer study this intersection for roundabout warrants and feasibility. Based upon the Transportation Impact Analysis (TIA), and confirmation from NCDOT, a roundabout at the intersection of Mingo Bluff Boulevard extension and Old Faison Road at the proposed development's entrance is not warranted nor recommended. In lieu of the roundabout, the TIA recommends that this intersection is designed as a stop-controlled intersection (stop signs along Mingo Bluff Boulevard extension; no stop signs along Old Faison Road) with left turn lanes for westbound traffic to enter the neighborhood. *Staff concur with this recommendation and will continue to work with the Applicant to ensure this intersection is designed in accordance with all Town and NCDOT standards.* Although an alternative design is agreed upon by Town and NCDOT Staff, a Site Development Allowance request is still required (See SDA #5 on Pages 16-17 of this Staff Report).



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IX. COMPREHENSIVE PLAN:

Since the adoption of the original KnightdaleNext 2035 Comprehensive Plan in 2018, the Town of Knightdale has experienced significant growth and there have been new pressures and realities not contemplated in 2018. These development pressures and considerable growth prompted a desire to update the Comprehensive Plan to allow for the proactive management of the location, timing, and magnitude of future development to protect the character and integrity of Knightdale. As shown in the plan's new name, KnightdaleNext V.2, is an update to an existing document, not an entirely new plan, with the same horizon year of 2035 as the 2018 version.

KnightdaleNext V.2 includes better guidance for where development activity should occur and to what scale, with detailed information to help inform the style of development Town Council expects when striving to create an “inclusive and connected urban small town with unique gathering places that foster a sense of community” as noted in the Council’s Strategic Plan. A mantra for KnightdaleNext V.2 is to grow intentionally between now and the planning horizon (2035). This includes an emphasis on growing inward, versus outward, in the near future to be good stewards of land and cognizant of the time and money realities associated with extending infrastructure.

The General Framework, presented in Chapter 3, includes three maps that should guide future decisions about conservation and development in the community. All three maps should be used in a series when contemplating the future of a parcel or area.

A. Intentional Growth Area Map

The first map in the series offers a means to efficiently organize and illustrate a hierarchy of growth and conservation priorities for town leaders.

Adherence to this Map will facilitate the convergence of public and private decision-making processes, leverage town resources with other investment dollars, manage the amount and timing of new infrastructure required to support future development while avoiding sprawl, implement a town-wide strategy for identifying and securing open space, and enhance the town’s influence in future development decisions that directly impact the quality-of-life for all residents in the area.





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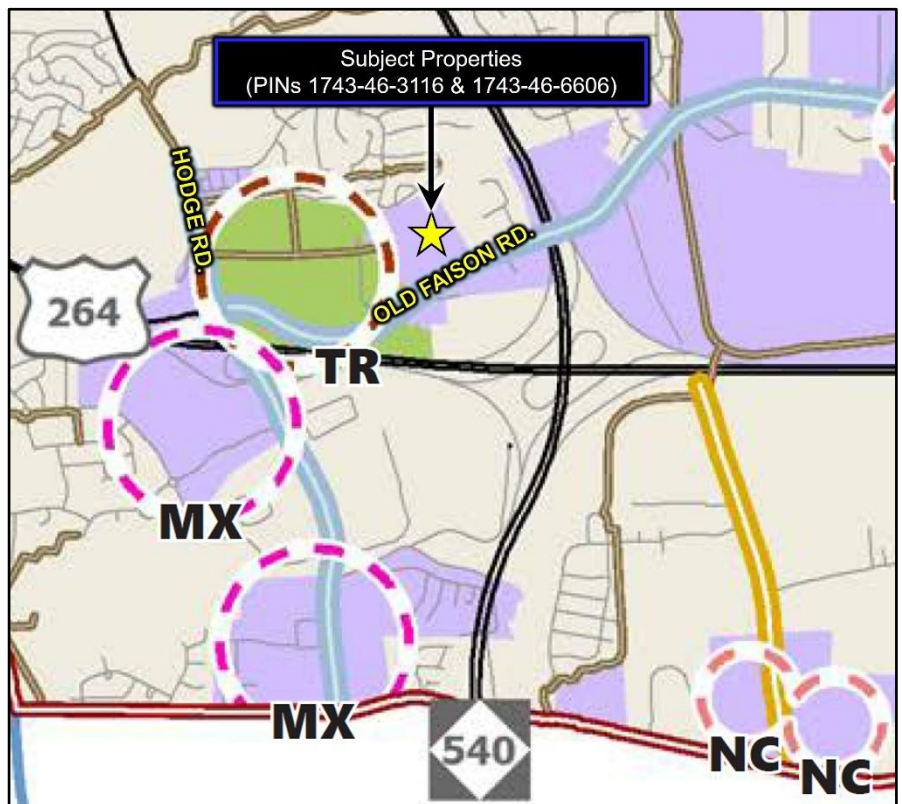
The subject property is located within a Secondary Growth Area. These areas include land contiguous to town limits that could help accommodate growth projected for an eleven-year planning horizon, 2024 to 2035, assuming annual growth rates similar to the last decade (i.e., 4.21% per year). Some areas on the map include smaller parcels anticipated to infill in a manner that complements existing buildings and their surrounding environment. Other areas on the map include large, undeveloped parcels that provide opportunities to be forward-thinking about a vision for conservation and development that includes a mix of uses or densities that create new, energized centers, corridors, or neighborhoods in the community.

A portion of the secondary growth area is inside existing or planned utility service areas. These areas should be targeted for small- to large-scale infrastructure investments that keep properties economically-viable in the future. Town officials should advocate for new infrastructure projects that upsize for additional capacity and fill-in missing segments for important street, water, or sewer connections. Continued investment in these areas should maximize return on-investment potential for the town by 1) minimizing utility expansion costs and 2) maximizing land productivity for private property owners that increases tax revenues for the town. Infrastructure investments that support economic development — new jobs or new sales tax revenue — in the secondary growth area should be a high priority.

B. Urban Small Town Framework Map

The next map in the series provides descriptions establishing general expectations regarding desired characteristics for different geographic areas in the community, including the general objective of each category, important considerations based on location, and targeted treatments to facilitate progress.

The Urban Small Town Framework Map presents five center-based categories and four corridor-based categories to further organize the town's planning area and emphasize treatments for specific areas. Town officials should advocate for increased or decreased development intensities in areas depicted on the map to support the town's broad goals and initiatives presented in the comprehensive plan (e.g., increased development intensities to support transit ridership or decreased development intensities to reinforce rural landscapes). Officials should also coordinate decisions for specific lots or parcels in an identified center or corridor to instill (reinforce) a unique development brand or sense of place.





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The Urban Small Town Framework Map identifies the subject properties within a Mixed-Use Development Target Area that is along a Mixed-Use Corridor (Old Faison Road). In addition, the subject properties are immediately adjacent to a Trail-Oriented Development Activity Center, a Trail-Oriented Development Target Area, and a Trail-Oriented Development Corridor. Further definition and additional details are provided below; however, more specific direction for decision making in these areas is presented on the Future Place Type Map and its corresponding place type category descriptions.

Mixed Use Corridor: In some cases, the mixed-use development form, pattern, and intensity described for an activity center category may extend outward from a mixed-use area along one or more transportation corridors. A mixed-use corridor leading to a mixed-use activity center, or linking two or more activity centers together should be considered a desirable condition in the community.

Some mixed-use corridors in Knightdale are developed as auto-oriented shopping centers, industrial parks, or residential neighborhoods with few vacant parcels left for infill development. Larger, deeper parcels in the developed corridors may allow for significant redevelopment in the future that supports mixed-use buildings and medium- to high density residential buildings. Smaller, shallower parcels in developed corridors may be less likely to redevelop as mixed-use buildings in the future, and should emphasize improvements in their limited spaces that create more connected and consistent walkable/bikeable environments for travelers in the corridor.

Other mixed-use corridors in Knightdale are less developed and offer immediate opportunities to support mixed-use buildings and medium- to high density residential buildings. Stub-out streets should be provided in new development if adjacent land is vacant to create a secondary street network along the corridor that supports more localized trips. Considerations should also be given to ensure building heights, scale, and massing are compatible with existing development in the corridor or adjacent to the corridor.

New (re)development of parcels in a mixed-use corridor should meet the town's goals for creating more walkable and bikeable streets in the community. Streetscape treatments — street lights, street signs, street trees, sidewalks, bicycle facilities, traffic signals, sidewalks, or seating areas — should be consistent along specific corridors, or at least for portions of corridors identified as districts, to reinforce unique experiences for visitors.

Trail-Oriented Development Activity Center (TR): A new or improved activity center located along an existing or proposed long-distance greenway or trail (see the map in the Shift Knightdale Comprehensive Transportation Plan) should incorporate trail-oriented development principles into its overall design, including buildings oriented toward the greenway corridor, outdoor seating that fronts the greenway, public or private spaces near the greenway corridor that accommodate trail users, and a network of secondary trails that connect into the primary greenway corridor. Road crossings with the greenway in the activity center should be minimized. Amenities in a trail-oriented activity center — bike parking, outdoor furniture, bike repair stations, and public art — should also be incorporated into the overall design concept.

Trail-Oriented Development Corridor: In some cases, trail-oriented development principles focused in an activity center may extend outward along one or more greenway corridors. A trail-oriented development corridor leading to a trail-oriented development activity center, or linking two or more trail-oriented development activity centers together should be considered a desirable conditions in the community.

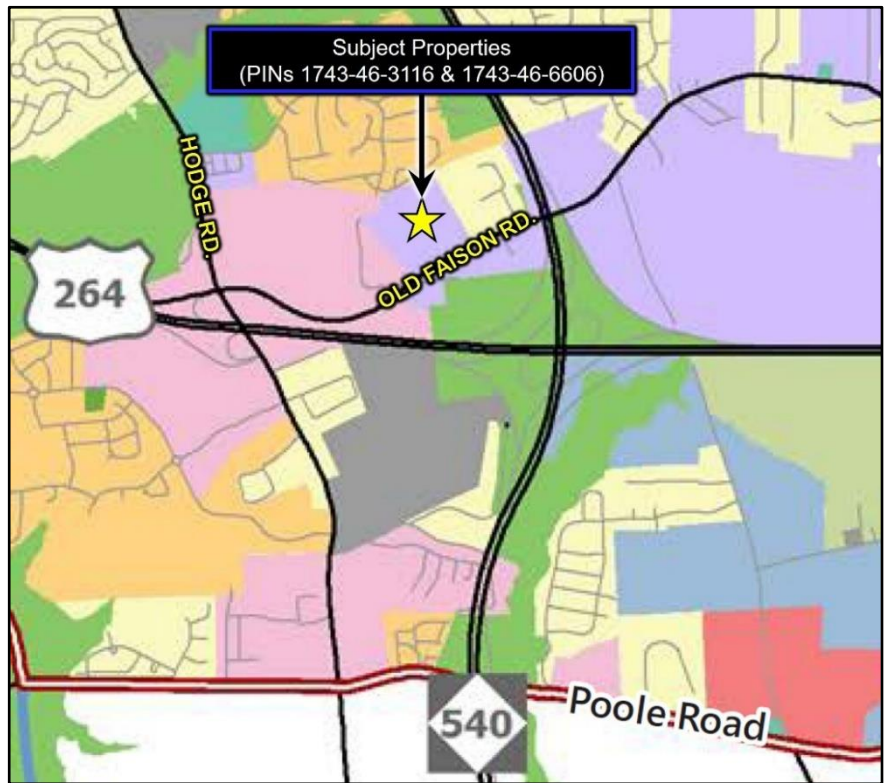


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C. Future Place Type Map

The final map in this series depicts preferred development types, locations, patterns, styles, and intensities for the planning jurisdiction assuming full build-out of the community. It also provides a physical framework to more effectively realize the community vision statement and guiding principles presented in the comprehensive plan. These recommendations set a long-term vision for a more diverse development portfolio in Knightdale that is forward-thinking, focused on new or improved development activity centers, promotes economic development via strategic place-making investments, mindful of supporting infrastructure, aware of residents' quality-of-life, and economically-viable and financially sustainable for the town.



The depiction of all thirteen categories on the map sets high-level expectations for the types, locations, patterns, and styles of development preferred in the community. Developers and property owners should consult the map to confirm desirable (supportable) development outcomes before making major investment decisions. Town residents should consult the map to understand what is generally intended for vacant or declining properties in the community if or when a developer or property owner decides to make an investment.

The subject property is located within the Mixed-Use Neighborhood Future Place Type, which is defined as:

“Land offering the opportunity to live, work, shop, and play in a single community. Mixed Use Neighborhoods emphasize a mix of uses, one or more Neighborhood Centers, and one or more neighborhoods connected to the activity centers by a network of pathways or walkable streets.

An activity center within the Mixed-Use Neighborhood provides goods and services to surrounding neighborhoods. The center's proximity to neighborhoods requires that operations be at a scale and design compatible with nearby residential development. The design of Mixed-Use Neighborhoods transitions effectively between residential and non-residential uses, and includes safe and convenient pedestrian and bicycle access for nearby residents.

Residential neighborhoods within and adjacent to the activity center(s) are encouraged to offer different home types on varied lot sizes with a range of home choices. Neighborhoods should mix home types, lot sizes, or home



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sizes on the same block or offer accessory dwelling units. Homes should be oriented toward streets or public open space. New neighborhoods should include a comprehensive and connected network of open space throughout the site to accommodate small parks, multiple gathering spaces, and community gardens, as well as environmental site design features to help manage stormwater runoff. Internal streets and open space throughout new Mixed-Use Neighborhoods should connect to existing or future neighborhoods nearby.”

Mixed-Use Neighborhood

Land offering the opportunity to live, work, shop, and play in a single community. Mixed Use Neighborhoods emphasize a mix of uses, one or more Neighborhood Centers, and one or more neighborhoods connected to the activity centers by a network of pathways or walkable streets.

An activity center within the Mixed-Use Neighborhood provides goods and services to surrounding neighborhoods. The center's proximity to neighborhoods requires that operations be at a scale and design compatible with nearby residential development. The design of Mixed-Use Neighborhoods transitions effectively between residential and non-residential uses, and includes safe and convenient pedestrian and bicycle access for nearby residents.

Residential neighborhoods within and adjacent to the activity center(s) are encouraged to offer different home types on varied lot sizes with a range of home choices. Neighborhoods should mix home types, lot sizes, or home sizes on the same block or offer accessory dwelling units. Homes should be oriented toward streets or public open space. New neighborhoods should include a comprehensive and connected network of open space throughout the site to accommodate small parks, multiple gathering spaces, and community gardens, as well as environmental site design features to help manage stormwater runoff. Internal streets and open space throughout new Mixed-Use Neighborhoods should connect to existing or future neighborhoods nearby.



Open Spaces & Natural Resources

- Be sensitive to existing natural resources on a site, including tree stands, steep topography, and natural drainage ways.
- Incorporate open space elements throughout the neighborhood, including common greens, squares, plazas, small parks, playgrounds, community gardens, trails, or greenways.

Transportation

- Streets in new neighborhoods are built as Complete Streets. Design elements serve multiple modes of transportation, such as bike racks, benches, bus stops, etc.
- A connected network of streets and sidewalks serves the neighborhood.
- Streets are designed with curb and gutter drainage.
- Formal tree plantings are located along streets and in open space areas.

Street & Block Pattern

- A grid street network is used throughout the neighborhood.
- Small- to medium-size blocks promote a more walkable environment.
- Stub out streets provide connections to adjacent vacant land.
- Formal, designated on-street parking throughout the neighborhood.
- Off-street parking should be accommodated at the side or rear of the lot to minimize the presence of parked automobiles on driveways along residential streets.

Building Types & Massing

- Different building types should not be fully separated from each other (like pods) in a development.
- Incorporate “missing middle” home choices in the neighborhoods, including: single dwelling homes on small lots, townhomes, duplexes, triplexes, and quadplexes.
- Accessory dwelling units are allowed on all residential lots.
- Concentrate low-profile non-residential buildings (two to three stories) in a discernible center of the neighborhood. Encourage residential units or office space above storefronts.
- Cross reference with House, Townhouse, Apartment, Mixed Use, or Commercial Building Types in the Town of Knightdale Unified Development Ordinance.



Sustainability

- Solar panels on rooftops in public areas or private lots should be considered to promote renewable energy.
- Minimize site grading to maintain natural stormwater flows.
- Maintain large tree stands to keep spaces cool in warmer months.
- Public electric vehicle charging stations should be considered in the activity center of a larger neighborhood.
- Green stormwater infrastructure should be utilized to the greatest extent possible.

Infill Development or Redevelopment

- Provide connections via streets, sidewalks, or greenways to future neighborhoods and non-residential development.
- New, infill homes should complement adjacent existing development in terms of building height, setbacks, and architectural details.
- Install missing pedestrian and bicycle infrastructure in existing neighborhoods.

The place type transect, shown below, builds off of the Future Place Type Map and helps to illustrate how developments should blend together as Knightdale grows. The Mixed-Use Neighborhood Place Type falls into the Neighborhoods category.



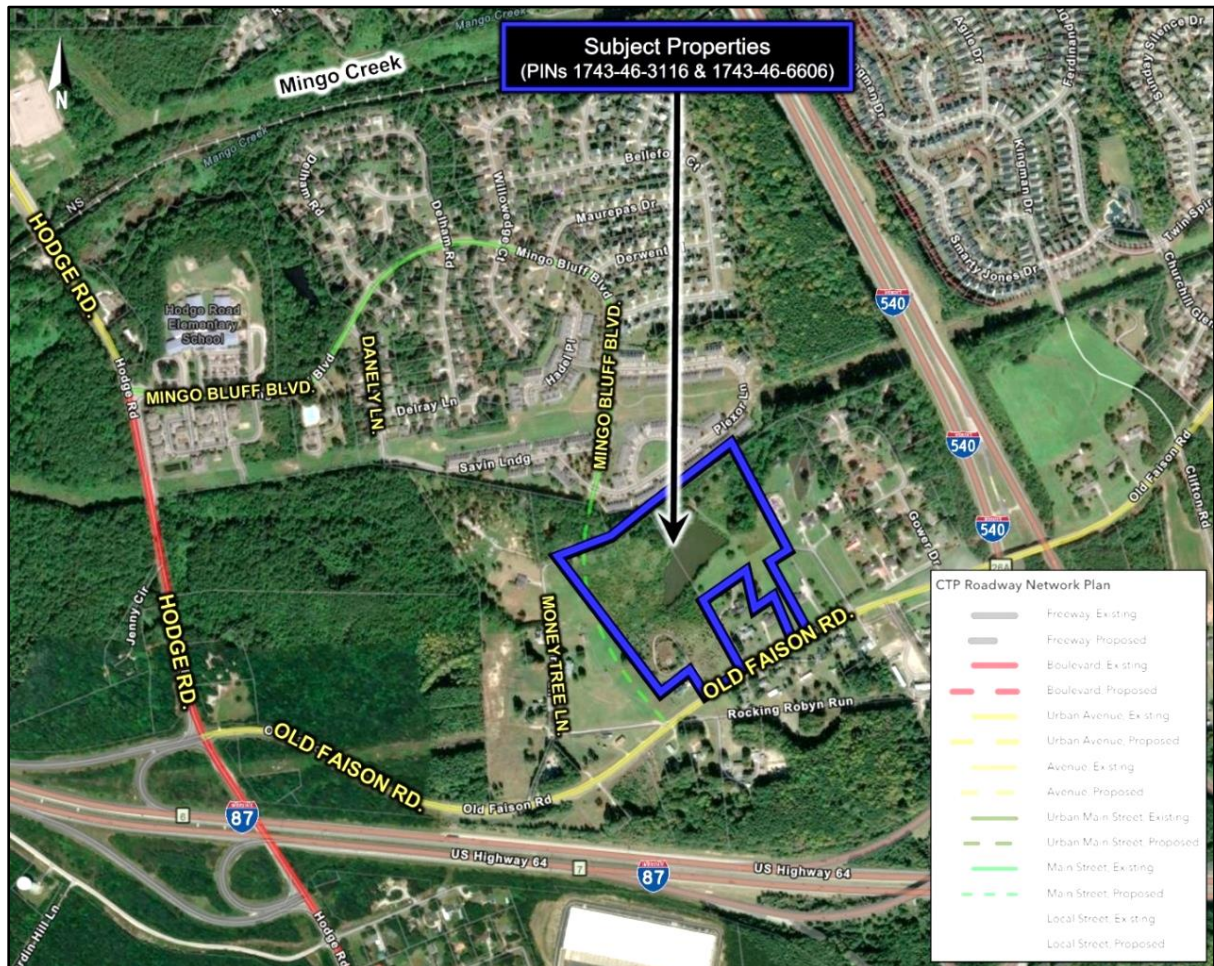


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D. Street Network Map

Development activities should assist with the construction of a comprehensive transportation network. The proposed development will construct the portion of Mingo Bluff Boulevard, which will eventually connect Old Faison Road to Hodge Road through the Mingo Creek subdivision. However, there is an intervening parcel between the subject properties and the existing Mingo Bluff Boulevard within the Mingo Creek subdivision that reduces the viability of making this roadway connection through construction of this proposed project.



Staff encourage the Applicant to continue conversations with the adjacent neighbors and look for ways to acquire the necessary land area to make this roadway connection between the existing Mingo Creek neighborhood and the proposed development. Staff also recommend the Applicant is prepared to discuss the measures taken to acquire said land area.

Although this roadway connection is identified as a Main Street on the Comprehensive Transportation Plan (CTP), Staff determined that an Urban Main Street (Wide Sidewalk) would be an appropriate deviation in design, largely based upon the fact that townhomes are the predominant housing types proposed with this development (see requested Site Development Allowance on Pages 15-17 of this Staff Report). If approved, this design change from a Main Street to an Urban Main Street could result in an additional thirty on-street parking spaces being added for residents and visitors of the neighborhood. The differences in design are shown on the next page.



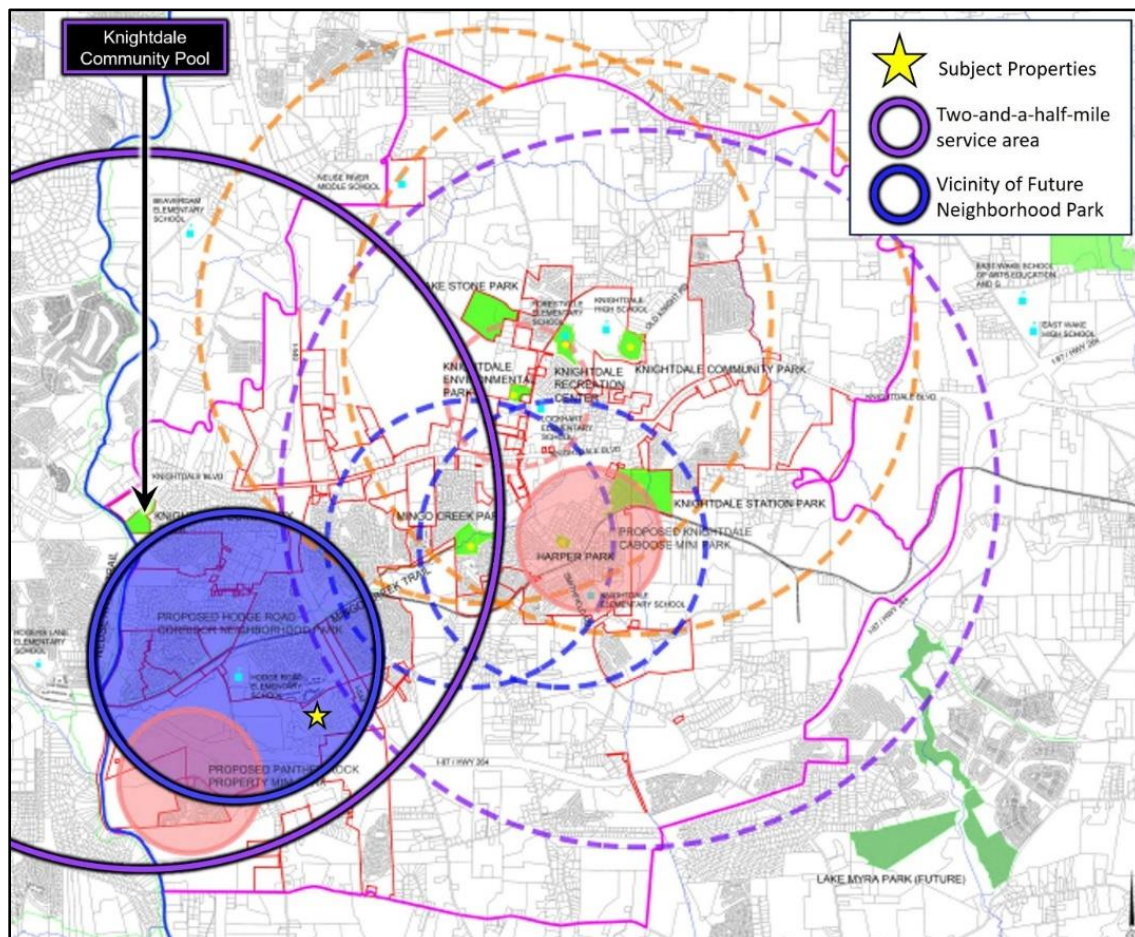
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CONSISTENCY WITH THE COMPREHENSIVE PARKS AND RECREATION MASTER PLAN:

The Knightdale Town Council adopted the Town's first ever Comprehensive Parks and Recreation Master Plan in April 2022. According to the Plan, geographic equity of park access is an important consideration to provide short distance access and service to all Knightdale residents. The Plan identifies recreation service areas to help formulate recommendations for future park facilities based on community needs and existing conditions. Future park site acquisitions should be balanced and proportional to service populations in all areas of the Town to better serve the community and provide additional, needed recreation opportunities for the current and future Knightdale population.

Of the existing parks throughout Knightdale, this proposal is within the two-and-a-half-mile service area of Knightdale Community Pool. The subject properties are within the one-mile service area of a future neighborhood park that has been identified along the Hodge Road corridor; however, at this time, there are no plans for programming that park. As mentioned previously, the Applicant has offered, as part of the proposal, to build the active recreational open space elements when the neighborhood is constructed, then dedicate those areas to the Town for operation and maintenance of those recreational amenities in the form of a new public park. As noted in the Plan, a future neighborhood park has been identified in the southwest area of town, proximal to the subject properties. These types of recreational opportunities are recommended to provide a place for general day use activities in a portion of the community where they don't currently exist for those living outside of private neighborhood based recreation services or within proximity to existing Town owned parks.





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Typical neighborhood park amenities and elements summarized below and are further outlined on Pages 17 & 18 of the Parks & Recreation Master Plan.

Neighborhood Parks

Neighborhood parks offer the public a convenient source of recreation while serving as the social focus point of neighborhoods. Usually located within walking distance of the area serviced, neighborhood parks provide both active and passive recreation opportunities that are of interest to all age groups. The smaller size of neighborhood parks, as compared to those previously mentioned, requires intense development with fifty percent of each site remaining undisturbed to serve as a buffer between the park and adjacent property owners.

Labels in diagram: STREET, PLAYGROUND, PARKING, COURT, TRAIL, BASEBALL FIELD, SOCCER FIELD.

7-15 ACRES

TYPICAL NEIGHBORHOOD PARK

Specific standards/criteria for developing neighborhood parks are as follows:

| | |
|--|--|
| Service area: ¼ to 1-mile radius to serve walking | Acreage/Population Ratio: 2 acres per 1,000 persons |
| Desirable Size: 7-15 acres | |
| Typical Facilities: Softball or baseball field Half basketball courts Picnic shelter with grill Benches or bench swings | Multipurpose field 50% of site to remain undeveloped Picnic tables with grills (not under shelter) Playground |
| *Alternate Facilities: Nature trails * Alternate facilities may be added or substituted. | Tennis courts |

Harper Park, located in the southeastern part of Knightdale, is an example of a neighborhood park that offers a variety of recreational facilities for visitors to the park. It is within close walking distance to multiple neighborhoods, shopping, and Knightdale Elementary School.

CONSISTENCY WITH AFFORDABLE KNIGHTDALE PLAN:

Town elected officials and leaders recognized the growing concern for the shortage of housing options available to serve a range of incomes in town and the need to ensure a variety of housing choices and opportunities for the present and future workforce. As a result, an action-oriented plan was developed, and adopted on March 20, 2024 by Town Council, that provides unique, innovative, and tailored policy recommendations for our community.

An essential aspect of Affordable Knightdale is inclusionary growth principles that ensure economic mobility to all Knightdale residents. While housing is an important tool to ensure livability, other variables also contribute. Elements to consider regarding housing include: the character of the neighborhood in which the house is located, the ease of navigation through streets that connect housing to the rest of the town, the amenities and services nearby, and the place of housing in the natural environment. When making planning decisions, mobility, connectivity, amenities, vitality, and opportunity should be the focus in order to ensure Knightdale remains a place for all who want to be here.

The Plan establishes twelve recommendations that encompass a variety of methods, approaches, programs, and policy goals and fit under four main categories: Funding, Process, Subsidy, and Demonstration.

One item to note related to the recently updated Water Allocation Policy, developments can now receive Bonus Points for implementing recommendations of the Affordable Knightdale Plan. *Staff recommend the Applicant continue to review the recently updated Policy alongside Affordable Knightdale and revise the proposal to better align itself with the recommendations of that Policy.*

Once additional details are provided by the Applicant, this section of the Staff Report will be updated as it relates to consistency with the Affordable Knightdale Plan.



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CONSISTENCY WITH THE COMPREHENSIVE PLAN:

North Carolina General Statute 160D-605 requires that prior to adoption or rejecting any zoning amendment, the governing board shall adopt a statement describing whether its action is consistent with an adopted comprehensive plan and explaining why the board considers the action to be reasonable and in the public interest.

The KnightdaleNext V.2 Comprehensive Plan contains eight guiding principles categories developed by the community. These guiding principles should be embraced by development proposals to enhance quality-of-life and economic prosperity across town. The principles embody the core philosophy and Town vision expressed by the community.

The Guiding Principles are rooted in those presented in the 2018 KnightdaleNext Comprehensive Plan. They were consolidated or refined based on the following:

1. New opportunities for the town not considered during the previous comprehensive plan update
2. The need for more precise preference statements to influence intended outcomes, or
3. Comments received from residents during the planning process to update the comprehensive plan.

Of the eight guiding principles detailed in the Comprehensive Plan, the following could apply to the proposal; however, additional information, details, and Master Plan revisions are recommended to better align the proposal with the Comprehensive Plan, including, but not limited to the Guiding Principles.



Home and Neighborhood Choices

Dynamic neighborhoods in Knightdale should be places that provide exceptional quality-of-life, attainable and diverse housing options, and connections to neighborhood-serving uses like community gardens, parks, schools, neighborhood commercial areas, or mixed-use activity centers. New neighborhoods should continue to mix two or more home choices in the same development, including "missing middle" home choices such as single dwelling homes on small lots, townhomes, duplexes, triplexes, quadplexes, and accessory dwelling units. Neighborhoods should also organize lots around a continuous and connected system of open space that represents a prominent feature of the development.

Neighborhoods in the town should be treated as the "connective tissue" between destinations, and provide a place for social interaction, and foster connections between residents, that together create a source of pride and belonging in the community. Positive actions, routines, or experiences in a Knightdale neighborhood should lead to a willingness of residents to stay and put down roots in the community.

Residents in neighborhoods provide regular and loyal customers for nearby businesses and should be connected to them in meaningful ways. Physical connections may include new street connections or greenways that strengthen the relationship between origin and destination. Visual connections may include special paving treatments, street lighting, landscaping, or coordinated signage that reinforce a connection between homes and businesses. Social connections may include district names or special events held to present the activity center and nearby neighborhoods as a unified and desirable location.



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Multi-Modal Transportation System

Provide a transportation system that is safe and reliable for moving people and goods throughout the community by vehicle, bus, bicycle, or walking. Equal emphasis on land use (demand), transportation (supply), and design (character) topics in the town's transportation and land use decision-making processes should improve the overall efficiency of the multi-modal transportation system. Build a transportation system that favors walkable, complete streets; mobility hubs for bringing travel modes together; and place-making principles important to creating notable neighborhoods, districts, and activity centers throughout the community. Recognize the special needs of children, seniors, runners, bikers, and families when planning for an effective transportation system.

Work with the town's transportation partners to build a more efficient and multimodal regional transportation system.



Community Facilities and Services

Continue to improve quality-of-life for all residents of Knightdale by maintaining and expanding facilities and services to meet current and future demands — including water, sewer, roads, parks, police, and fire — while ensuring elected officials are good stewards in allocating finances toward town-provided services that promote compact development patterns and a high quality-of-life for town residents.

Prioritize infrastructure investments that support more compact and efficient development patterns presented in the General Growth Framework (Chapter 3), and maintain a strong partnership with outside service providers to ensure infrastructure capacity is available to support intended growth and development depicted in the Plan's three-map series.

X. STAFF RECOMMENDATION:

- Hold a joint public hearing;
- Following public comment, close the public hearing; and
- Refer case ZMA-1-24 to the April 14, 2025, Land Use Review Board for review and recommendation.

Project Overview**#1215031****Project Title:** Olde Faison Place**Jurisdiction:** Town of Knightdale**Application Type:** Planned Unit Development (Full)**State:** NC**Workflow:** PUD: Town Council (Step 3)**County:** Wake**Project Contacts****Contact Info: Applicant**

Stephen Ballentine

353 E Six Forks Rd, Ste 230

Raleigh, NC 27609

P:9195941626

sballentine@riversandassociates.com**Contact Info: Property Owner**

Tom Quackenbush

Ashbury and the Holding Company, LLC

PO Box 90427

Raleigh, NC 27675-0427

P:919-741-7993

tom@glenwoodhomes.com**Contact Info: Developer**

Tom Quackenbush

Ashbury and the Holding Company, LLC

PO Box 90427

Raleigh, NC 27675-0427

P:919-741-7993

tom@glenwoodhomes.com**Contact Info: Engineer**

Stephen Ballentine

353 E Six Forks Rd, Ste 230

Raleigh, NC 27609

P:9195941626

sballentine@riversandassociates.com**Please indicate which of the following contacts should be included in this project.:** Developer,Engineer**Project Location****Applications must include accurate property information. If unsure of the Parcel Identification Number (PIN), please use [Wake County iMAPS](#).****Project Address:** VACANT LAND- NOT ASSIGNED**Parcel Identification Number (PIN):** 1743466606,
1743463116**Total Acreage:** 21.15**Project Description**

Brief Description of Project:

Planned Community on a 21.15 ac tract on the north side of Old Faison Road with a mix of single-family homes and townhomes. The community is served by public streets with sidewalks on both sides, preserved open space, pond with fountain, amenities including recreational field, playground, tennis and pickleball courts, picnic shelter, dog park and walking trails.

Project Location Data

Inside Corporate Limits: No

Current Land Use: Vacant/Undeveloped

Proposed Land Use (General): Residential

Current Zoning: GR8 - General Residential 8, GR8 PUD

Residential Information

Does this project include a residential component?: Yes

Proposed Density: 5.58

Number of Townhome Lots: 106

Number of Detached Single Family Lots: 12

Total Number of Proposed Residential Lots: 118

Number of Multi-Family/Apartments Units: 0

Number of alley loaded lots: 118

Number of front loaded lots: 0

Please Note: Knightdale's Unified Development Ordinance has unique regulations pertaining to mass grading and landscape preservation. Please consult the UDO regulations below to ensure your proposed project meets the requirements.

As outlined in **UDO Chapter 9.3: Residential Clearing & Grading**, mass grading is prohibited except where residential lots are less than 60 feet in width.

As outlined in **UDO Chapter 7.4.H: Tree Protection**, there are requirements for landscape preservation, tree save areas, protection measures, and replacement specifications.

Non-Residential Information

Does this project include a non-residential component?: No **Total Number of Proposed Non-Residential Lots:**

Total Number of Non-Residential Buildings:

If your project contains more than three buildings, please upload total proposed square footage information as part of your project documents.

Building #1 Square Footage:

Building #2 Square Footage:

Building #3 Square Footage:

Please Note: Knightdale's Unified Development Ordinance has unique regulations pertaining to landscape preservation. Please consult the UDO regulations below to ensure your proposed project meets the requirements.

As outlined in **UDO Chapter 7.4.H: Tree Protection**, there are requirements for landscape preservation, tree save areas, protection measures, and replacement specifications.

Utility & Stormwater Information

Proposed Water Supply: Public or Municipal

Description of Public Utility Connection (Water Supply):

Will connect to City of Raleigh 16" DIP in Old Faison Road

Proposed Wastewater Supply: Public or Municipal

Description of Public Utility Connection (Wastewater Supply):

Connection to City of Raleigh sewer system at existing manhole north east of the property via a proposed sewer easement on adjacent property. See Sketch Plan Sheet S2.

How will stormwater management be addressed?: New System or Device

New Device Type: Wet Pond

Does the proposed development anticipate the addition or extension of public streets?: Yes

Does the parcel(s) contain any of the following environmental/natural features?: Stream Buffers, Body of Water

Neighborhood Meeting Information

Please Note: A full Neighborhood Meeting Report is a required element of this application. The questions below must be answered and are not a substitution for providing a full report, as outlined in the application checklist.

Date of Mailed Notification: 10/02/2024

Date of Neighborhood Meeting: 10/15/2024

Number of Property Owners Contacted: 82

Number of Residents in Attendance: 9

Provide a brief summary of the meeting, including any issues or concerns that were discussed.:

Please refer to the meeting notes document, which describes what was discussed at the meeting.

Comprehensive Plan Consistency

Please Note: A standalone PUD document is a required application element. The questions below must be answered and are not a substitution for providing a PUD document, as outlined in the application checklist.

Current 2035 Comprehensive Plan Placetype Designation:
Mixed-Use Neighborhood

Provide brief statements regarding whether the rezoning request is consistent with the Growth Framework designation, Growth & Conservation Placetype, and any applicable policies contained within the 2035 Comprehensive Plan.:

This project does not include retail nor commercial components, and is therefore not a mixed use project. This is a residential developer. We do not believe there is demand for a commercial nor retail development at this location, and any investment in those uses would likely not succeed at this location. This project qualifies as a "mixed density neighborhood" per the UDO. Please refer to the PUD document for additional information.

Provide brief statements explaining how the rezoning request is reasonable and in the public interest.:

This rezoning request is compatible with the existing GR8 zoning of the property, and with the adjacent townhouse and single family developments surrounding the property. As indicated in the Comprehensive Plan, there is a shortage of townhouse developments in the Town of Knightdale. Only 6% of all residences in Knightdale are townhouses. This project will help the Town to achieve a more balanced mix of townhouses. This project includes multiple amenities and open spaces, which will benefit the community.

UDO Consistency

Please Note: A standalone PUD document is a required application element. The questions below must be answered and are not a substitution for providing a PUD document, as outlined in the application checklist.

Proposed Base Zoning District: GR8 - General Residential 8

Explain how the project is consistent with the Unified Development Ordinance:

This project has been designed to comply with the Town of Knightdale Unified Development Ordinance, including but not limited to the zoning standards, allowable uses, buffers, landscaping, stormwater, utilities, lighting, roadway, parking, and open space requirements.

Provide any proposed modifications to UDO standards:

This project is not proposing any modification to the UDO. However, this project is proposing the following, which can be approved under the PUD classification. The Town has indicated that they are in the process of changing their road cross section standards. The proposed road cross sections differ slightly from the current UDO, in the following ways: The shoulders contain narrower sidewalk pavement than the "urban street" section in the UDO and more grass. The roads contain more parallel parking spaces instead of bike lanes, and less median islands due to the proximity of intersections and driveways. The reduced shoulder impervious is a benefit to the environment and stormwater runoff.

Water Allocation Policy Compliance

Please Note: Connection to municipal water requires compliance with the [Water Allocation Policy](#). Projects must be awarded 50 points or more to merit water allocation. Specific information regarding policy compliance must be included in the Master Plan submittal.

Land Use Category: 50 pts - Change of Use

Base Points: 50

Bonus Points: 0

Total Points Proposed: 50

**TOWN OF KNIGHTDALE
OWNER & AGENT AUTHORIZATION FORM**

Application or

Project #: _____

Submittal Date: _____

I, _____ am the owner of the following property/properties,
located within in the Town of Knightdale:

_____.

I do hereby give permission to _____ to submit the
following type of application to the Town of Knightdale on my behalf for the above
referenced property/properties.

Master Plan (By Right)
General Rezoning or Conditional District
Special Use Permit
Planned Unit Development
Construction Drawings
Variance/Appeal
Other:

I am the owner of the property and will be acting as my own agent.

Signature(s) of Owner(s):



Print or type name(s):

Attach additional sheets if there are additional owners.

***If ownership has been recently transferred, a copy of the deed must accompany this authorization.**

OLD FAISON PLACE

MASTER PLAN
OLD FAISON RD, KNIGHTDALE, WAKE COUNTY, NC 27545
MASTER PLAN SUBMITTAL #1 10/21/2024
MASTER PLAN SUBMITTAL #2 2/5/2025

TOWN OF KNIGHTDALE PROJECT NUMBER: ZMA-1-24

DEVELOPER/ APPLICANT

ASHBURY & THE HOLDING
COMPANY L.L.C.
RALEIGH, NC

PO BOX 90427
RALEIGH, NC 27675-0427
(919) 741-7993

Contact: TOM QUACKENBUSH
TOM@GLENWOODHOMES.COM

CIVIL ENGINEER

Rivers & Associates, Inc.
Raleigh, NC

353 E. Six Forks Rd.
Raleigh, NC 27609
(919) 594-1626

Contact: Stephen Ballentine, PE
sballentine@riversandassociates.com

SURVEYOR

RESIDENTIAL LAND
SERVICES, PLLC.
Cary, NC

1917 EVANS ROAD
CARY NC 27513
(919) 378-9316

Contact: Dean M. Rhoads
dean@rls-nc.com

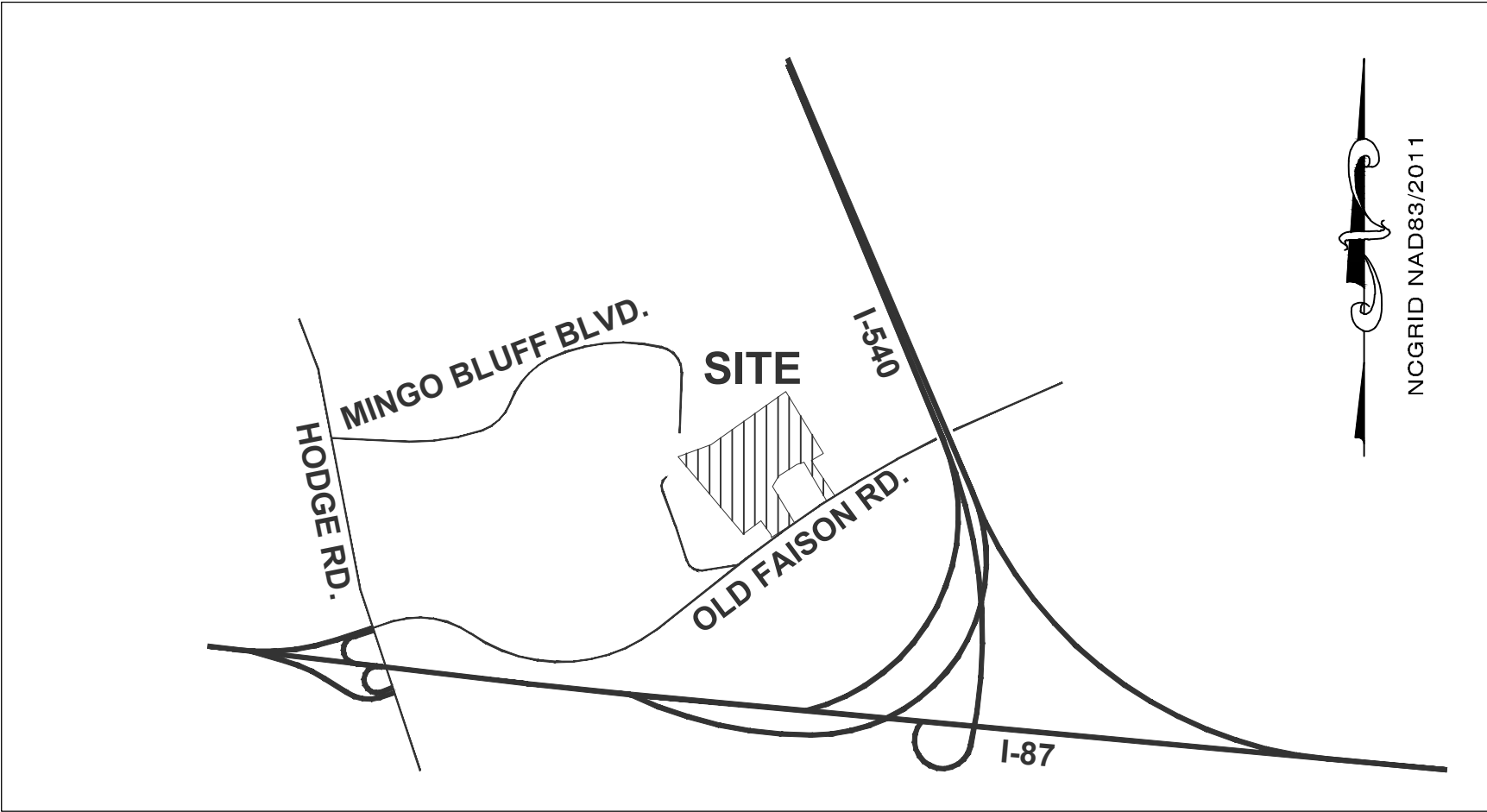
LAND USE ATTORNEY

MATTOX LAW FIRM
Raleigh, NC

127 W HARGETT STREET STE #500
RALEIGH, NC 27601
(919) 828-7171

Contact: Isabel Mattox
isabel@mattoxlawfirm.com

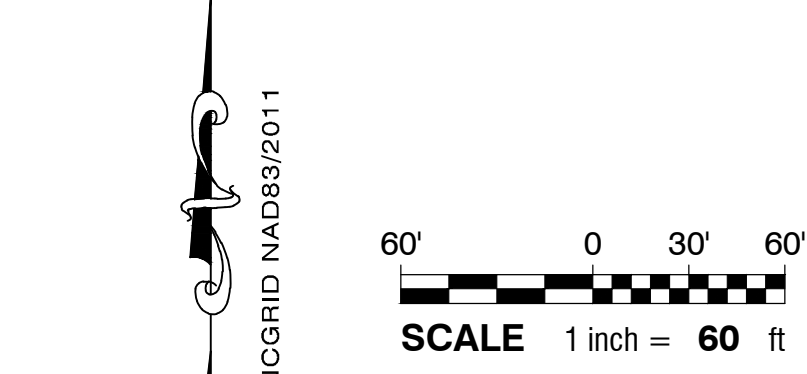
RALEIGH WATER CONDITIONS OF APPROVAL:
*ALL WATER, SANITARY SEWER AND REUSE FACILITIES SHALL BE INSTALLED, INSPECTED, TESTED, AND ACCEPTED BY THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT FOR OPERATIONS AND MAINTENANCE PRIOR TO 1ST CO
*A PETITION FOR ANNEXATION INTO THE CITY LIMITS SHALL BE SUBMITTED IN ACCORDANCE WITH CITY COUNCIL POLICY FOR EXTENSION OF UTILITY SERVICE TO PROPERTIES CURRENTLY OUTSIDE OF THE CITY LIMITS PRIOR TO PLAT RECORDATION
*CONSTRUCTION DRAWINGS MUST BE APPROVED BY THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT FOR ALL PUBLIC WATER, PUBLIC SEWER AND/OR PRIVATE SEWER EXTENSIONS PRIOR TO PLAT RECORDATION.
*A PLAT MUST BE RECORDED AT THE WAKE COUNTY REGISTER OF DEEDS OFFICE FOR ALL UTILITY EASEMENT DEDICATIONS PRIOR TO BUILDING PERMIT ISSUANCE
*A DOWNSTREAM SEWER CAPACITY STUDY IN COMPLIANCE WITH CORPUD HANDBOOK SHALL BE SUBMITTED BY THE PROJECT ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION DRAWING APPROVAL
*A DEED SHALL BE RECORDED PRIOR TO CONSTRUCTION DRAWING APPROVAL FOR ALL NECESSARY OFFSITE CITY OF RALEIGH SANITARY SEWER EASEMENTS TO ACCOMMODATE FUTURE SEWER LINE CONSTRUCTION



VICINITY MAP
N.T.S.

| Sheet List Table | |
|------------------|--|
| Sheet Number | Sheet Title |
| C1.01 | COVER SHEET |
| C1.21 | EXISTING CONDITIONS |
| C2.01 | SITE PLAN |
| C2.11 | OPEN SPACE PLAN |
| C3.01 | PRELIMINARY UTILITY PLAN |
| C4.41 | PRELIMINARY STORMWATER MANAGEMENT PLAN |
| C6.31 | SIGNS & PAVEMENT MARKING PLAN |
| L1.01 | LANDSCAPE PLAN |
| SL1.01 | PRELIMINARY LIGHTING PLAN |
| -- | ELEVATIONS SINGLE FAMILY |
| -- | ELEVATIONS TOWNHOUSES |

| SITE DATA | |
|---|--|
| Subdivision Name | Old Faison Place |
| PIN #S | 1743463116 / 1743466606 |
| Property Area | 20.90 Acres |
| Current Zoning | GR8 (Medium Density) |
| Proposed Zoning | GR8 PUD |
| Adjacent Zoning | North- UR12 (Urban Residential 12) Townhomes South- RT (Rural Transition) Single Family Residential East- GR8 (Single Family Residential) West- RT (Rural Transition) Single Family Residential |
| River Basin | Nause |
| Watershed | Property is not within a water supply watershed |
| Land Use: | |
| Current | Vacant |
| Proposed | Residential- Single-Family and Townhomes |
| Dwelling Units: | |
| Single Family | 12 |
| Townhomes | 105 |
| Total DU's | 117 |
| Proposed Density | 5.6 DU/AC Prior to R/W Dedication |
| NOTES: Survey performed by Residential Land Services, PLLC Description of existing natural resources: • There are no wetlands onsite. There are some offsite wetlands to the east on the neighbor's property. • There is an existing pond in the middle of the site, which will be removed. • There is a riparian stream buffer at the east edge of the property. Description of Stormwater Management: • This project includes one proposed wet detention pond in the northeast corner of the property as shown. Stormwater discharges towards the northeast. • Offsite stormwater enters the property from the north, west, and south. • This project will retain the pre-existing drainage patterns. | |



NCGRID NAD83/2011

NOTES:
STREAMS, WETLANDS AND RIPARIAN BUFFERS TAKEN FROM MAP
PROVIDED BY SAGE ECOLOGICAL SERVICES DATED AUG 10, 2023

THIS SHEET IS NOT A SURVEY. THE PROFESSIONAL ENGINEER IS NOT SEALING A SURVEY. CONTACT RESIDENTIAL LAND SERVICES, PLLC TO OBTAIN THEIR SEALED SURVEY.

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SHSBURY & THE HOLDING COMPANY, LLC
OLD FAISON PLACE
CITY OF KNIGHTDALE - WAKE COUNTY - NORTH CAROLINA

EXISTING CONDITIONS

DAISON PLACED
— NIGHTDALE — WAKE COUNTY — NORTH CAROLINA —
EXISTING CONDITIONS

| | |
|--------------|----------|
| DATE: | |
| DESIGNED BY: | C |
| DRAWN BY: | TA |
| CHECKED BY: | SJ |
| PROJECT No. | 202211 |
| DRAWING No. | W-410 |
| SCALE: | AS SHOWN |
| SHEET No. | |

C1.21

| REVISIONS: | | | |
|------------|--|--------------------------|----------|
| | | MASTER PLAN SUBMITTAL #2 | 02/05/25 |
| | | MASTER PLAN SUBMITTAL #1 | 10/21/24 |
| | | SKETCH PLAN SUBMITTAL #2 | 1/17/24 |
| | | SKETCH PLAN SUBMITTAL #1 | 5/22/23 |

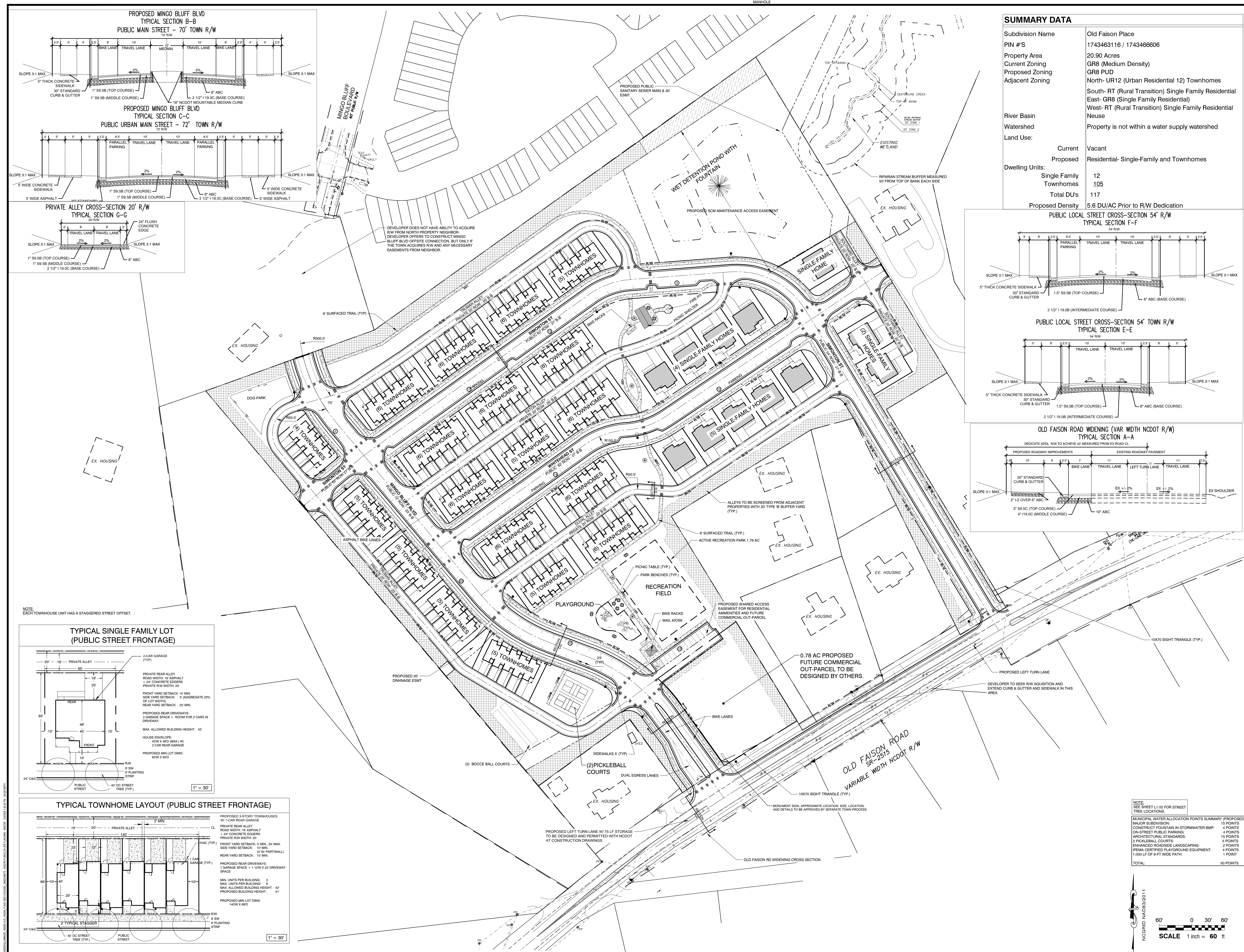
| REVISIONS: | | | |
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| | | MASTER PLAN SUBMITTAL #2 | 02/05/25 |
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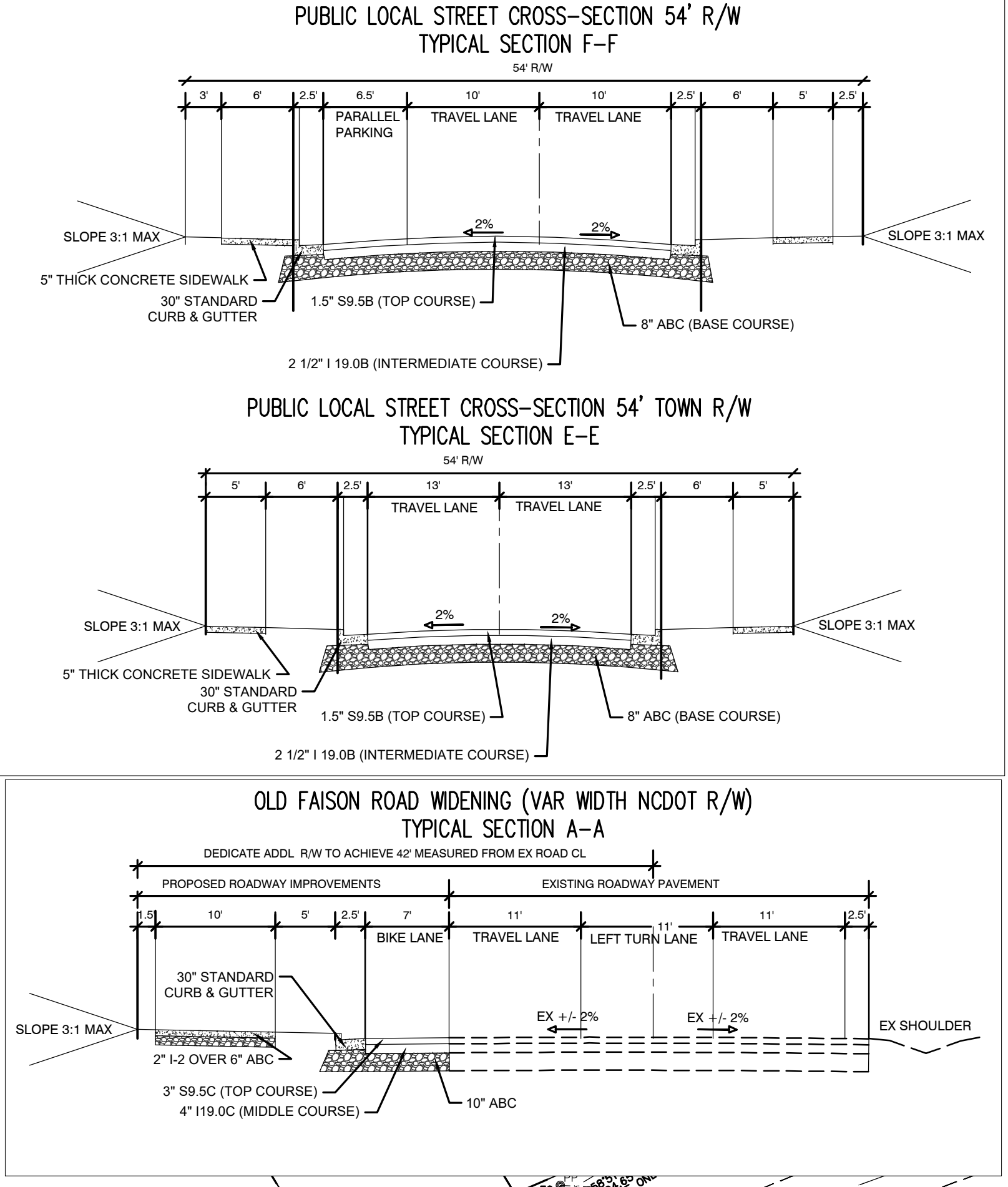
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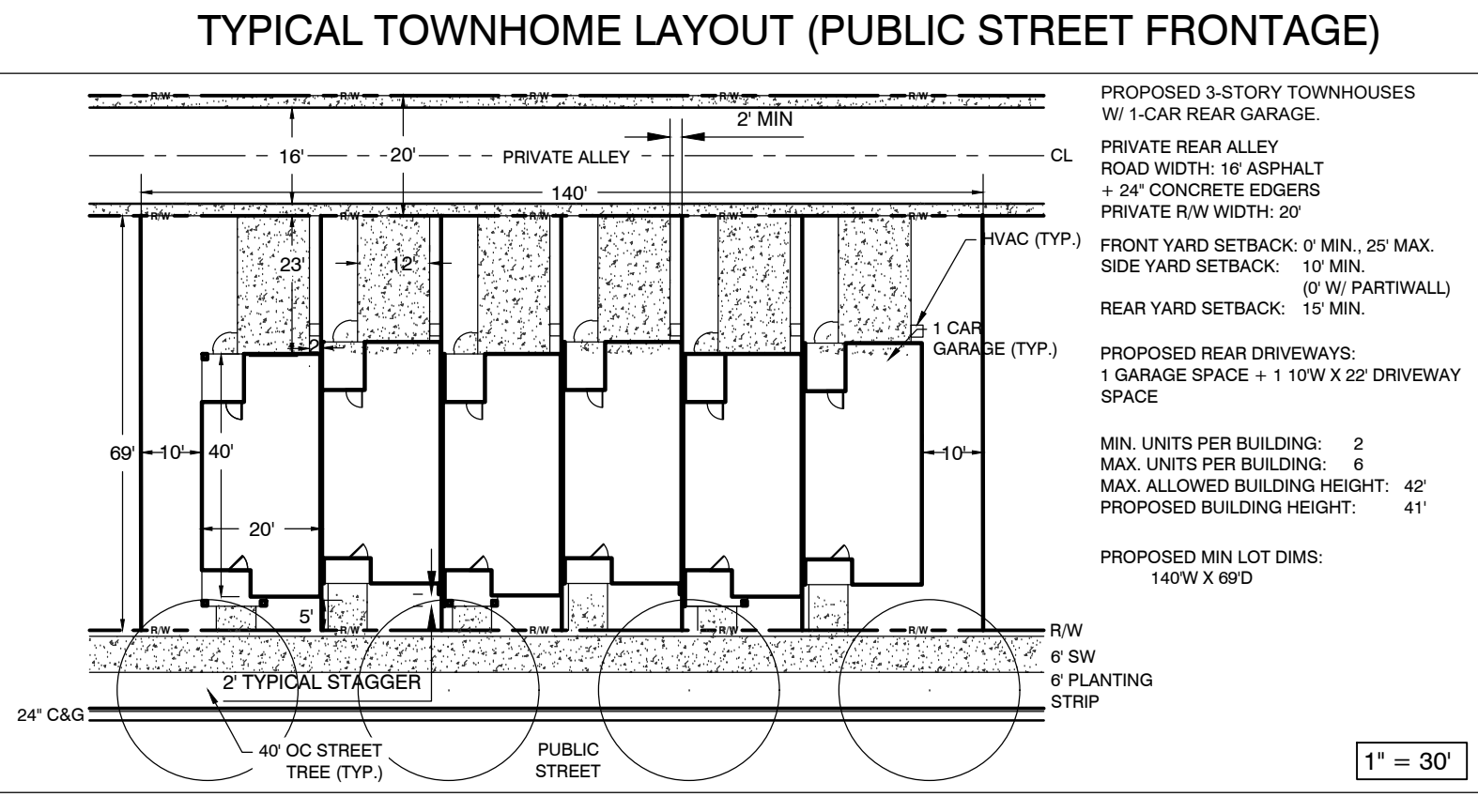
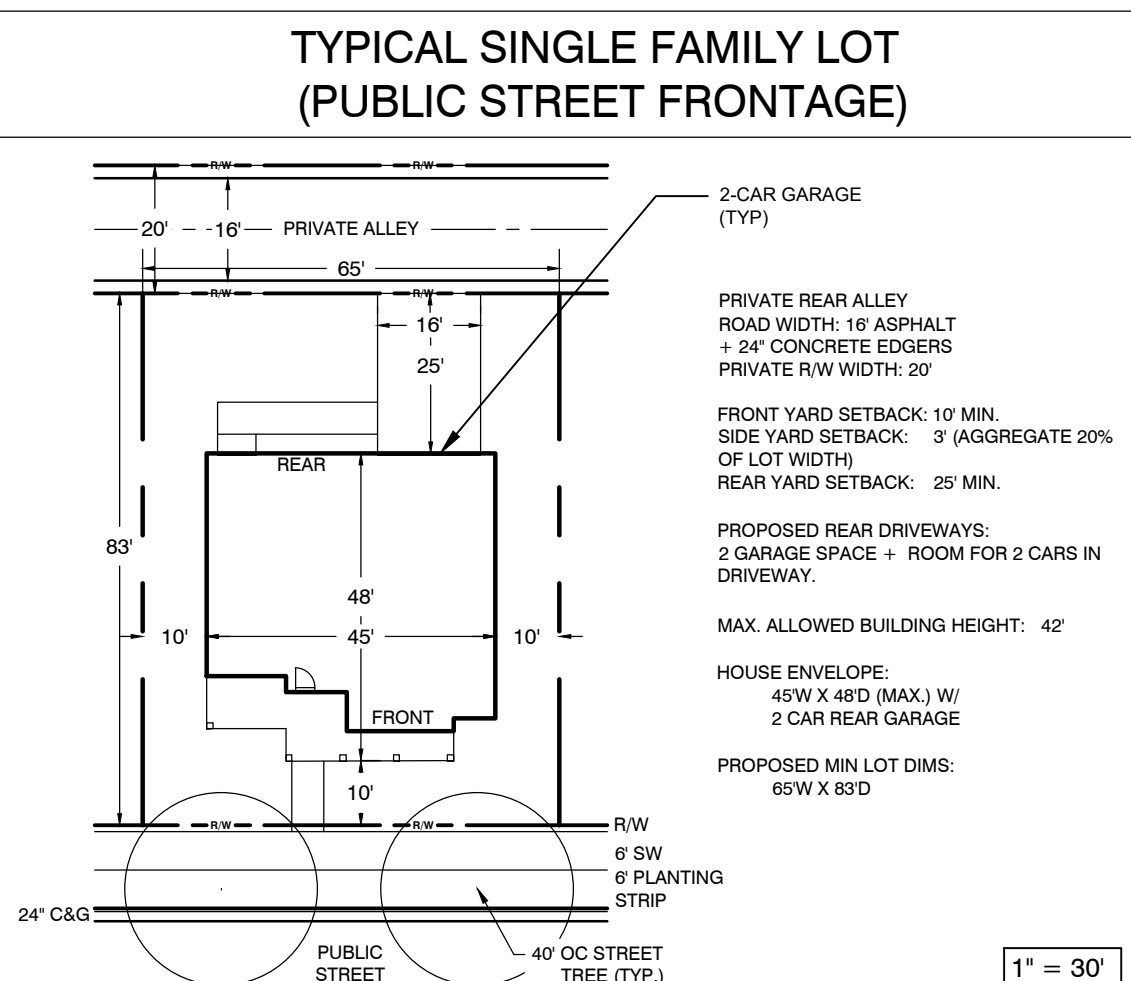
Six Forks Place III Ste. 230
553 E. Six Forks Road
Raleigh, NC 27609
919.594.1626



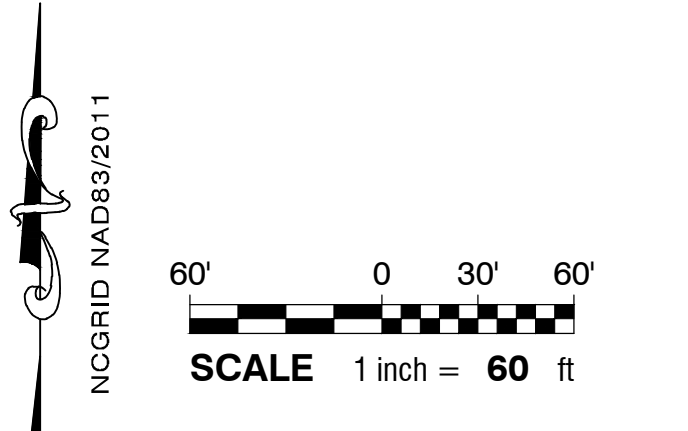
| SUMMARY DATA | |
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| Adjacent Zoning | North- UR12 (Urban Residential 12) Townhomes South- RT (Rural Transition) Single Family Residential East- GR8 (Single Family Residential) West- RT (Rural Transition) Single Family Residential |
| River Basin | Neuse |
| Watershed | Property is not within a water supply watershed |
| Land Use: | Current Vacant Proposed Residential- Single-Family and Townhomes |
| Dwelling Units: | Single Family 12 Townhomes 105 Total DU's 117 |
| Proposed Density | 5.6 DU/AC Prior to R/W Dedication |



NOTE:
EACH TOWNHOUSE UNIT HAS A STAGGERED STREET OFFSET.



| MUNICIPAL WATER ALLOCATION POINTS SUMMARY (PROPOSED) | |
|--|-----------|
| MAJOR SUBDIVISION: | 15 POINTS |
| CONSTRUCT FOUNTAIN IN STORMWATER BMP: | 4 POINTS |
| ON-STREET PUBLIC PARKING: | 4 POINTS |
| ARCHITECTURAL STANDARDS: | 15 POINTS |
| PICKLEBALL COURTS: | 5 POINTS |
| ENHANCED ROADSIDE LANDSCAPING: | 2 POINTS |
| IPEMA CERTIFIED PLAYGROUND EQUIPMENT: | 4 POINTS |
| 1,000 LF OF 6-FT WIDE PATH: | 1 POINT |
| TOTAL: | 50 POINTS |



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| REVISIONS: | | NO. | DATE | BY |
|--------------------------|---------|-----|------|----|
| MASTER PLAN SUBMITTAL #2 | 10/2/25 | | | |
| MASTER PLAN SUBMITTAL #1 | 10/2/24 | | | |
| SKETCH PLAN SUBMITTAL #2 | 1/17/24 | | | |
| SKETCH PLAN SUBMITTAL #1 | 5/22/23 | | | |

ASHBURY & THE HOLDING COMPANY, LLC

OLD FAISON PLACE

CITY OF KNIGHTDALE - WAKE COUNTY - NORTH CAROLINA

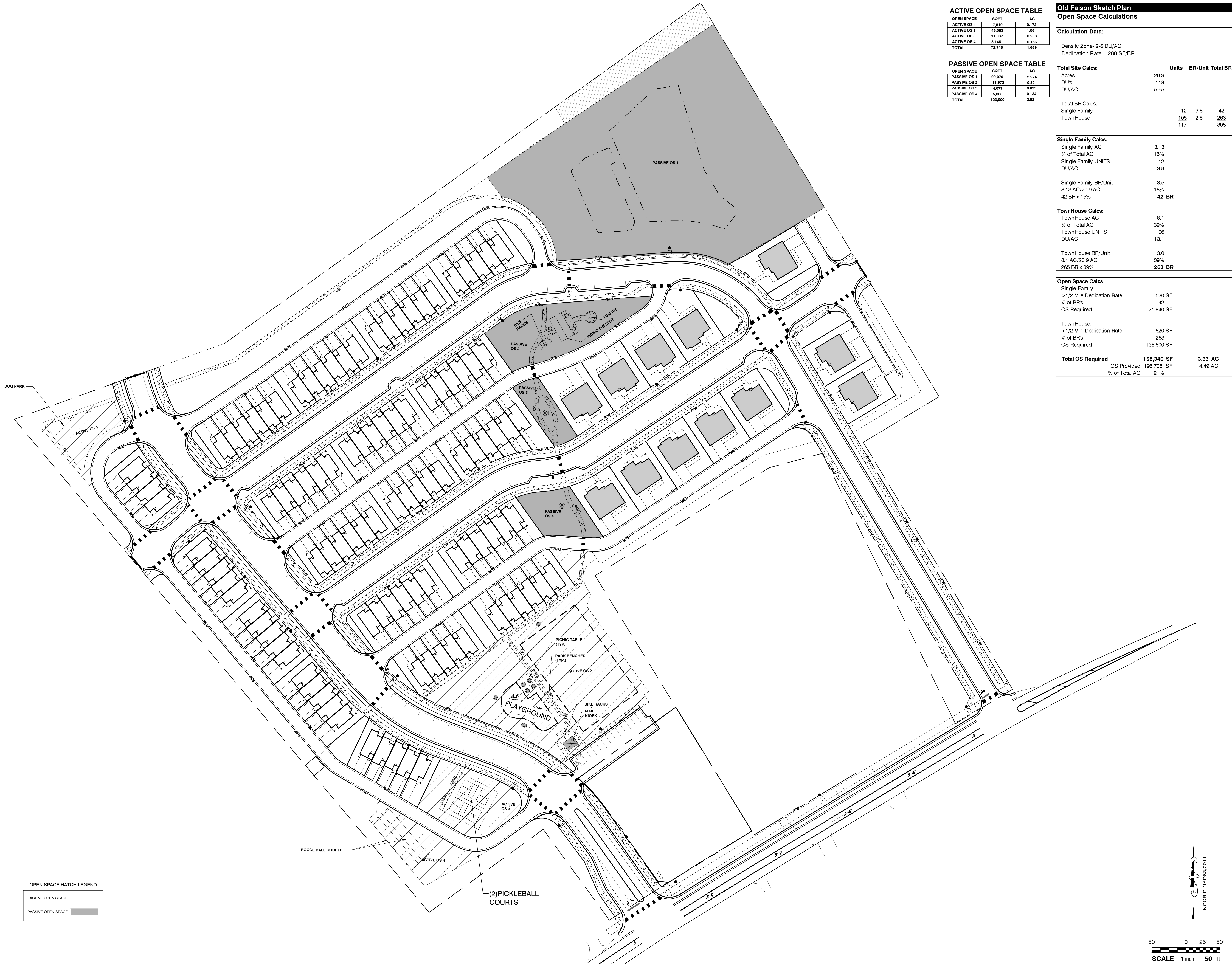
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| DESIGNED BY: | CS |
| DRAWN BY: | TAH |
| CHECKED BY: | SJB |
| PROJECT No: | 2022113 |
| DRAWING No: | W-4104 |
| SCALE: | AS SHOWN |
| SHEET No: | |

C2.01

DATE:

Do Not Use for Construction - PRELIMINARY

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ACTIVE OPEN SPACE TABLE

| OPEN SPACE | SQFT | AC |
|-------------|--------|-------|
| ACTIVE OS 1 | 7,510 | 0.172 |
| ACTIVE OS 2 | 46,053 | 1.06 |
| ACTIVE OS 3 | 11,037 | 0.253 |
| ACTIVE OS 4 | 8,145 | 0.185 |
| TOTAL | 72,745 | 1.569 |

PASSIVE OPEN SPACE TABLE

| OPEN SPACE | SQFT | AC |
|--------------|---------|-------|
| PASSIVE OS 1 | 99,079 | 2.274 |
| PASSIVE OS 2 | 13,972 | 0.32 |
| PASSIVE OS 3 | 4,077 | 0.093 |
| PASSIVE OS 4 | 5,833 | 0.134 |
| TOTAL | 123,000 | 2.82 |

Old Faison Sketch Plan

Open Space Calculations

Calculation Data:

Density Zone- 2-6 DU/AC
Dedication Rate= 260 SF/BR

| Total Site Calcs: | Units | BR/Unit | Total BR |
|-------------------|-------|---------|----------|
| Acres | 20.9 | | |
| DUs | 118 | | |
| DU/AC | 5.65 | | |
| Total BR Calcs: | | | |
| Single Family | 12 | 3.5 | 42 |
| TownHouse | 105 | 2.5 | 263 |
| | 117 | | 305 |

Single Family Calcs:

| | |
|---------------------|------|
| Single Family AC | 3.13 |
| % of Total AC | 15% |
| Single Family UNITS | 12 |
| DU/AC | 3.8 |

| | |
|-----------------------|-------|
| Single Family BR/Unit | 3.5 |
| 3.13 AC/20.9 AC | 15% |
| 42 BR x 15% | 42 BR |

TownHouse Calcs:

| | |
|-----------------|------|
| TownHouse AC | 8.1 |
| % of Total AC | 39% |
| TownHouse UNITS | 106 |
| DU/AC | 13.1 |

| | |
|-------------------|--------|
| TownHouse BR/Unit | 3.0 |
| 8.1 AC/20.9 AC | 39% |
| 265 BR x 39% | 263 BR |

Open Space Calcs

| | |
|----------------------------|-----------|
| Single-Family: | |
| >1/2 Mile Dedication Rate: | 520 SF |
| # of BRs | 42 |
| OS Required | 21,840 SF |

| | |
|----------------------------|------------|
| TownHouse: | |
| >1/2 Mile Dedication Rate: | 520 SF |
| # of BRs | 263 |
| OS Required | 136,500 SF |

| | | |
|-------------------|------------|---------|
| Total OS Required | 158,340 SF | 3.63 AC |
| OS Provided | 195,706 SF | 4.49 AC |
| % of Total AC | 21% | |

OPEN SPACE HATCH LEGEND

| | |
|--------------------|--|
| ACTIVE OPEN SPACE | |
| PASSIVE OPEN SPACE | |

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| REVISIONS: | | | |
|------------|--------------------------|----------|----|
| NO | DESCRIPTION | DATE | BY |
| 1 | MASTER PLAN SUBMITTAL #2 | 10/20/21 | |
| 2 | MASTER PLAN SUBMITTAL #1 | 10/21/21 | |
| 3 | SKETCH PLAN SUBMITTAL #2 | 1/17/24 | |
| 4 | SKETCH PLAN SUBMITTAL #1 | 5/22/23 | |

ASHBURY & THE HOLDING COMPANY, LLC
OLD FAISON PLACE
CITY OF KNIGHTDALE - WAKE COUNTY - NORTH CAROLINA
OPEN SPACE PLAN

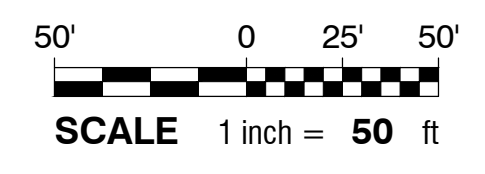
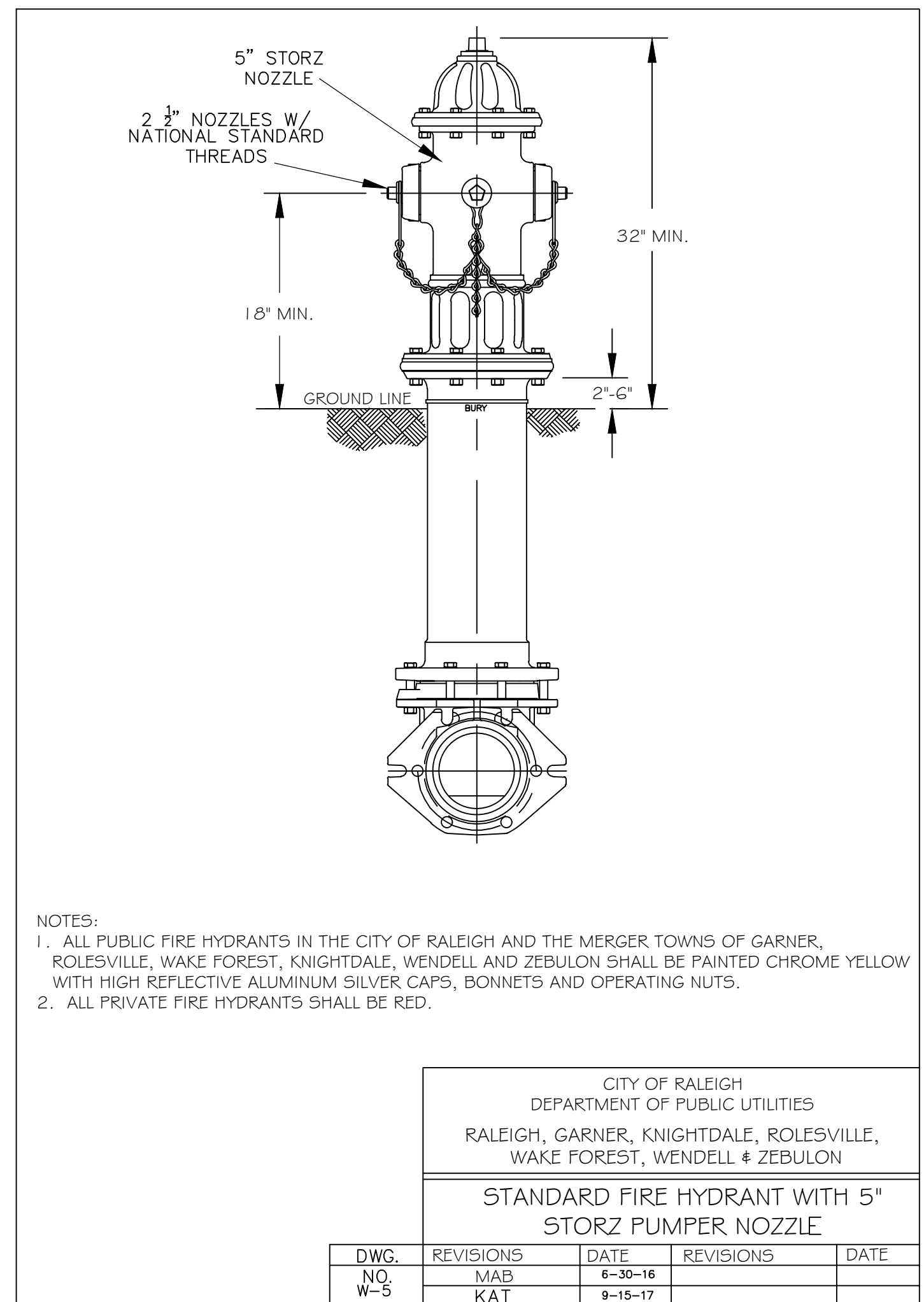
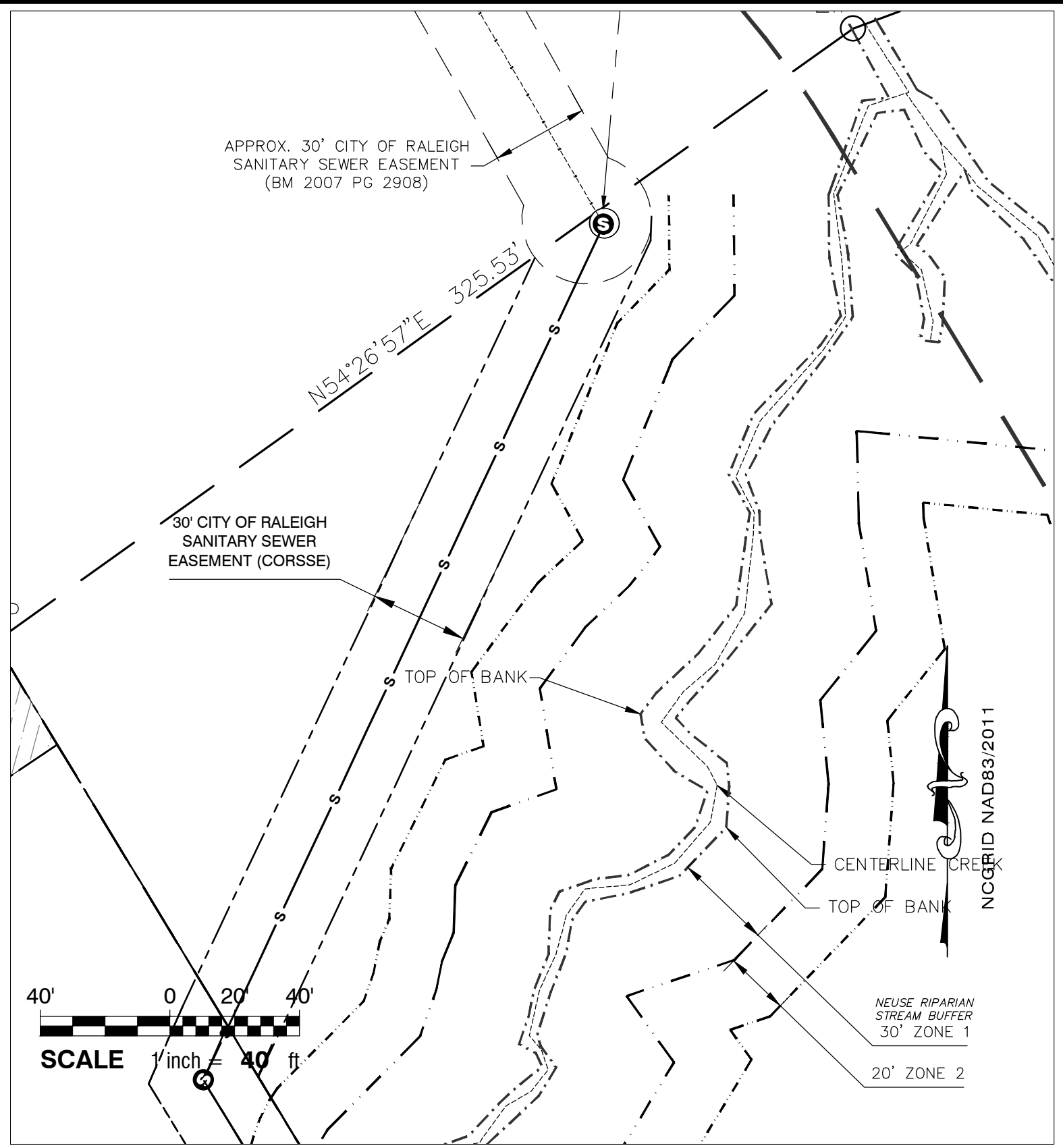
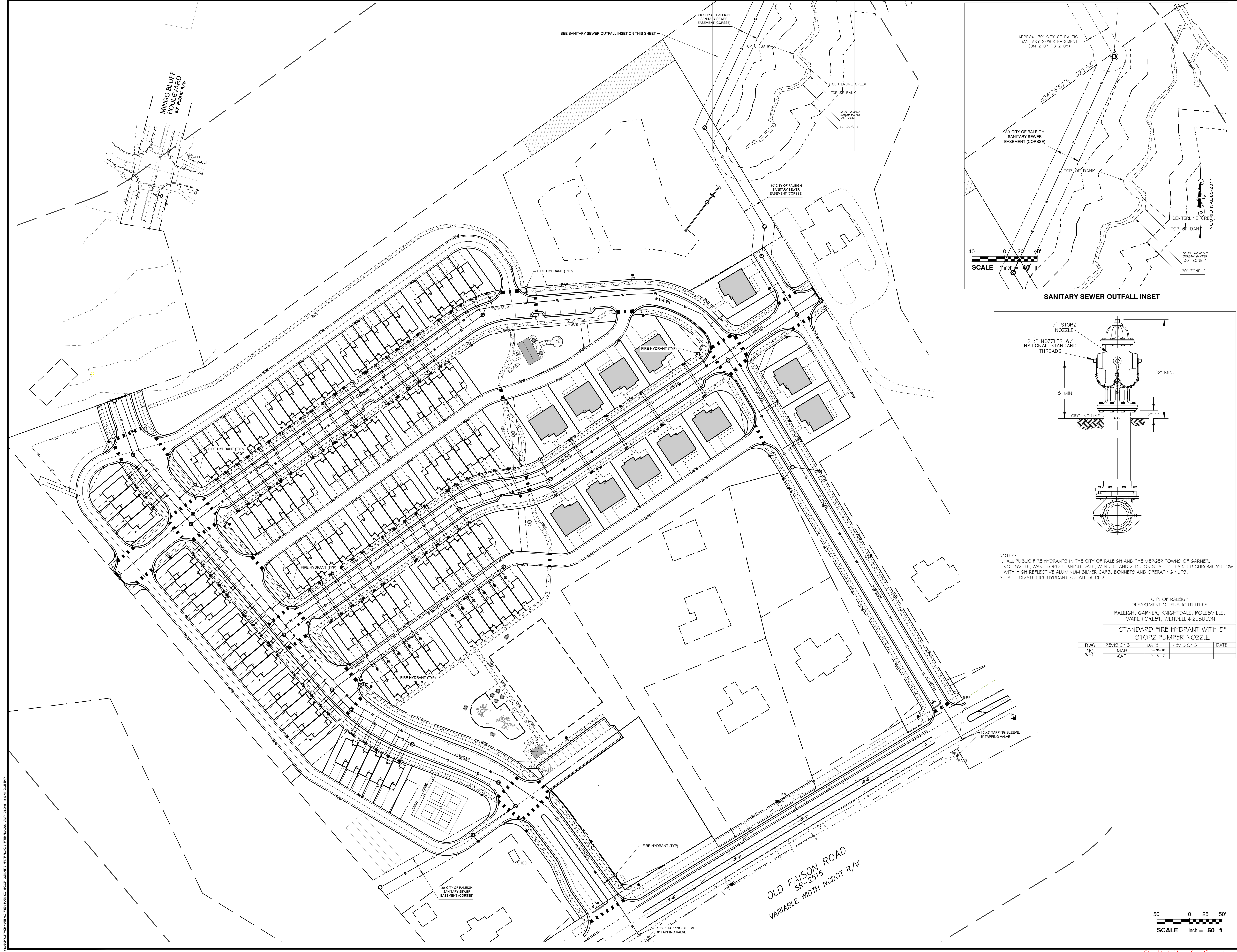
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| DATE: | |
| DESIGNED BY: | CS |
| DRAWN BY: | TAH |
| CHECKED BY: | SJB |
| PROJECT No: | 2022113 |
| DRAWING No: | W-4104 |
| SCALE: | AS SHOWN |
| SHEET No: | |

C2.11

50' 0 25' 50'
SCALE 1 inch = 50 ft

NGRID NAD83/2011

Do Not Use for Construction - PRELIMINARY



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919.594.1626

| REVISIONS: | | | |
|------------|--------------------------|----------|----|
| NO. | DESCRIPTION | DATE | BY |
| 1 | MASTER PLAN SUBMITTAL #2 | 10/20/23 | |
| 2 | MASTER PLAN SUBMITTAL #1 | 10/21/24 | |
| 3 | SKETCH PLAN SUBMITTAL #2 | 1/17/24 | |
| 4 | SKETCH PLAN SUBMITTAL #1 | 5/22/23 | |

ASHBURY & THE HOLDING COMPANY, LLC

OLD FAISON PLACE

CITY OF KNIGHTDALE — WAKE COUNTY — NORTH CAROLINA

PRELIMINARY UTILITY PLAN

DATE:

DESIGNED BY: CS

DRAWN BY: TAH

CHECKED BY: SJB

PROJECT No: 2022113

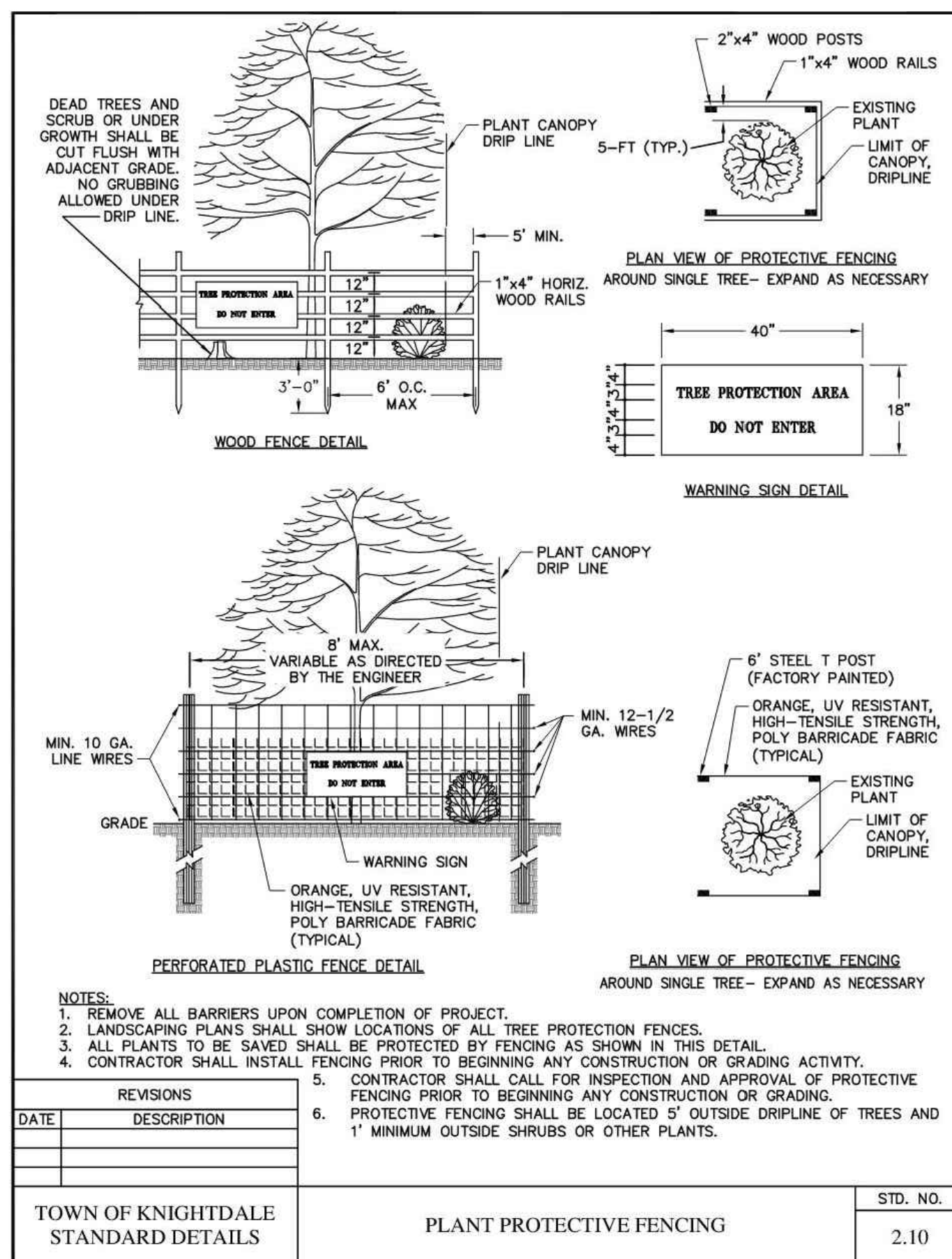
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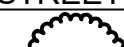


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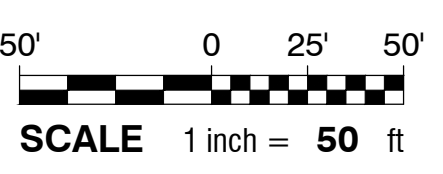
SHEET No:

C3.01

Do Not Use for Construction - PRELIMINARY



| PLANT SCHEDULE | | | | | |
|---|---|-----------|---------|---------------|-------|
| SYMBOL | BOTANICAL / COMMON NAME | CONTAINER | CALIPER | SIZE | QTY |
| STREET TREES | | | | | |
|  | Pistacia chinensis / Chinese Pistache | B & B | 2" CAL. | 9'-11' HT. | +/-48 |
|  | Ulmus parvifolia / Chinese Lacebark Elm | B & B | 2" CAL. | 12' - 14' HT. | +/-80 |
|  | Zelkova serrata / Sawleaf Zelkova | B & B | 2" CAL. | 12' - 14' HT. | +/-38 |



LIGHTING LEGEND

- ROADWAY LED LIGHTS
(6,000 LUMENS LED 75' ROADWAY LIGHT)
ALUMINUM POLE - HEIGHT 30'
- PEDESTRIAN LIGHT POLE
OPEN MONTICELLO LED LIGHTS
(2,000 LUMENS LED 75' ROADWAY LIGHT)
ALUMINUM POLE - HEIGHT 8'

LIGHTING NOTES

- ALL FIXTURES SHALL HAVE A FULL CUTOFF CLASSIFICATION OR BE FULLY SHIELDED.
- ALL POLE-MOUNTED LED SHALL NOT EXCEED 37' IN HEIGHT.
- ALL LIGHT POLES SHALL MEET THE TOWNS MINIMUM STANDARD OF A FIBERGLASS POLE WITH A FULLY-SHIELDED, ARM-MOUNTED LED.
- RESIDENTIAL ROADS SHALL HAVE LIGHT POLES SPACED A MAXIMUM OF 250' AND ARTERIAL STREETS SPACED AT 150' MAXIMUM.
- LOCAL STREETS SHALL MEET 4,000 LUMEN MINIMUM.

THIS PLAN IS PRELIMINARY. FINAL LIGHTING PLAN TO BE DESIGNED BY LIGHTING PROFESSIONAL.

SCALE 1 inch = 50 ft

50' 0 25' 50'

NCGRID MA082/011

— 100 —



- COMPOSITE ROOF SHINGLES
- FIBER CEMENT BOARD AND BATTEN
- VINYL WINDOWS
- FIBER CEMENT LAP SIDING
- BRICK ROWLOCK OVER SOLDIER
- BRICK VENEER

Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 12/12 Roof Pitch Front Gable
- Brick Veneer on Front Elevation
- Mix of Lap & Board & Batten Siding
- Dormer
- Composite Roof Shingles
- Wrapped Porch Columns w/ Brackets

Denmark '1' - Front Elevation

Knightdale, NC

09.26.2024



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Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 12/12 Roof Pitch Front Gable
- Brick Veneer on Front Elevation
- Mix of Lap & Board & Batten Siding
- Composite Roof Shingles
- Wrapped Porch Columns

Denmark '2' - Front Elevation

Knightdale, NC

09.26.2024



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Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 9/12 Roof Pitch Front Gable
- Brick Veneer on Front Elevation
- Mix of Lap & Shingle Siding
- Composite Roof Shingles
- Wrapped Porch Columns

Denmark '3' - Front Elevation

Knightdale, NC

09.26.2024



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Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 12/12 Roof Pitch Front Gable
- Stone Veneer on Front Elevation
- Mix of Lap, Board & Batten, Shingle Siding
- Composite Roof Shingles
- Tapered Porch Columns

Denmark '4' - Front Elevation

Knightdale, NC

09.26.2024



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Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 9/12 Roof Pitch Front Gable
- Brick Veneer on Front Elevation
- Mix of Lap & Shingle Siding
- Composite Roof Shingles
- Wrapped Porch Columns

Denmark '5' - Front Elevation

Knightdale, NC

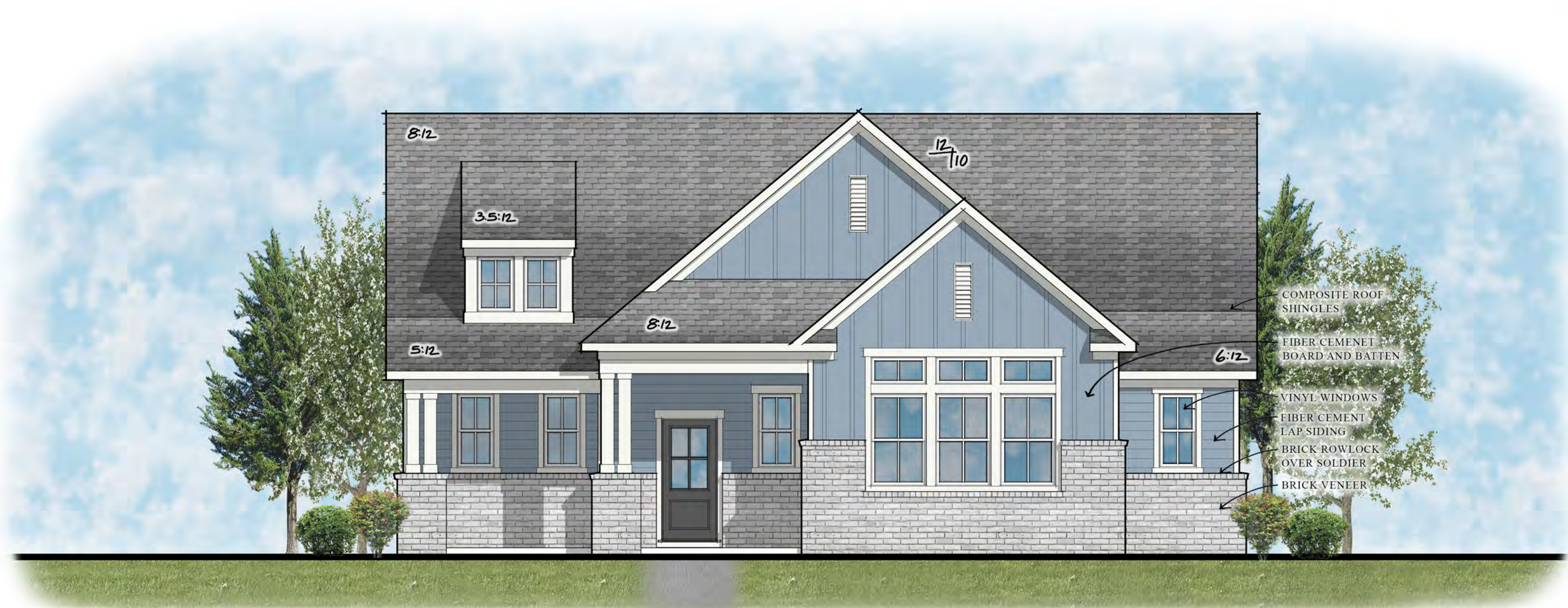
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- COMPOSITE ROOF SHINGLES
- FIBER CEMENT BOARD AND BATTEN
- VINYL WINDOWS
- FIBER CEMENT LAP SIDING
- BRICK ROWLOCK OVER SOLDIER
- BRICK VENEER

Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 10/12 Roof Pitch Front Gable
- Brick Veneer on Front Elevation
- Mix of Lap & Shingle Siding
- Dormer
- Composite Roof Shingles
- Wrapped Porch Columns

Denmark '6' - Front Elevation

Knightdale, NC

09.26.2024



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Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 12/12 Roof Pitch Front Gable
- Brick & Stone Veneer on Front Elevation
- Mix of Lap, Board & Batten & Shingle Siding
- Composite Roof Shingles
- Wrapped Porch Columns



6-Plex - Front Elevation

20' 3-Story Rear Load Townhomes - Knightdale, NC

09.27.2024



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FIBER CEMENT
LAP SIDING



6-Plex - Rear Elevation

20' 3-Story Rear Load Townhomes - Knightdale, NC

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6-Plex - Sides Elevations

20' 3-Story Rear Load Townhomes - Knightdale, NC

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Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 12/12 Roof Pitch Front Gable
- Brick & Stone Veneer on Front Elevation
- Mix of Lap, Board & Batten & Shingle Siding
- Composite Roof Shingles
- Wrapped Porch Columns



5-Plex - Front Elevation

20' 3-Story Rear Load Townhomes - Knightdale, NC

09.27.2024



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5-Plex - Rear Elevation

20' 3-Story Rear Load Townhomes - Knightdale, NC

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5-Plex - Sides Elevations

20' 3-Story Rear Load Townhomes - Knightdale, NC

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Architectural Features

- Covered Front Porches
- Variety of Window Fenestrations
- 12/12 Roof Pitch Front Gable
- Brick & Stone Veneer on Front Elevation
- Mix of Lap, Board & Batten & Shingle Siding
- Composite Roof Shingles
- Wrapped Porch Columns

3-Plex - Front Elevation

20' 3-Story Rear Load Townhomes - Knightdale, NC

09.27.2024



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3-Plex - Rear Elevation 20' 3-Story Rear Load Townhomes - Knightdale, NC

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3-Plex - Sides Elevations 20' 3-Story Rear Load Townhomes - Knightdale, NC

09.27.2024



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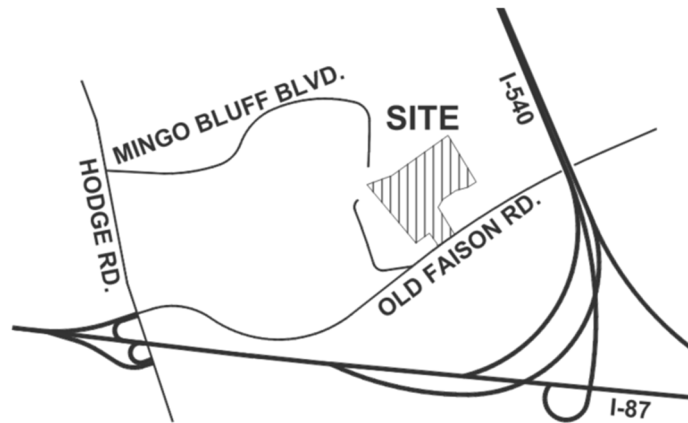
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OLD FAISON PLACE

A MASTERPLANNED RESIDENTIAL COMMUNITY IN KNIGHTDALE BY GLENWOOD HOMES



INTRODUCTION:

This development is located on approximately 21 acres off of Old Faison Road in Knightdale. Existing zoning is GR8 Medium Density. Proposed zoning is GR8 PUD. The project includes 12 proposed single family residential homes and 103 townhouses for a total of 115 residential units.

VISION STATEMENT:

Glenwood Homes was founded on the principal of quality homes for every lifestyle. We are passionate about building homes that have functional floor plans for today's life choices. We bring these homes to our homebuyers at a price that demonstrates value and the best quality possible. Our team has a wealth of knowledge in the North Carolina market and together has over 80 years of experience in building communities that will stand the test of time.

Our vision for this project in Knightdale is to create a vibrant, sustainable community that embodies quality and elegance. Drawing on our rich history of crafting attractive residential developments, we aim to blend innovative design with a commitment to environmental stewardship, fostering a neighborhood that not only enhances the beauty of the area but also promotes a strong sense of community. Our goal is to provide future residents with not just homes, but a lifestyle defined by comfort, accessibility, and connection.

UDO COMPLIANCE:

This project has been designed to comply with the Town of Knightdale Unified Development Ordinance, including but not limited to the zoning standards, allowable uses, buffers, landscaping, stormwater, utilities, lighting, roadway, parking, and open space requirements.

COMPREHENSIVE PLAN:

The Comprehensive Plan describes how the Town's population has grown substantially from 1,884 people in 1990 to 20,971 in 2023. The Town takes pride in its' expanding population, and this project will help the Town to continue to grow. The Plan indicates that the median home value in Knightdale is \$364,693. The proposed townhouses and single family houses in this development are expected to have a similar price point, and will complement the real estate value of the area. The Comprehensive Plan indicates that the Town has a shortage of townhouses: Only 6% of all residential units in Knightdale are townhouses. This project has a significant number of proposed townhouse units, and will help the Town to achieve a more balanced mix of residential housing types in the Town. The Comprehensive Plan indicates that the economy is an important factor to the Town. This project will create hundreds of new job opportunities with the construction of this development, and the additional residents will boost the available work force of the community. A hallmark of the Comprehensive plan is to provide comprehensive and connected open spaces. This project includes a connected open space corridor, including pickleball and tennis courts, multi-use field, playground, walking trail, landscaped areas, benches, fire pits, dog park, and a stormwater pond with a fountain. The Plan indicates that the Town intends for this property to be a "Secondary Growth Area", and is located next to a "trail-oriented development activity center". This project is proposing an internal walking trail. A wide pedestrian sidepath is intended to be constructed along Old Faison Road per the Town standards. The Plan indicates this land as a "future mixed-use neighborhood". However, this project is not proposing any commercial/retail uses, as the developer specializes in residential construction, and does not feel that there is a demand for commercial/retail uses at this location. The size, shape, and topography of this property provides limited buildable area, and does not provide enough buildable land for additional uses. This project qualifies as a "mixed density neighborhood", according to page 77 of the Comprehensive Plan. The project includes a grid street network with parallel parking along some streets, as well as multiple stub streets to adjacent properties, which is a goal of the Plan.

DESIGN GUIDELINES:

This project is designed to comply with the following design guidelines and others.

- Town of Knightdale UDO
- City of Raleigh Public Utilities Handbook
- NCDOT road design standards (Old Faison Rd)
- NCDEQ Stormwater BMP Design Manual
- NCDENR Sedimentation & Erosion Control Manual

DIMENSIONAL STANDARDS:

Refer to the Site Plan sheet, which shows the dimensions. The internal roads are designed per Town of Knightdale dimensional standards. The Old Faison Road widening is designed per NCDOT and Town dimensional standards. The townhouse and single-family residential lots are designed per Town of Knightdale dimensional standards.

TRANSPORTATION ANALYSIS:

A Traffic Impact Analysis report was prepared by DRMP traffic engineers and submitted to the Town. The report is included again with this Master Plan submittal. Refer to the report for additional information.

- The Town has asked the developer to extend and connect to existing Mingo Bluff Blvd to the north. As stated on the plans, the developer is willing to construct the road extension, but only if the Town is able to acquire the necessary road R/W and additional easements from the neighboring property owner to the north. Our project cannot legally perform work on the neighbor's property, and the Town must acquire the land before a road connection can be built.
- The Town requested that the developer construct a roundabout at our main entrance along Old Faison Road. The TIA indicates that a roundabout is not warranted. According to the TIA, a single eastbound left turn lane is needed along Old Faison Rd, as shown on the plans. The developer is not proposing to construct a roundabout, due to cost, and hardships of acquiring significant land from multiple adjacent residential properties.
- The Town asked the developer to perform a widening along the north shoulder for our property frontage, as well as the neighbor's property gap. The developer has agreed to do this, if the additional R/W can be obtained from the neighbors, as shown on the plans.
- The plans show that the south side of Old Faison Rd will be widened where needed to support the required turn lane and tapers. The developer is not proposing to do a full ultimate road widening on the neighbor's south road frontage, as that will be the responsibility of the property owners on the south side of Old Faison Rd when their property is improved.
- The Town asked the developer to construct a median in Old Faison Rd. This is not feasible, due to multiple existing driveways on both sides of the road, as the median would inhibit their driveway access.

- The Town asked the developer to construct a 10' sidepath along the north shoulder of Old Faison Road. The sidepath is shown on the plans.
- The proposed road network includes (2) road intersections with Old Faison Road, creating a looped road network and improved vehicular circulation for residents and emergency vehicles.
- The internal roads are designed based on the Town's design standards, and as discussed with Planning staff. The project is designed to provide on-street parallel parking in portions of the development. Residential properties are designed with individual driveway access on rear alleys per the Town's standards. A partial median is proposed on Mingo Bluff Blvd near the main entrance, as requested by the Town. It is not practical to construct additional medians due to the proximity of road intersections and driveways.
- Multiple road stubouts to neighboring properties are shown on the plans, which can be extended by potential future neighboring developments.

RECREATIONAL OPEN SPACE:

The plans show the following recreational open space types:

- (2) pickleball courts
- Tennis court
- Playground
- Multi-use field
- Walking trail
- Dog part
- (2) fire pits with hardscape & landscape areas
- (2) covered pavilions
- Picnic tables
- Stormwater wet detention pond with fountain

WATER ALLOCATION POINTS:

This project meets the Town's standards for water allocation points as follows:

- | | |
|---|-----------|
| • Major Subdivision: | 15 points |
| • Construct Fountain in Stormwater Pond: | 4 points |
| • On-Street Public Parking: | 4 points |
| • Architectural Standards: | 15 points |
| • Pickleball and Tennis Courts: | 5 points |
| • Enhanced Roadside Landscaping (Old Faison): | 2 points |
| • IPEMA Certified Playground Equipment: | 4 points |
| • 1,000 LF of 6-ft Wide Path: | 1 point |
| • Total: | 50 points |

Proposed Architectural Standards – Single Family

- Single Family units shall have a variety of siding materials, which shall include a minimum of three of the following: Fiber cement lap siding, Fiber cement shake, Fiber cement board and batten, Fiber cement smooth panel, Stone veneer, Adhered stone veneer, Brick veneer.
- Single Family units will all have recessed entry with min. 6" door trim.
- Single Family units shall have a variety of siding colors. No adjacent unit shall be painted the same color.
- Single Family units shall have architectural style roof shingles.
- Single Family unit roofs shall have a minimum main roof pitch of 5:12. Forward facing Gables shall be between 8:12-14:12 roof pitch. Accent and porch roofs shall be between 3:12-6:12 pitch. Accent elements consisting of Gable dormers, shed dormers are to be utilized when consistent with the style of the home.
- Metal accent roofs are encouraged.
- Single Family units will all be two stories.
- Single Family units shall have front covered porches, min 5' deep x 4' wide with a 6" minimum width pillar/post/column.
- Single Family unit roofs will all have either a dormer or gable accent.
- Single Family units will have at a minimum a covered porch and window trim of 4" minimum in width.
- Single Family units shall have 18% of glazing on the front.
- Single Family units shall have rear loaded garages.
- Single Family units shall have a minimum of 1500 heated square feet.
- Single Family roof eave overhang shall be a minimum of 12".
- 18" foundation brick or stone veneer.

Proposed Architectural Standards – Townhomes

- Townhouse units shall have a variety of siding materials, which shall include a minimum of three of the following: Fiber cement lap siding, Fiber cement shake, Fiber cement board and batten, Fiber cement smooth panel, Stone veneer, Adhered stone veneer, Brick veneer.
- Townhouse units will all have recessed entry with min. 6" door trim.
- Townhouse units shall have a variety of siding colors. No adjacent unit shall be painted the same color.
- Townhouse units shall have architectural style roof shingles.
- Townhouse unit roofs shall have a minimum main roof pitch of 5:12. Forward facing Gables shall be between 8:12-14:12 roof pitch. Accent and porch roofs shall be between 3:12-6:12 pitch. Accent elements consisting of Gable dormers, shed dormers are to be utilized when consistent with the style of the home.
- Metal accent roofs are encouraged.
- Townhouse units will all be three stories.
- Townhouse units shall have front covered porches, min 5' deep x 4' wide with a 6" minimum width pillar/post/column.
- Townhouse units roofs will all have either a dormer or gable accent.
- Townhouse units will have at a minimum a covered porch and window trim of 4" minimum in width.
- Townhouse units shall have 18% of glazing on the front.
- Townhouse units shall have rear loaded garages.
- Adjacent townhouse units shall have staggered setbacks as shown on the plan.
- Townhomes shall be a minimum of 19'-8" wide and a minimum of 1500 heated square feet.
- Roof eave overhang shall be a minimum of 12". Tight rake overhangs are permitted on porches and main roofs between townhome units. 12" min. rake overhangs on end unit exposed elevation.
- 18" minimum brick or stone veneer.

NICHOLLS & CRAMPTON, P.A.
ATTORNEYS AT LAW

W. Sidney Aldridge†
Gregory B. Crampton††
Nicholas J. Dombalis II (1950-2020)
Adam M. Gottsegen
Emmett Boney Haywood†
Isabel Worthy Mattox
Steven C. Newton
F. Timothy Nicholls
John B. Ward
Louis E. Wooten, III
Thomas C. Worth, Jr. (Of Counsel)
† Certified Superior Court Mediator
†† Board Certified Specialist in Business
Bankruptcy Law

October 2, 2024

3700 Glenwood Avenue
Suite 500
Raleigh, North Carolina 27612

Mailing Address:
Post Office Box 18237
Raleigh, North Carolina 27619

(919) 781-1311
FAX (919) 782-0465

Re: NOTICE OF MEETING:

Dear Knightdale Area Property Owner:

You are invited to a neighborhood meeting to learn more about a proposed project adjacent to or near your property. A Neighborhood Meeting is required by the Town of Knightdale Unified Development Ordinance and all property owners within 200 feet of the proposed development must receive notification of the meeting. The meeting will be an opportunity for residents and property owners to learn more about the project and provide feedback.

Meeting Date: October 15, 2024 Meeting Time: 5:30 pm

Meeting Location: See Attached Instructions

Application type: Planned Unit Development

Approving Authority: Town Council Legislative Approval

Address: 0 Old Faison Road

PIN: 1743463116 and 1743466606

Description of proposal: Residential- Single-Family and Townhomes

Estimated Submittal Date: October 28, 2024

We will introduce our development team, make a presentation about the proposed project and conduct a question-and-answer session. Enclosed is a map showing the location of the property being considered for this proposal. Additional materials and information may be available at the meeting.

If you have any questions, comments, or concerns about the proposal prior to the meeting, you may contact the project engineer Stephen Ballentine at sballentine@riversandassociates.com. These will be recorded as part of the meeting summary, which is submitted to Town staff and elected officials. You may also contact the Town of Knightdale Development Services Department at 919-217-2245.

Sincerely,

NICHOLLS & CRAMPTON, P.A.

Isabel Mattox

Isabel Worthy Mattox

IWM/dnc

Cc: Town of Knightdale Development Services Department

Project Contact Information

Project Name: Old Faison Place Proposed Zoning: GR8 PUD

Location: 0 Old Faison Road

Property PIN(s): 1743463116 and 1743466606 Acreage/Square Feet: 20.9 Acres

Property Owner: Ashbury & the Holdings Company LLC

Address: P.O. Box 90427

City: Raleigh State: North Carolina Zip: 27675

Phone: 919-741-7993 Email: tom@glenwoodhomes.com

Developer: Ashbury & the Holdings Company LLC

Address: P.O. Box 90427

City: Raleigh State: North Carolina Zip: 27675

Phone: 919-741-7993 Email: tom@glenwoodhomes.com

Engineer: Rivers & Associates, Inc.

Address: 353 E Six Forks Road, Suite 230

City: Raleigh State: North Carolina Zip: 27609

Phone: 919-594-1626 Email: sballentine@riversandassociates.com

Instructions for Virtual Meeting

1. Type the following URL into your internet browser:
https://teams.microsoft.com/l/meetup-join/19%3ameeting_ZTNkMGVhNDQtMTIjNy00NzVmLTk0YTctZWQ1ZDVhN2M3ZGY1%40thread.v2/0?context=%7b%22Tid%22%3a%22f31866cf-0afe-4b86-baaf-dd02d41bad50%22%2c%22Oid%22%3a%22736f66de-4e46-4b6b-b9db-c00dc8a0f18b%22%7d
2. Or email Darby Cody at dcody@nichollscrampton.com and receive an e-mail invitation.
3. Or go to the Microsoft Teams website and enter the meeting ID: **213 601 136 252** and the passcode: **dd32dp**.



0 200 400 800 ft
1 inch equals 400 feet

Disclaimer
iMaps makes every effort to produce and publish the most current and accurate information possible. However, the maps are produced for information purposes, and are **NOT** surveys. No warranties, expressed or implied, are provided for the data therein, its use, or its interpretation.

| Address | Owner |
|------------------------|---|
| 4906 OLD FAISON RD | GOWER, RICHARD MELVIN GOWER, SANDRA C |
| 3812 ROCKING ROBYN RUN | FORD, ERNEST A JR FORD, BRENDA D |
| 1207 PLEXOR LN | WALTERS, RUDY B |
| 1407 PLEXOR LN | EDDIE & LUKE ENTERPRISES LLC |
| 106 MONTVIEW WAY | BARAKAT, MIKE M |
| 4833 OLD FAISON RD | NEWBERN, LLOYD R |
| 1214 PLEXOR LN | HEADY, MARK R |
| 108 MONTVIEW WAY | HOOD-ROUSE, NINETHIA K ROUSE, DEANTHONY M |
| 907 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 901 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 903 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 905 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 909 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 911 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 913 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 915 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1001 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1003 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1005 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1007 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1009 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1011 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1013 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1015 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1101 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1103 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1105 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1107 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1109 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1111 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1113 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 1115 PLEXOR LN | MINGO CREEK INVESTMENTS II LLC |
| 508 MONTVIEW WAY | MINGO CREEK INVESTMENTS II LLC |
| 1306 PLEXOR LN | COLEMAN, MICHELLE N. |
| 0 MONEY TREE LN | HONEYCUTT, BRENDA J. |
| 4902 OLD FAISON RD | GRAY, CARLTON L |
| 1300 PLEXOR LN | SEALEY, TERRY VANN |
| 1205 PLEXOR LN | SHAHID-EL, LAILA |
| 4912 OLD FAISON RD | GOWER, JOSEPH A GOWER, CAROLYN W |
| 254 MONEY TREE LN | NARRON, ANTHONY NELSON |
| 4908 OLD FAISON RD | SEBASTIEN, DEMETRA P A SEBASTIEN, JUDE MICHEAL PHILIPPE |
| 1315 PLEXOR LN | POLLINO, TRASI |
| 0 MINGO BLUFF BLVD | FRED SMITH COMPANY |
| 230 MONEY TREE LN | WINDER, WILLIAM RAY WINDER, CATHERINE ANN |
| 1405 PLEXOR LN | LOOMIS, DANIEL ROBERT LOOMIS, TONYA LYNN |

| | |
|------------------------|---|
| 1213 PLEXOR LN | AMH NC PROPERTIES, LP |
| 1309 PLEXOR LN | AMH NC PROPERTIES, LP |
| 402 GOWER DR | WALL, TRAVIS DEE WALL, ALEATHIA CUTSHAW |
| 1203 PLEXOR LN | MANN, CHRISTOPHER |
| 1215 PLEXOR LN | CARIC CORPORATION |
| 4932 OLD FAISON RD | TORRES-GARCIA, MANUEL ELOY AGUILAR-PUENTES, MARGARITA |
| 1313 PLEXOR LN | ORTEGA, FRALIS M |
| 1305 PLEXOR LN | BUIE, JASMINE |
| 1216 PLEXOR LN | EMKE, CARRIE G EMKE, DAVID M |
| 1301 PLEXOR LN | DAVIS, CHARLES DAVIS, JOSIE B |
| 1012 FINALLY MY WAY | WALL, REBECCA |
| 1304 PLEXOR LN | AMH 2014-3 BORROWER LP |
| 1401 PLEXOR LN | RHOADES, AMY MILLRING |
| 1409 PLEXOR LN | MULL, MELISSA |
| 1308 PLEXOR LN | BURSE, DERRION STEWART, DERSHAN |
| 249 MONEY TREE LN | GP ASSOCIATES NORTH CAROLINA INC |
| 1307 PLEXOR LN | PURI, SUSAN PURI, RAVI |
| 4909 OLD FAISON RD | FANOUS, BASEL MAGDY MESEHA, ENGY ALBEER FAHMY |
| 4901 OLD FAISON RD | TIMOTHY A GRIFFIN LIVING TRUST |
| 1004 FINALLY MY WAY | TIMOTHY A GRIFFIN LIVING TRUST |
| 0 MINGO BLUFF BLVD | MINGO CREEK COMMUNITY ASSOCIATION INC |
| 1403 PLEXOR LN | SMALL, TAYLOR |
| 1311 PLEXOR LN | DAVIS, STEPHANIE |
| 4905 OLD FAISON RD | WALL, DALE A SR |
| 0 OLD FAISON RD | ASHBURY & THE HOLDING COMPANY LLC |
| 0 OLD FAISON RD | ASHBURY & THE HOLDING COMPANY LLC |
| 1211 PLEXOR LN | IBENSILALEN, MOHAMED AMEJJOD, MALIKA |
| 242 MONEY TREE LN | PYLES, GREGORY |
| 1310 PLEXOR LN | LEWIS, LOWRY DURANT |
| 1201 PLEXOR LN | POMNO LLC |
| 3804 ROCKING ROBYN RUN | THROWER, TIMOTHY L THROWER, SABRINA M |
| 1302 PLEXOR LN | HIGH, OMAR S |
| 4814 OLD FAISON RD | GOWER, JERRY L GOWER, JOANN W |
| 1303 PLEXOR LN | SMITH, JAMES C |
| 4812 OLD FAISON RD | BUTLER, TAMMY G |
| 1209 PLEXOR LN | AMH NC PROPERTIES, LP |

[illegible]

KNIGHTDALE NC 27545-9190
KNIGHTDALE NC 27545-8716
KNIGHTDALE NC 27545-7279
KAILUA KONA HI 96740
DURHAM NC 27703-7796
KNIGHTDALE NC 27545-9189
KNIGHTDALE NC 27545-7278
KNIGHTDALE NC 27545-9776
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CLAYTON NC 27527-9271
KNIGHTDALE NC 27545-6900
MACON NC 27551-9222
KNIGHTDALE NC 27545-9190
KNIGHTDALE NC 27545-6900
KNIGHTDALE NC 27545-7279
KNIGHTDALE NC 27545-9190
YOUNGSVILLE NC 27596-7537
KNIGHTDALE NC 27545-9190
KNIGHTDALE NC 27545-6901
CLAYTON NC 27527-5500
KNIGHTDALE NC 27545-8680
RALEIGH NC 27614-6555

| | | |
|--------------------------|----------------------------|----------------------------|
| PROPERTY TAX DEPT | 30601 AGOURA RD STE 200PT | AGOURA HILLS CA 91301-2150 |
| PROPERTY TAX DEPT | 30601 AGOURA RD STE 200PT | AGOURA HILLS CA 91301-2150 |
| 402 GOWER DR | KNIGHTDALE NC 27545-9700 | |
| 1203 PLEXOR LN | KNIGHTDALE NC 27545-7279 | |
| 11208 SADDLEWOOD CT | RALEIGH NC 27614-9697 | |
| 4932 OLD FAISON RD | KNIGHTDALE NC 27545-9190 | |
| 1313 PLEXOR LN | KNIGHTDALE NC 27545-6901 | |
| 7304 STONECLIFF DR APT 7 | RALEIGH NC 27615-7710 | |
| 1216 PLEXOR LN | KNIGHTDALE NC 27545-7278 | |
| 1301 PLEXOR LN | KNIGHTDALE NC 27545-6901 | |
| 1012 FINALLY MY WAY | KNIGHTDALE NC 27545-8754 | |
| 30601 AGOURA RD STE 200 | AGOURA HILLS CA 91301-2148 | |
| 1401 PLEXOR LN | KNIGHTDALE NC 27545-6903 | |
| 1409 PLEXOR LN | KNIGHTDALE NC 27545-6903 | |
| 1308 PLEXOR LN | KNIGHTDALE NC 27545-6900 | |
| 7048 KNIGHTDALE BLVD | KNIGHTDALE NC 27545-8894 | |
| 1307 PLEXOR LN | KNIGHTDALE NC 27545-6901 | |
| 4124 PALAFOX CT | RALEIGH NC 27604-3466 | |
| 1732 FONT HILLS LN | KNIGHTDALE NC 27545-8184 | |
| 1732 FONT HILLS LN | KNIGHTDALE NC 27545-8184 | |
| 2009 MINGO CREEK BLVD | KNIGHTDALE NC 27545 | |
| 1403 PLEXOR LN | KNIGHTDALE NC 27545-6903 | |
| 1311 PLEXOR LN | KNIGHTDALE NC 27545-6901 | |
| 4905 OLD FAISON RD | KNIGHTDALE NC 27545-9191 | |
| PO BOX 90427 | RALEIGH NC 27675-0427 | |
| PO BOX 90427 | RALEIGH NC 27675-0427 | |
| 1211 PLEXOR LN | KNIGHTDALE NC 27545-7279 | |
| 249 MONEY TREE LN | KNIGHTDALE NC 27545-8680 | |
| 290 BARN HILL LN | WAKE FOREST NC 27587-7916 | |
| 1201 PLEXOR LN | KNIGHTDALE NC 27545-7279 | |
| 4205 MARSH VIEW LN | ZEBULON NC 27597-5347 | |
| 1302 PLEXOR LN | KNIGHTDALE NC 27545-6900 | |
| 4814 OLD FAISON RD | KNIGHTDALE NC 27545-9188 | |
| 1303 PLEXOR LN | KNIGHTDALE NC 27545-6901 | |
| PO BOX 799 | SWANSBORO NC 28584-0799 | |
| PROPERTY TAX DEPT | 30601 AGOURA RD STE 200PT | AGOURA HILLS CA 91301-2150 |

Summary of Discussions

Provide a summary of any questions or comments received from meeting attendees along with responses by the applicant. If the question or comment will result in a change to the proposal, please state how that change will be made and the resulting follow up with the applicant. Use duplicate sheets if needed.

APPLICANT'S STATEMENT:

The Town has asked the developer to extend an offsite roadway connection of Mingo Bluff Blvd onto a neighbor's property. We cannot legally perform any work on the neighbor's property. However, if the Town is able to acquire the necessary R/W and easements from the property neighbor, then the developer has stated that he is willing to construct the road connection based on the Town's request. If the Town is not able to acquire the offsite R/W and easements, then the roadway connection will not be possible.

QUESTION/CONCERN:

What improvements is your project proposing to the existing Mingo Bluff Blvd located to the north of your project to alleviate our concerns about safety on the existing road?

Applicant's Response:

This project is not proposing any modifications to the portion of existing offsite Mingo Bluff Blvd in your neighborhood, other than the aforementioned roadway connection. We understand that the road was designed, approved, and constructed 20+ years ago per the Town's standards, and we are not aware of existing safety issues with the existing road in your neighborhood. Any improvements to the existing road in your neighborhood would need to be handled by your HOA or the Town.

QUESTION/CONCERN:

How will your project affect traffic on existing Mingo Bluff Blvd?

Applicant's Response:

Refer to the Traffic Impact Analysis report, which was submitted to the Town. If Mingo Bluff Blvd is connected, then this would improve the traffic of your neighborhood, as it would allow residents and emergency vehicles an additional way to get in and out of your neighborhood.

DEVELOPER'S STATEMENT:

The Town has asked our project to construct a roundabout located at the intersection of Old Faison Rd and Mingo Bluff Blvd. However, our project is not proposing to construct a roundabout for the following reasons:

A roundabout would require significant R/W acquisition of multiple neighboring residential lots, which would have significant impacts to their homes. A roundabout would be prohibitively expensive to build. A roundabout is not warranted from a traffic standpoint according to the Traffic Impact Analysis.

Summary of Discussions

Provide a summary of any questions or comments received from meeting attendees along with responses by the applicant. If the question or comment will result in a change to the proposal, please state how that change will be made and the resulting follow up with the applicant. Use duplicate sheets if needed.

QUESTION/CONCERN:

What is the expected completion date?

Applicant's Response:

The construction is expected to begin in 2026 and be completed around 2029, but this will be affected based on the timeframe of the permitting and market conditions.

QUESTION/CONCERN:

What is the planned square footage and selling price of the townhouses and houses?

Applicant's Response:

The townhouses and houses will have 1,500 SF minimum, but could be larger. The sale price will be based on market conditions in years to come. At this time, we anticipate a sale price in the \$450,000 range.

QUESTION/CONCERN:

How many houses and townhouses will be built?

Applicant's Response:

The current plan is proposing 103 townhouses and 12 single family houses.

DEVELOPER'S STATEMENT:

The Traffic Impact Analysis indicates that the only warranted turn lane along Old Faison Rd is an eastbound left turn lane at the Mingo Bluff Blvd entrance, which will be constructed. In addition, the Town has asked the developer to seek to acquire R/W and widen the road in the gap between our two entrances, which will affect several neighboring properties. The widening will be based on the Town's planned cross section of Old Faison Rd. Our project is proposing to widen our side of the road per the Town's ultimate cross section, which includes R/W dedication, curb and gutter and a 10' pedestrian side path. Our project is not proposing to widen the south side of Old Faison Road, except as needed for the turn lane tapers required by NCDOT. Any additional widening along the south road frontage would be the responsibility of the any potential development on the south side of Old Faison Rd. The Town asked us to construct a median in Old Faison Rd, but this will not be feasible, as it would inhibit multiple existing residential driveway accesses along Old Faison Rd.

Summary of Discussions

Provide a summary of any questions or comments received from meeting attendees along with responses by the applicant. If the question or comment will result in a change to the proposal, please state how that change will be made and the resulting follow up with the applicant. Use duplicate sheets if needed.

QUESTION/CONCERN:

A member of the Mingo Creek Townhouses HOA stated the following:

The Town promised our community that when the property to the south is developed, the Mingo Bluff Blvd Road connection would be extended to Old Faison Rd.

QUESTION/CONCERN:

The residents in the gap along Old Faison Rd asked how the widening would affect their properties, including drainage and R/W acquisition.

Applicant's Response:

The Town is requiring our project to widen Old Faison Rd and construct a 10' wide sidepath along your property frontage, so we have no choice. It will be necessary to relocate the existing roadside ditch further north onto your property, construct new driveway pipes, and connect the new ditch to a proposed storm pipe system under Old Faison Rd. This will be designed in the future after the rezoning is approved. The developer will contact you to discuss R/W acquisition.

QUESTION/CONCERN:

A member of the Mingo Creek Townhouses stated the following: Back around 2004 when our community was built, the Town did not require any guest parking. The Town only required a garage parking space and a driveway parking space. Some residents have converted their garage into a living room and this has caused a parking shortage problem in our neighborhood. Will your project make our existing parking problem worse?

Applicant's Response:

That is unfortunate that your community has no guest parking and that some of your residents have made the problem worse by eliminating their garages. Our proposed project includes a significant number of on-street parallel parking spaces. There is also a proposed amenity/guest parking lot near the entrance. We initially designed the amenity/guest parking lot to be larger, but the Town asked us to reduce it. Given the walking distance between your project and our units, and the fact that you have an existing parking problem and we do not, we do not anticipate that our residents will want to park in your neighborhood.

QUESTION/CONCERN:

Will there be a stop sign on Old Faison Road at the Mingo Bluff Blvd intersection?

Applicant's Response:

We are proposing a southbound stop sign on Mingo Bluff Blvd. However, there will be no stop sign on existing Old Faison Road, which is a significant NCDOT road. We believe it is very unlikely that NCDOT would allow a stop sign on their major road.

Summary of Discussions

Provide a summary of any questions or comments received from meeting attendees along with responses by the applicant. If the question or comment will result in a change to the proposal, please state how that change will be made and the resulting follow up with the applicant. Use duplicate sheets if needed.

DEVELOPER'S STATEMENT:

The Town has asked that this project include Mixed Use. However, this project is not proposing any mixed use for the following reasons:

Our team does not believe that there is demand for commercial/retail uses at this location, and any investment in this would likely fail. Commercial/retail uses like to be next to other commercial/retail uses, such as in a shopping center setting, so a small commercial/retail project would likely fail at this location. This is a residential developer.

Summary of Discussions

Provide a summary of any questions or comments received from meeting attendees along with responses by the applicant. If the question or comment will result in a change to the proposal, please state how that change will be made and the resulting follow up with the applicant. Use duplicate sheets if needed.

Question/Concern #1:

Street extension/improvements of Mingo Bluff Road

Applicant's Response:

The town wants this connection and we have agreed to build, if the Town acquires the necessary right-of-way

Question/Concern #2:

What is the estimated selling price on the townhomes and single-family houses?

Applicant's Response:

That is difficult to answer this far out but we believe prices will be in the mid \$400,000 (based on 2024 dollars)

Question/Concern #3:

When is the expected start and completion date?

Applicant's Response:

Lots of variables about construction, but expected to start in 2026 and complete in approximately 2.5 years.

Question/Concern #4:

How many townhouses and how many single-family homes will there be?

Applicant's Response:

We propose 103 townhomes and 12 single-family homes.

Question/Concern #5:

Will there be any improvements to the "old" side of Mingo Bluff that would allow for some consistency and enhancement with the new addition?

Applicant's Response:

We will not improve the existing Mingo Bluff road except for the possible road connection.

Question/Concern #6:

Will there be anything to improve Mingo Bluff Road to consider safety and the addition of traffic with this addition?

Applicant's Response:

Any connection to Mingo Bluff road will follow the Town of Knightdale standards and will be consistent with good engineering safety standards.

Question/Concern #6:

How will this affect 4908 Old Faison Road?

Applicant's Response:

We will need to acquire additional right-of-way adjacent to Old Faison road.

Question/Concern #6:

Is the intention to ultimately have the streets connect?

Applicant's Response:

If Knightdale timely obtains the necessary right-of-way, we will connect Mingo Bluff to our project..



Town of Knightdale | 950 Steeple Square Ct. | Knightdale, NC 27545
KnightdaleNC.gov | 919-217-2241

NEIGHBORHOOD MEETING ATTENDANCE SHEET

Please list Neighborhood Meeting Attendees who provided their name and/or contact information either during the meeting or via phone/email before or after the meeting. Use duplicate sheets if needed.

| | Name/Organization | Address | Phone Number | Email Address | Follow Up Requested? |
|-----|-------------------|-----------------------|--------------|-----------------------------|----------------------|
| 1. | John High | | 919-812-6192 | | |
| 2. | Tabitha Hagen | | | Tabitha_Hagen@bellsouth.net | |
| 3. | Richard Gower | 4906 Old Faison Road | | rgower1@nc.rr.com | |
| 4. | Ray Winder | 230 Money Tree Lane | | | |
| 5. | CB | | | | |
| 6. | Mingo Creek HOA | | | | |
| 7. | Demetra Allen | 4908 Old Faison Road | | dpsebastien@gmail.com | |
| 8. | Lisa Pierson | 1104 Willowedge Court | | | |
| 9. | Shahid-El, Laila | 1205 Plexor Lane | | | |
| 10. | | | | | |
| 11. | | | | | |
| 12. | | | | | |
| 13. | | | | | |
| 14. | | | | | |
| 15. | | | | | |

February 24, 2025

Gideon Smith
Town of Knightdale
950 Steeple Square Court
Knightdale, NC 27545

Reference: Olde Faison Place - Knightdale, North Carolina

Subject: Traffic Impact Assessment

Dear Mr. Smith,

This letter provides a summary of the traffic impact analysis prepared for the proposed Olde Faison Place development to be located on the north side of Old Faison Road between Hodge Road and Interstate 540 (I-540) in Knightdale, North Carolina. Through coordination with the Town of Knightdale (the Town), it was requested that a commercial parcel be included as a part of the development. While there is no known tenant and the land use is not yet determined, a conservative approach to this analysis was taken. For the purpose of this traffic impact analysis, a 3,500 square foot (s.f.) fast-food restaurant with drive-through was assumed to be constructed on the commercial parcel. The development is anticipated to be constructed in two (2) phases, summarized below:

Phase 1 – construction estimated to be complete by 2028

- 12 single-family homes
- 105 townhomes

Phase 2 – unknown completion (assumed 2028 for the purpose of this analysis)

- 3,500 s.f. fast food restaurant with drive through

The purpose of this study is to determine the potential impacts to the surrounding transportation system created by traffic generated by the proposed development, as well as recommend improvements to mitigate the impacts, if necessary.

Study Area and Analysis Scenarios

Based on coordination with the Town of Knightdale (Town), the proposed site accesses were the only intersections to be analyzed, and would be analyzed under the following conditions, as per the Town's Unified Development Ordinance (UDO):

- 2028 + 1 Traffic Conditions (Phase 1)
- 2028 + 10 Traffic Conditions (Phase 1)
- 2028 + 1 Traffic Conditions (Phase 2)
- 2028 + 10 Traffic Conditions (Phase 2)

Refer to Attachment A for the approved scoping documentation. Refer to Attachment B for the site location map.

Proposed Land Use and Site Access

The proposed development is expected to be constructed in two phases and consist of the land uses below:

Phase 1

- 12 single-family homes
- 105 townhomes

Phase 2

- 3,500 s.f. fast food restaurant with drive through (yet to be confirmed)

Completion of the proposed development is anticipated by the end of 2028. Site access is proposed via two (2) full-movement driveways along Old Faison Road. During scoping, the Town requested analysis of a potential connection to the existing Mingo Bluff Boulevard to the north. Per coordination with the Town, all intersections were analyzed under two scenarios.

- Scenario 1 - assumes that the connection to Mingo Bluff Boulevard is not constructed (access only available via Old Faison Road)
- Scenario 2 - assumes Site Access A operates as an extension of Mingo Bluff Boulevard.

Refer to Attachment B for a copy of the preliminary site plan.

Adjacent Land Uses

The proposed development is located north of I-87, in an area consisting primarily of residential development.

Existing Roadways

Speed limits, cross section, and other roadway information about Old Faison Road are shown in Table 1 below.

Table 1: Existing Roadway Inventory

| Road Name | Route Number | Typical Cross Section | Speed Limit | ADT (vpd) |
|-----------------|--------------|-----------------------|-------------|-----------|
| Old Faison Road | SR 2515 | 2-lane undivided | 45 mph | *6,510 |

*ADT based on the traffic counts from 2024.

Existing Traffic Conditions

Existing peak hour traffic volumes were determined by collecting traffic count data along Old Faison Road at the approximate location of the two proposed site driveways. Weekday count data was recorded for 24-hours but only the typical AM (7:00 – 9:00) and PM (4:00 – 6:00) peak hours were considered in the capacity analysis. Refer to Attachment B for an illustration of 2024 existing traffic volumes and Attachment C for the raw traffic count data.

2029 (+1) and 2038 (+10) No-Build Traffic Conditions

As mentioned previously, the Town’s UDO requires a build +1 and build +10 analysis. Existing traffic volumes were projected to the future year 2029 utilizing a 3% annual growth rate approved by the Town. For each year after 2029, a 1% growth rate was applied. Refer to Attachment B for illustrations of the 2029 and 2038 projected peak hour traffic volumes.

The following adjacent developments were identified to be included as a part of the analysis:

- Silverstone
- Stone River
- Riverview Commons
- Lyndon Oaks

Refer to Attachment D for additional adjacent development data. It should be noted that Lyndon Oaks is the only development that adds site traffic along Old Faison Road, therefore this was the only development included in the analysis.

2029 and 2038 no-build traffic volumes were determined by adding the adjacent development trips to the correlating projected peak hour traffic volumes. Refer to Attachment B for illustration of the adjacent development trips and 2029/2038 no-build peak hour traffic volumes.

Site Trip Generation

Average weekday daily, AM peak hour, and PM peak hour trips for the proposed development were estimated using methodology contained within the ITE Trip Generation Manual, 11.1 Edition. Table 2a provides a summary of the trip generation potential for the site under each phase.

Table 2a: Site Trip Generation (Phase 1)

| Land Use (ITE Code) | Intensity | Daily Traffic (vpd) | Weekday AM Peak Hour Trips (vph) | | Weekday PM Peak Hour Trips (vph) | |
|---------------------------------|-----------|---------------------------|--|------|--|------|
| | | | Enter | Exit | Enter | Exit |
| Single-Family Detached (210) | 12 DU | 144 | 3 | 8 | 9 | 5 |
| Single-Family Attached (215) | 105 DU | 750 | 12 | 37 | 35 | 24 |
| Total Trips | | 894 | 15 | 45 | 44 | 29 |

Table 2b: Site Trip Generation (Full Build)

| Land Use (ITE Code) | Intensity | Daily Traffic (vpd) | Weekday AM Peak Hour Trips (vph) | | Weekday PM Peak Hour Trips (vph) | |
|--|-----------|---------------------------|--|------|--|------|
| | | | Enter | Exit | Enter | Exit |
| Single-Family Detached (210) | 12 DU | 144 | 3 | 8 | 9 | 5 |
| Single-Family Attached (215) | 105 DU | 750 | 12 | 37 | 35 | 24 |
| Fast-Food w/ Drive-Thru (934) | 3.5 KSF | 1,636 | 80 | 76 | 60 | 56 |
| Total Trips | | 2,530 | 95 | 121 | 104 | 85 |
| <i>Pass-By Trips: Fast-Food Restaurant with Drive-Through (50% AM, 55% PM)</i> | | | -39 | -39 | -32 | -32 |
| Total Primary Trips* | | | 56 | 82 | 72 | 53 |

*No internal capture assumed; 5% of regional distributions assumed to originate from proposed development.

Trip Distribution and Assignment

The site trips were distributed based on existing traffic patterns, population centers adjacent to the study area, and engineering judgment. A summary of the residential regional distributions for Scenario 1 is shown below:

- 85% to/from the west via Old Faison Road
- 15% to/from the east via Old Faison Road

A summary of the residential regional distributions for Scenario 2 is shown below:

- 75% to/from the west via Old Faison Road
- 15% to/from the east via Old Faison Road
- 10% to/from the north via Mingo Bluff Boulevard

Primary (new) trips associated with the commercial parcel were distributed as follows under Scenario 2:

- 80% to/from the west via Old Faison Road
- 15% to/from the east via Old Faison Road
- 5% to/from within proposed Old Faison Place development OR within adjacent Mingo Bluff development (accessing via potential Mingo Bluff Boulevard connection)

Additionally, with the potential connection to Mingo Bluff Boulevard, trips from the existing Mingo Bluff development would be expected to be diverted from their current route (Mingo Bluff Boulevard-to-Hodge Road-to-Old Faison Road) to the new Mingo Bluff Boulevard connection directly to Old Faison Road (via proposed Site Access A). Traffic counts at the intersections of Hodge Road/Mingo Bluff Boulevard and Hodge Road/Old Faison Road were used to estimate the amount of Mingo Bluff Boulevard traffic that could be diverted with an extension connecting to Old Faison Road. The diverted peak hour traffic was only utilized under Scenario 2. Refer to Attachment B for illustrations of the site trip distributions and assignments, diverted trip distributions and assignments, and total trip assignments.

2029 (+1) and 2038 (+10) Build Traffic Conditions

To estimate the traffic conditions with the site built-out, the site trips were added to the no-build traffic volumes for each respective analysis year, scenario, and phase. Refer to Attachment B for an illustration of the 2029 and 2038 build peak hour traffic volumes for each scenario and phase.

Traffic Capacity Analysis

Traffic capacity analysis for the study intersections was performed using Synchro 11.1 and SIDRA, which are comprehensive software packages that allows the user to model signalized, unsignalized, and roundabout intersections to determine levels-of-service based on the thresholds specified in the Highway Capacity Manual (HCM).

Refer to Table 3 for HCM levels of service and related average control delay per vehicle for signalized intersections, unsignalized intersections, and roundabouts.

Table 3: Highway Capacity Manual – Levels-of-Service and Delay

| Unsignalized Intersections and Roundabouts | | Signalized Intersections | |
|--|---|--------------------------|---|
| Level of Service | Average Control Delay per Vehicle (Seconds) | Level of Service | Average Control Delay per Vehicle (Seconds) |
| A | 0-10 | A | 0-10 |
| B | 10-15 | B | 10-20 |
| C | 15-25 | C | 20-35 |
| D | 25-35 | D | 35-55 |
| E | 35-50 | E | 55-80 |
| F | >50 | F | >80 |

Old Faison Road and Site Access A

Refer to the table below for a summary of the capacity analysis of the subject intersection during the analysis scenarios.

Table 4a: Analysis Summary for Old Faison Road and Site Access A

| ANALYSIS SCENARIO | A P P R O A C H | LANE CONFIGURATIONS | WEEKDAY AM PEAK HOUR LEVEL OF SERVICE | | WEEKDAY PM PEAK HOUR LEVEL OF SERVICE | |
|-------------------------------------|--------------------------------------|--|---|-------------------|--|-------------------|
| | | | Approach | Overall (seconds) | Approach | Overall (seconds) |
| 2029 Build – Scenario 1 (Phase 1) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT , 1 RT | A (8) ¹ -- B (12) ² | N/A | A (9) ¹ -- C (15) ² | N/A |
| 2029 Build – Scenario 2a (Phase 1) | EB WB SB | 1 LT , 1 TH 1 TH- RT 1 LT , 1 RT | A (8) ¹ -- B (14) ² | N/A | A (9) ¹ -- C (18) ² | N/A |
| 2029 Build – Scenario 2b* (Phase 1) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | A (9) A (6) A (5) | A (7) | A (7) A (9) A (6) | A (8) |
| 2029 Build – Scenario 1 (Phase 2) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT , 1 RT | A (8) ¹ -- C (17) ² | N/A | A (9) ¹ -- C (19) ² | N/A |
| 2029 Build – Scenario 2a (Phase 2) | EB WB SB | 1 LT , 1 TH 1 TH, 1 RT 1 LT , 1 RT | A (8) ¹ -- C (18) ² | N/A | A (10) ¹ -- C (22) ² | N/A |
| 2029 Build – Scenario 2b* (Phase 2) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | A (10) A (6) A (6) | A (8) | A (8) B (10) A (7) | A (9) |

Improvements/lane modifications by developer shown in bold.

*Analyzed as roundabout per Town of Knightdale UDO.

1. Level of service for major-street left-turn movement.

2. Level of service for minor-street approach.

Table 4b: Analysis Summary for Old Faison Road and Site Access A

| ANALYSIS SCENARIO | APPROACH | LANE CONFIGURATIONS | WEEKDAY AM PEAK HOUR LEVEL OF SERVICE | | WEEKDAY PM PEAK HOUR LEVEL OF SERVICE | |
|-------------------------------------|-----------------------|--|---|-------------------|--|-------------------|
| | | | Approach | Overall (seconds) | Approach | Overall (seconds) |
| 2038 Build – Scenario 1 (Phase 1) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT , 1 RT | A (8) ¹ -- B (13) ² | N/A | A (9) ¹ -- C (16) ² | N/A |
| 2038 Build-Scenario 2a (Phase 1) | EB WB SB | 1 LT , 1 TH 1 TH- RT 1 LT , 1 RT | A (8) ¹ -- B (15) ² | N/A | A (10) ¹ -- C (19) ² | N/A |
| 2038 Build – Scenario 2b* (Phase 1) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | A (9) A (6) A (5) | A (8) | A (8) B (10) A (7) | A (9) |
| 2038 Build – Scenario 1 (Phase 2) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT , 1 RT | A (8) ¹ -- C (18) ² | N/A | A (10) ¹ -- C (21) ² | N/A |
| 2038 Build-Scenario 2a (Phase 2) | EB WB SB | 1 LT , 1 TH 1 TH, 1 RT 1 LT , 1 RT | A (9) ¹ -- C (20) ² | N/A | A (10) ¹ -- C (24) ² | N/A |
| 2038 Build – Scenario 2b* (Phase 2) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | B (11) A (6) A (6) | A (9) | A (8) B (11) A (8) | A (10) |

Improvements/lane modifications by developer shown in bold.

*Analyzed as roundabout per Town of Knightdale UDO.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis under all conditions indicates the major-street left-turn movement is expected to operate at LOS A. The minor-street approach of the proposed site access (and Mingo Bluff Boulevard Extension) is expected to operate at an overall LOS C or better during the AM and PM peak hours.

Turn lanes were considered based on the North Carolina Department of Transportation *Policy on Street and Driveway Access to North Carolina Highways*. An eastbound left-turn lane with 75 feet of storage is warranted and recommended by the developer under Phase 1 conditions. Under Phase 2 conditions, an eastbound left turn lane with 125 feet of storage and a westbound right turn lane with 50 feet of storage are warranted and recommended by the developer.

Per scoping with the Town of Knightdale, 8-hour, 4-hour, and peak hour warrants for signalization were analyzed at the intersection. Under phase 2 conditions with the connection to Mingo Bluff, 8-hour and 4-hour warrants for signalization are expected to be met. 50% reduction of right turns was assumed along the major and minor street; however, if ample storage is supplied along the minor-street, right-turns to be included in the signal warrant analysis could further be reduced resulting in warrants for signalization not being met. If connection to Mingo Bluff occurs, the intersection should be monitored for signalization in the future due to uncertainty of the commercial land use at this time.

Per the Town's Unified Development Ordinance (UDO) and coordination with the Town, Site Access A was also analyzed as a single-lane roundabout (Scenario 2b). When analyzed as a roundabout, the overall intersection and each approach are expected to operate at LOS B or better during the AM and PM peak hours. No significant queuing is expected at the approach under any conditions.

Due to acceptable operations (including insignificant queuing) without and with the connection to Mingo Bluff Boulevard, no roundabout appears to be necessary. Upon completion of the development, the intersection should be monitored for signalization in the future.

Old Faison Road and Site Access B

Refer to the table below for a summary of the capacity analysis of the subject intersection during the analysis scenarios.

Table 5a: Analysis Summary of Old Faison Road and Site Access B

| ANALYSIS SCENARIO | A P P R O A C H | LANE CONFIGURATIONS | WEEKDAY AM PEAK HOUR LEVEL OF SERVICE | | WEEKDAY PM PEAK HOUR LEVEL OF SERVICE | |
|-----------------------------------|--------------------------------------|---|---|-------------------|---|-------------------|
| | | | Approach | Overall (seconds) | Approach | Overall (seconds) |
| 2029 Build – Scenario 1 (Phase 1) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | A (8) ¹ -- C (15) ² | N/A | A (9) ¹ -- C (18) ² | N/A |
| 2029 Build – Scenario 2 (Phase 1) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | A (8) ¹ -- C (15) ² | N/A | A (9) ¹ -- C (18) ² | N/A |
| 2029 Build – Scenario 1 (Phase 2) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | A (8) ¹ -- C (15) ² | N/A | A (9) ¹ -- C (19) ² | N/A |
| 2029 Build – Scenario 2 (Phase 2) | EB WB SB | 1 LT -TH 1 TH- RT 1 LT - RT | A (8) ¹ -- C (15) ² | N/A | A (9) ¹ -- C (19) ² | N/A |

Improvements/lane modifications by developer shown in bold.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Table 5b: Analysis Summary of Old Faison Road and Site Access B

| ANALYSIS SCENARIO | A P P R O A C H | LANE CONFIGURATIONS | WEEKDAY AM PEAK HOUR LEVEL OF SERVICE | | WEEKDAY PM PEAK HOUR LEVEL OF SERVICE | |
|-----------------------------------|--------------------------------------|--|---|-------------------|---|-------------------|
| | | | Approach | Overall (seconds) | Approach | Overall (seconds) |
| 2038 Build – Scenario 1 (Phase 1) | EB WB SB | 1 LT-TH 1 TH-RT 1 LT-RT | A (8) ¹ -- C (16) ² | N/A | A (9) ¹ -- C (20) ² | N/A |
| 2038 Build-Scenario 2 (Phase 1) | EB WB SB | 1 LT-TH 1 TH-RT 1 LT-RT | A (8) ¹ -- C (16) ² | N/A | A (9) ¹ -- C (20) ² | N/A |
| 2038 Build – Scenario 1 (Phase 2) | EB WB SB | 1 LT-TH 1 TH-RT 1 LT-RT | A (8) ¹ -- C (17) ² | N/A | A (9) ¹ -- C (20) ² | N/A |
| 2038 Build-Scenario 2 (Phase 2) | EB WB SB | 1 LT-TH 1 TH-RT 1 LT-RT | A (8) ¹ -- C (17) ² | N/A | A (9) ¹ -- C (20) ² | N/A |

Capacity analysis under all conditions indicates the major-street left-turn movement is expected to operate at LOS A. The minor-street approach of the proposed site access is expected to operate at an overall LOS C during the AM and PM peak hours. Turn lanes were considered based on the NCDOT *Policy on Street and Driveway Access to North Carolina Highways*. No turn lanes are warranted at Site Access B.

Due to acceptable operations at the intersection, no improvements are recommended by the developer.

Findings and Summary

Capacity analysis at all study intersections was completed in accordance with the Town of Knightdale UDO and NCDOT capacity analysis guidelines. All study area intersections are expected to operate at acceptable levels-of-service. Based on the findings of this study, it was determined that the proposed development would not have a major impact on traffic operations in this corridor. See below for a description of the recommended lane configuration and traffic control at the proposed access points along Old Faison Road. Refer to Attachment B for an illustration of the recommended lane configuration and traffic control.

Phase 1

Old Faison Road and Site Access A

- Construct Site Access A as a full-movement access with one ingress lane and two egress lanes with approximately 100 feet of storage.
- Construct an exclusive eastbound left turn lane on Old Faison Road with approximately 75 feet of storage plus appropriate deceleration and taper.
- Provide stop control for Site Access A.

Old Faison Road and Site Access B

- Construct Site Access B as a full-movement access with one ingress lane and one egress lane.
- Provide stop control for Site Access B.

Phase 2

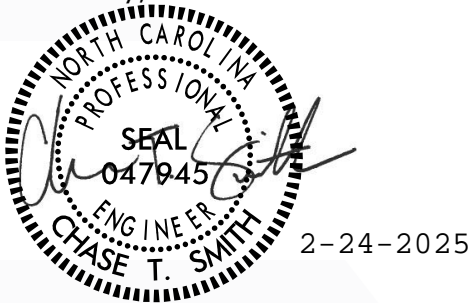
Old Faison Road and Site Access A

- Construct an exclusive westbound right turn lane with 50' of storage plus appropriate deceleration and taper.
- Monitor for signalization upon completion of Phase 2.

As mentioned, the land use for the commercial parcel is yet to be determined. Additionally, the developer of the residential parcel is unlikely to develop the commercial parcel, instead selling it to a separate entity exclusively for development of the parcel. At such time that a tenant is known, the aforementioned improvements recommended for Phase 2 (commercial parcel) should be verified, with new analysis and traffic provided when necessary.

If you have questions regarding information contained herein, feel free to contact me at (336) 714-0112.

Sincerely,



Chase Smith, PE
Traffic Analysis Project Manager
DRMP, Inc.

Attachments

- Attachment A – Approved Scoping Document
- Attachment B – Figures
- Attachment C – Traffic Count Data
- Attachment D – Adjacent Development Information
- Attachment E – Synchro, SIDRA, and SimTraffic Reports
- Attachment F – Turn Lane Warrants
- Attachment G – Signal Warrant Analysis